



**SEATTLE CENTRAL
COLLEGE**

**DRAFT MIMP
PROJECT #3034600-LU**

BROADWAY EDISON CAMPUS

October 2024

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EXECUTIVE SUMMARY

This Draft Major Institutional Master Plan (MIMP) documents the expected development of the Seattle Central campus over the coming foreseeable future.

This plan is based on a projected growth of enrollment of 22% from 2019's fall FTE (full time equivalent) to a total of 7,500. It also included an expected increase in the resident population from the current 70 students, to a planned 500. To support the planned growth and to address current space deficiencies/excesses, new facilities, renovations, and expansion need to be developed. This planned development included in this MIMP anticipates a total of approximately 350,000 square feet of new space (excludes parking garage and below grade utility plant space). This represents an almost 50% increase over the existing campus area.

New academic, residential, student-centered spaces, along with structured parking and retail/street-activation uses make up the planned increase. Recent careful and purposeful planning has seen the college's disposition of four under-utilized buildings to a community non-profit housing organization for developing housing to support under-represented neighborhood populations. Additionally, the college demolished the severely aging North Plaza building. Combined this represents a reduction of over 49,000 square feet from the previous MIMP inventory.

As funding for maintenance and operation of its facilities is limited, the College has committed to a more compact and contiguous campus. This will help support preservation and improvements to the greater campus environment including its open space, greenspace, pedestrian connections, active outdoor spaces, and general campus safety. With the efficient use of its currently owned land parcels, Seattle Central College can fully meet its planned development without any new site acquisitions. For any of the potential projects to be realized, expansion of the MIMP boundary would be required and is included in this MIMP.

While proposing an increase of allowable density from an FAR of 2.1 to 2.50, the proposed MIO boundaries' underlying zones have densities twice the proposed (5.5 at NCP3-75 and 5.75 at MR). This density represents that approximate actual density should all development proposed in this MIMP be realized. This lower density (buildings with lower allowable heights) will result in greater interaction between the SCC and Capitol Hill communities, strengthening and integrating an overall sense of community. Lower building heights, ease movement between classes (elevators required to move students between classes is inefficient and time-consuming). For these reasons, student intensive uses, and instructional spaces will typically occupy the lower floors of buildings. Upper floors will house residential, administration, and research uses.

This MIMP document describes characteristics of the plan in greater detail along with other information required by the Major Institution Overlay Code (SMC 23.69).



"Park Sculpture" by Charles W. Smith, 1975

CHAPTER ONE - INTRODUCTION

An Introduction to Seattle Central College


Seattle Central College (SCC) opened in 1966 and is the oldest of the three colleges that collectively make up the Seattle Colleges district. The college has a national reputation for excellence and innovation in its educational offerings. SCC serves approximately 10,000 students each academic quarter and is the center of many special community programs.

SCC is located on Capitol Hill, the vibrant urban center of Seattle life. It is the educational home for students, a leadership incubator for our community, and an economic catalyst for our state and beyond. Since 1966, the college has served the higher education and workforce training needs of more than 500,000 students.

Seattle Central College is committed to creating a learning environment that is accessible, diverse, responsive, and innovative.

- Seattle Central College has an open admissions policy.
- The campus is minutes from downtown Seattle and easy to reach from every part of the city via public transportation.
- Tuition at Seattle Central College is half the price of a four-year public college or university.
- There are departments dedicated to serving special populations such as veterans, former foster youth, first-generation college students, students of color, undocumented students, students with disabilities and many more.
- There is something for everyone at Seattle Central College: distance learning, college transfer, online courses, basic studies, ESL, and evening and weekend classes.
- Students from across town and across the world come together to explore their possibilities, extend their knowledge, and expand their potential. The student body numbers almost 15,000 students, including more than 1,400 from other counties.
- The college takes great pride in its diversity. Greater than 50% of the students identify as students of color. Thirty percent of the full-time faculty are people of color.
- Nearly half of the degrees and certificates are awarded to students of color.
- The professional and technical training programs prepare students for high-demand careers in healthcare, information technology, and more.
- The Worker Retraining office helps people get re-employed as soon as possible by offering the vital training, skills, and credentials needed for today's job market.
- Seattle Central College is one of several community and technical colleges in Washington state that offers a Bachelor of Applied Science degree.
- The college transfer program not only prepares students for the rigors of a four-year college, but it also provides an ever-evolving curriculum in small, creative classes.
- There are more than 40 student-created clubs and committees on campus that reflect diversity, instill self-reliance, leadership, and responsible action.

Seattle Central College is accredited by the Northwest Commission on Colleges and Universities, an institution-accrediting body recognized by the Council for Higher Education Accreditation and the U.S. Department of Education.



FACTS

Academic Year 2021-2022

OUR STUDENTS

Total Headcount 10,178 FTE 4,782 Running Start 644 International 800

PREPARE FOR THEIR FUTURE

Highest Enrolled Programs (Top 5)

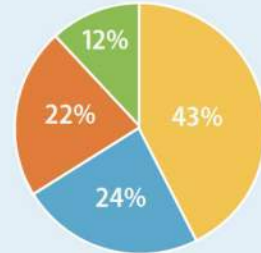
- Nursing
- Computer Science
- Information Technology
- Associate of Arts
- Social & Human Services

Programs of Study

- 6 Bachelor of Applied Science Degrees
- 23 Associate Degrees
- 16 Certificates (Excluding Short-term)

Enrollment By Purpose for Attending (FTEs)

- Academic Transfer
- Workforce Education
- Basic Skills
- Other




FIND SUCCESS

Graduates (1296 Total)

- 600** Associate – transfer
- 79** Bachelor
- 237** Certificate Long-term (≥ 45 credits)
- 73** Certificate Short-term (< 45 credits)
- 67** High School Completion/GED
- 240** Workforce Associate (Career & Technical Education)

65.3% People of Color
2.27% People with Disabilities



70%

Career & Technical Grads are employed nine months after graduation or continue with their education.

ARE DIVERSE

Ethnicity


- 41%** White/Caucasian
- 31%** Asian
- 2%** Pacific Islander
- 24%** Black/African American
- 16%** Hispanic/Latino
- 2%** Native American

May not add up to 100% because students may be counted in more than one race.

24


Median Age

60%



Female


40%



Male

RECEIVE SUPPORT

15:1 Student to faculty ratio




29%

Percentage of classes taught by people of color

\$1 MILLION+ Scholarships awarded by the Seattle Colleges Foundation

25% Students receiving need-based financial aid

\$7,222 Average financial aid award accepted* (*20-21 data)



BALANCE BUSY LIVES





22% Students with Children 

44% Students Who Work*  (*20-21 data)

Surrounding Neighborhood and Relationship to Seattle Central College

Location

Seattle Central College is in the vibrant Capitol Hill neighborhood, just north of the Pike/Pine neighborhood, and at the southern end of the active Broadway Avenue commercial district. Its campus buildings sit amongst other commercial and residential properties between E Pike Street and E Denny Way in the north-south direction, and Boylston Avenue and Cal Anderson Park in the east-west direction. Broadway is a main commercial street which runs through the middle of campus and is the main organizing element for all campus and neighborhood circulation.

The Capitol Hill Sound Transit Station and the mixed-use developments that surround it above have brought new energy to the area and established this location as a major activity point in the city. Seattle Central College's proximity provides many opportunities for campus and community engagement in the neighborhood.

Located at the nexus of the Capitol Hill and Pike/Pine Urban Villages (as defined by the City of Seattle) with Capitol Hill to the north and Pike/Pine to the south, the campus is also split by the Capitol Hill Station Overlay District on the north, and the Pike/Pine Conservation District on the south. In proximity are fellow major Institutions: Seattle University, Kaiser Permanente, and Swedish Medical Center. The surrounding neighborhood is a mixed medium-to-high density area with a strong commercial core, multifamily housing, apartment buildings, civic institutions, hospitals, and schools.



Aerial view of the SCC campus from Capitol Hill looking northwest toward the South Lake Union Neighborhood

Major Institution Master Plan (MIMP) Background

The Seattle City Council adopted the current SCC MIMP in 2002. That planning horizon was anticipated to serve a 10-year period, but it remains in effect today, twenty years later. Most of the envisioned projects have now been completed. While the SCC Master Plan still reflects the college's vision, changed conditions and new opportunities require that the plan be revisited. Two important developments have taken place in the immediate college vicinity that have substantively impacted the college's future.

First, a Sound Transit station was completed adjacent to campus. A portion of the Sound Transit land used to construct the southwestern station has recently been acquired by SCC. The acquired land and its proximity to transit make it a logical location for one of the college's next major planned projects.

Second, the city's "up zone" of the station overlay district and the 2012 development agreement between the City and Sound transit has brought an explosion of mixed-use developments to the immediate vicinity, including thousands of new residential units on the Sound Transit lots and elsewhere.

Key Issues

There are several issues that are addressed by the master plan. The key issues include:

- Expansion of the MIO (Major Institution Overlay) boundary to include acquired parcels as well as additional parcel that support consolidation of SCC's campus
- The location and nature of future growth (boundaries, property acquisition, development density)
- Enrollment-driven space shortages due to college and community resource programs and the best utilization of existing facilities recognizing the extended hours of operation
- Identification of strategies to address space need deficiencies for instruction, student center, student housing, library, and auditorium uses
- Parking, security, and transit linkages
- Uncertainty and timing of state funding and the need for flexibility
- Seattle Colleges District office needs and location
- Off-campus programs, facilities, and their relationships with the Broadway Edison Campus
- Neighborhood changes and development intensification, shared campus uses and support

Enrollment

SCC's growing access to mass transit service, the region's booming tech sector, and population growth should drive annual increases in enrollment.

Future enrollment growth for the master plan period is expected to be significant, due primarily to an explosion of in-migration to the city of Seattle – and, in particular, SCC's service area. From 2010 to 2020 the city saw a net population increase of 21.1%. (130,000 people). (Source is Seattle Times article of December 7, 2021). The Washington State Board of Community and Technical Colleges projects a population increase of seven percent (7%) over the next 10-years (ending 2029) in SCC's service area. Voters in the city of Seattle in 2018 authorized a city-wide property tax to fund two years of free community college tuition for graduates of Seattle's public high schools. This new initiative has spurred a jump in Fall 2019 enrollment of recent high school graduates.

The Planned Development included in this MIMP will support growth to an expected enrollment of 7,500 Full Time Equivalent (FTE) students. This equates to growth of 22% over the 6,132 FTE fall quarter of 2019.

New MIMP Application

In the summer of 2019, Seattle Central College began the process of developing a new MIMP. This internal Concept Plan document represented the beginning of the formal MIMP process, as specified in Section 23.69.032. C. of the Seattle Land Use and Zoning Code. The components required by the Code are as follows:

- *Proposed institution boundaries*
- *A proposed site plan including planned development and an estimate of total gross floor area proposed by the Major Institution*
- *Planned uses*
- *Any planned street vacations and planned parking location and access*
- *A description of alternative proposals for physical development and decentralization options, including a detailed explanation of the reasons for considering each alternative*
- *A description of the uses and character of the neighborhood surrounding the major institution and how the Major Institution relates to the surrounding area. This shall include pedestrian connections, physical and visual access to surrounding amenities and services, and the relationship of the Major Institution to other Major Institution development within two thousand five hundred (2,500) feet of its MIO District boundaries*

It is vital for SCC to conduct its own internal planning to better respond to the transit and neighborhood initiatives. Changing education and community service needs must be addressed in future college planning. The master planning process provides an opportunity to inform the community and encourage participation in shaping the future of Seattle Central College.

Issues and Needs

The profile of user needs for SCC is characterized by:

- Stable but growing enrollment (note: SCC's enrollment generally runs inversely with the Seattle area economic growth)
- Changes to program needs mix (more academic, workforce, and basic skills, less vocational)
- Changes to program needs for new initiatives (primarily transfer-based programs)
- Increasing number of transfer students (more likely to be full-time on-campus)
- More services for targeted groups (such as ABE, ESL, BTS, High School programs and International Students)
- Importance of access due to adjacency of downtown business district and service area expansion (Sound Transit and Seattle Streetcar)
- Increasing use of college facilities for community program use

There are several major issues that are addressed by the master plan. The key issues include:

- Expansion of the MIO (Major Institution Overlay) boundary to include acquired parcels
- The location and nature of future growth (boundaries, property acquisition/dispersal, development density)
- Space shortages due to college and community resource programs and the best utilization of existing facilities recognizing the extended hours of operation and appropriateness for the College mission
- Identification of strategies to address space deficiencies for Basic Skills, Library/LRC, and Auditorium uses
- Parking, security, and transit linkages
- Uncertainty and timing of state funding and the need for flexibility
- District office needs and location
- Off-campus programs, facilities, and relationships with the Broadway Edison Campus
- Neighborhood changes and development intensification, shared campus uses and support

Purpose and Use

One of the primary components of this master plan document is to project the needs of SCC. This document will be used to support the college's biennial funding request in the state capital budget process as well as efforts to secure funding via private-public partnerships. The state capital budget provides funding for all community and technical colleges to maintain and preserve state-owned facilities, upgrade program spaces to meet the changing needs of students, local communities, and businesses, and to construct new facilities to accommodate growth and accreditation requirements.

As part of the SBCTC Capital Budget Process, SCC submits capital requests that support their most critical needs. These requests are divided into major categories such as repairs, minor improvements, replacements, renovations, and major new construction, which are prioritized statewide by the State Board for Community and Technical Colleges. Requests are compiled and forwarded to the Office of Financial Management (OFM) for consideration by the executive branch. Securing funding from amongst the pool of applying colleges is highly competitive. It is imperative that the SCC Master Plan supports the College's Mission, Core Themes, Vision, and Strategic Goals. It must also receive the support of its service area; the Seattle Colleges District; the City of Seattle; its immediate neighborhoods and their associated community groups; state and local political leaders; and the SCC students and faculty.

When granted, state capital funds are typically appropriated on a biennial basis in odd numbered years by the state legislature. The typical capital construction project development schedule is as follows:

1 st Biennium:	Submit Project Request to SBCTC and State Legislature for consideration
2 nd Biennium:	Pre-Design Funding
3 rd Biennium:	Design Funding
4 th Biennium:	Construction Funding

Therefore, the time from the initial decision to request funding until building occupancy is usually eight years. With a timeline this long, the college would like to maintain as much flexibility as possible in terms of development requirements.

The purpose of this document is to define development guidelines to be utilized in planning the growth of Seattle Central College at the Broadway Edison Campus. The main objectives of this Master Plan are to:

- Structure the long-term growth of the college and develop logical methods and guidelines for its implementation.
- Provide background information for use by the college in application of funding proposals to the State Board of Technical and Community Colleges.
- Maximize the effective utilization of all existing spaces.
- Form the basis of a Major Institution Master Plan (MIMP) submittal to the City of Seattle for their acceptance and approval as well as an updated Master Plan as required by the city zoning code.
- Define the breadth of the SCC Campus within its service district.
- Aid in programmed space allocation.

Master Plan Goals

The primary goals of the master plan are to support the college Mission, Core Themes, Vision, and Strategic Plans through the physical development of its campus. Specific goals include the following:

- SCC's mission and core themes will drive planning decisions.
- The master plan will define an urban community collegiate environment that inspires and educates the campus, community, and region.
- The master plan will address new and renovated facilities and will incorporate the Health Education Center, Wood Technology Center, and Seattle Maritime Academy off-site campuses.
- Campus facilities will be developed in collaboration with other community and technical colleges, K-12 schools, universities, the community, and private industry.
- SCC will need to be entrepreneurial in its approach to capital funding as state resources will continue to decline.

Methodology

Successful master planning projects begin with the Planning Team gaining an understanding of the functions or operations to be performed within the campus. Because of this, the Planning Team began with a series of programming workshops, facility tours, data collection, observations, and active listening. This approach provided the team with valuable insight and direction that otherwise may not have been communicated through more traditional programming and design methods. The information provided and gathered during these sessions is documented herein and is intended to be used as a guide for development of the SCC campus during the coming years.

To define the scope of growth to be incorporated into the Master Plan the following strategies were implemented:

- **Total Need Determination:** The total growth area needed was determined through Space Needs Analysis which looked at quantitative existing campus facilities, their current utilization, programs offered/anticipated, and future growth projections. This data was then analyzed against national community college standards and peer institutions. The resulting space needs program identified total square footage deficiencies and need. Total area of new construction was then calculated and evaluated against the SBCTC's CAM analysis to verify compatibility.
- **Building Development Site Planning:** During workshops with the Facilities Master Plan Committee, the committee discussed the relationships of spaces with their associated programs and services. Appropriate locations on campus for growth and the areas available/required at each location were determined. A series of new capital construction, replacement, and renovation projects were identified such that the projects organizationally supported the campus planning goals. Additional future capital construction projects are also included but not planned at this time.
- **Campus Character and Environs:** The workshop process, with campus constituencies, the Citizen's Advisory Committee, Capitol Hill Community groups, and others, included discussions regarding the physical presentation of the SCC campus to the community and the students it serves. A series of goals were developed as an aid to generating projects that will enhance not only the visual image of the college, but also strengthen the general campus organization and service opportunities resulting in a more user-friendly environment and partnership with the neighborhood.
- **Campus Infrastructure Plans:** Along with new and replacement building developments, associated infrastructure and utility improvements were identified and incorporated into the campus site plan.
- **Internal Renovation Plans:** While not part of the MIMP process or scope, the college realizes that with the completion of any new construction there are prime opportunities to re-organize and renovate existing program and service spaces within the existing campus such that they will better serve the SCC community. This campus re-organization will be extended to include all phases of building development. The Internal Renovation planning will provide a framework for future space allocation and utilization efforts. They also serve as a master plan for determination of existing building renovation projects.

Acknowledgments

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Executive Committee

Dr. Sheila Edwards Lange, President SCC
Lincoln Ferris, Consultant to the President, SCC
David Ernevad, Director of Facilities and Plant Operations
Sam Lunsford, Capital Projects Coordinator

Facilities Master Planning Participants

Kurt Buttleman, Vice Chancellor of Academic and Student Success
Bradley Lane, Vice President of Instruction
Yoshiko Harden, Vice President of Student Services
Wendy Rockhill, Dean of Science, Technology, Engineering & Mathematics
Laura Dizazzo, Dean, Basic & Transitional Studies
Kate Krieg, Dean of Arts, Humanities, and Social Sciences
Chris Sullivan, Dean, Business, Information Technologies
Ricardo Leyva-Puebla, Dean of Student Development
Lynn Kanne, Interim Dean of Instructional Resources
Maureen Shadair - Interim Executive Dean of Seattle Vocational Institute
Naina Eshwar, Executive Director of Strategic Planning, and Institutional Research
Jeff Keever, Director, Auxiliary Services

Real Estate Advisory Council

Kurt Buttleman, Vice Chancellor of Academic and Student Success
Doris Koo, Doris Koo Enterprises
Lee Copeland, Mithun
Mike Malone, Hunters Capital
Jeanette Henderson, University of Washington Real Estate
Barbara Dingfield, The Giving Practice
Amy Worthington, Cha, LLC
Scott Redmond, Sellen Construction

City of Seattle

Maureen Sheehan, Department of Neighborhoods
Nelson Pesigan, Department of Neighborhoods
Carly Guillory, Seattle Department of Construction, and Inspections

MIMP Development Advisory Committee (DAC)

McCaela Daffern – Co-chair
Jacobi Bourdreux – Co-chair
Don Anderson
Adam Behrman
Michael Gilbride
Tori Halligan
Jacqueline Kim
Erica Loynd
Jamie Merriman-Cohen
Brittney Moraski
Michael Seiwerath
Emily Thurston

Design and Consultant Team

Architects - Starling Whitehead & Lux Architects

Stephen Starling

Connor Davidge

Legal – Foster Garvey

Steve Gillespie

SEPA/EIS Consulting - EA Engineering, Science, and Technology

Michele Sarlitto

Kristy Hollinger

Transportation Planning – Transpo Group

Stefanie Herzstein

Kassi Leingang

Development Consulting – Vanir

Larry Bjork

Historic Structures Analysis – The Johnson Partnership

Ellen Miro

Other Consulting Firms

PCS Structural Solutions

DCI Engineers

PBS Environmental

GeoEngineers

True North Surveying

Community Organization (participated through a variety of meetings and site discussions)

Aids Memorial Pathway

Broadway BIA

Capitol Hill EcoDistrict

Capitol Hill Historical Society

Central Seattle Greenways

Community Roots Housing

GenPride

The Northwest School

Intiman Theater

Pike/Pine Urban Neighborhood Council

References

The following reference documents (which are not included in this document) provided valuable information and insight which guided this master plan.

- State of Washington Department of General Administration - Space Allocation Standards Manual 2009
- City of Seattle - Right-of-Way Improvements Manual
- Seattle Design Guidelines 2013
- Capitol Hill Light Rail Station Design Guidelines 2013
- Capitol Hill Neighborhood Design Guidelines 2019 v2
- Pike Pine Neighborhood Design Guidelines 2017 v3
- UW Green Futures Lab – Capital Hill: Public Spaces + Public Life
- Capitol Hill Housing - Capitol Hill EcoDistrict – A proposal for district-scale sustainability 2012
- Seattle Central College – Compiled Major Institution Master Plan, September 2002
- Seattle Central College – Operational Plan 2018-2020
- Seattle Central College – Preliminary Strategic Plan 2016-2020
- Life Between Buildings by Jan Gehl
- NACTO – Urban Street Design Guide
- NACTO – Don't Give Up at the Intersection
- Project for Public Spaces - Lighter Quicker Cheaper (LQC) Placemaking

CHAPTER TWO- MISSION & GUIDING PRINCIPLES

This document provides a set of guiding principles that clearly articulate the values and needs of the Seattle Central College (SCC) campus community with respect to campus planning. All components of the SCC Master Plan will support the accomplishment of the college's mission, values, strategic initiatives, and other guiding principles.

Mission and Values

- Mission** As an open-access learning institution, Seattle Colleges prepares each student for success in life and work, fostering a diverse, engaged, and dynamic community.
- Vision** Seattle Colleges is recognized as an exemplary learning institution that transforms lives, promotes equity, and enriches the community.
- Values**
- **Accessibility** for all learners and partners
 - **Collaboration** through open communication and commitment to working together
 - **Diversity, Inclusion, and Equity** for all individuals, particularly the underserved in the community
 - **Fiscal Sustainability** for long-term viability and excellence in service and operations
 - **Growth and Engagement** of faculty and staff through professional development
 - **Innovation** in instruction, student services, operations, and organizational culture
 - **Integrity** by adhering to the highest standards of ethics and public stewardship
- Strategic Plan** SCC's most recent strategic planning is provided under the umbrella of the Seattle College District Strategic Plan 2017-2023 which established goals as strategies for the three institutions (Seattle Central College, South Seattle College, and North Seattle College). The plan identified the following Goals and Strategies.

STUDENT SUCCESS	
Goal	We strive to improve student satisfaction, retention, completion, and job placement, as well as to narrow student performance gaps.
Strategy 1:	Implement structured academic and career pathways.
Strategy 2:	Practice strategic enrollment management.
EQUITY, DIVERSITY, INCLUSION, AND COMMUNITY	
Goal	We firmly establish equity, diversity, and inclusion as a human right for all. We frame our decisions and actions with this lens and are accountable to the community.
Strategy 1:	Develop and implement a diversity action plan.
ORGANIZATION EXCELLENCE	
Goal	We seek continuous improvement in excellence in teaching and learning, operational efficiency and fiscal sustainability, strategic innovation, and employee growth and engagement.
Strategy 1:	Enhance teaching and learning.
Strategy 2:	Achieve system integration.
Strategy 3:	Foster sustainability.
PARTNERSHIPS	
Goal	We value and invest in strategic and ongoing partnerships with educational, business, governmental, labor, and community organizations.
Strategy 1:	Build Partnerships.

Resolutions and Statements

Resolutions and statement on where Seattle Colleges stands as an intuition in regard to important social issues are available here: [Where We Stand | Seattle Colleges](https://seattlecolleges.edu/where-we-stand) (seattlecolleges.edu/where-we-stand)

Land and Labor Acknowledgements

Seattle Central College acknowledge the indigenous peoples who lived on the land before us through the following land and labor acknowledgment.

Today we recognize and honor the original occupants and stewards of the land where we now gather—lands that are the traditional home of the Coast Salish people, the traditional home of all tribes and bands within the Duwamish, Suquamish, Tulalip, and Muckleshoot nations.

Today We honor the survival, the adaptations, the forced assimilation, and the resilience and creativity of Native peoples—past, present, and future. We encourage you to consider their responsibilities to the people and land, both here and elsewhere, and to stand in solidarity with Native, Indigenous, and First Nations People, and their sovereignty, cultural heritage, and lives.

We also pause to recognize and acknowledge the labor upon which our country, state, and institutions are built.

We remember that our country is built on the labor of enslaved people who were kidnapped and brought to the U.S. from the African continent and recognize the continued contribution of their survivors. We also acknowledge all immigrant labor, including voluntary, involuntary, trafficked, forced, and undocumented peoples who contributed to the building of the country and continue to serve within our labor force. We acknowledge all unpaid care-giving labor.

To the people who contributed this immeasurable work and their descendants, we acknowledge our/their indelible mark on the space in which we gather today. It is our collective responsibility to critically interrogate these histories, to repair harm, and to honor, protect, and sustain this land.

Master Plan Guiding Principles

The following over-arching principles apply to the SCC campus and its off-site facilities and provide a foundation for the remaining principles under each of the subheadings.

1. SCC's Master Plan will integrate with and complement other visioning plans related to the mission, vision, core themes, and strategic planning of the college.
2. SCC's Master Plan will define an urban community collegiate environment that inspires and educates the campus, community, and region through its architecture, landscaping, public art, sustainable design, and energy efficiency.
3. SCC's facilities should become an example to which others turn for information, education, and inspiration.
4. New and renovated facilities will:
 - Alleviate programmatic shortcomings of current facilities.
 - Incorporate plans to meet the future needs of affected departments and programs.
 - Consider the future technology requirements and potential future uses of facilities.
 - Address College-wide plans, such as the WACTC, District and SCC Strategic Plans, Instructional Plan, Core Themes and College Vision Statement; and
 - Maximize the effectiveness of space by transitioning space that is currently under-utilized into space that serves high demand needs.
5. The Master Plan will create an integrated plan in which the individual components are interwoven and coordinated. Master Plan decisions and activities will be coordinated through the Campus Facilities Master Plan Committee, which will establish a system of cross-coordination among the individual elements of the overall plan.
6. All facilities (new and existing) will be adequately maintained and updated to allow programs to remain current.

7. The Campus Facilities Master Plan Committee will evaluate and recommend sequencing of projects in consultation with other campus constituencies. Project sequencing will be coordinated in a manner to optimize access and use of existing facilities, minimize disruption of the campus environment, and achieve institutional goals.
8. The college will coordinate all relevant issues with community, municipal, county, and state agencies.
9. The Campus Facilities Master Plan Committee will review and update these Guiding Principles and the Master Plan at least every five years.
10. These guidelines and principles will be applied through a collaborative process acknowledging that these principles may at times need to be applied with flexibility, such resolutions will:
 - Maintain the integrity of the group principles and guidelines,
 - Be fiscally responsible, and
 - Encourage creative design and problem solving.
11. SCC off-site programs, including the Wood Technology Center, Seattle Maritime Academy, and Health Education Center, will integrate with the main campus and support the mission of Seattle Central College.
12. All students should be able to access facilities and fully participate in learning, formally and informally, in face-to-face formats or with the use of technologies. Special attention should be paid to access and ease of mobility for students with disabilities and special needs.
13. Campus facilities and resources should be developed in collaboration with other community and technical colleges, other education sectors (K-12 and universities), the community, and private industry.
14. Faculty and administrators should have the necessary skills and abilities to maximize the intended use of facilities and instructional resources to respond to needs of students, employers, and communities. This will require change and professional development and training in new uses of facilities, course scheduling, and instructional delivery.
15. Facilities will be sustainable and meet LEED requirements and contribute to sustainable practices related to curriculum and campus culture.
16. Facilities and campus-wide systems should be developed to reduce carbon emissions and reduce greenhouse gas emissions.
17. Design and construction of facilities should give consideration to emergency preparedness and disaster protection as a community resource.

Planning For Sustainability

Environmental concerns, especially climate change, are at the forefront of the global agenda as we better understand the implications of inaction upon our natural, built, and social systems.

Implementation of the Master Plan provides an unparalleled opportunity to transform the campus into a model of sustainability. With a substantive amount of outmoded, energy-inefficient buildings being replaced or remodeled with new modern, energy-efficient facilities, SCC has an opportunity for green building and other sustainability strategies to contribute to the communities it serves. By implementing green design and development on campus, environmental impacts will be reduced through the “greening” of construction and operation of multiple buildings. Incorporating ideas of sustainability into the everyday lives of students, faculty, and staff allows thousands of people to become accustomed to these strategies, and they in turn can incorporate the strategies into their lives outside of the institution.

Seattle Central College recognizes that there are limits to the world’s resources. To ensure the quality of life for future generations, SCC seeks to demonstrate leadership in environmental stewardship and sustainability. The college is committed to conserving resources and reducing the impact that its services and activities place on the environment. Seattle Central College is committed to achieving Leadership in Energy and Environmental Design® (LEED) certification for all new buildings. It will also seek LEED Silver certification, or higher, where practicable.

Statewide initiatives implemented in the last few years will guide much of SCC’s planning as it addresses its Sustainability goals. In particular:

- Executive Order 20-01 State Efficiency and Environmental Performance – which seeks zero energy complaint buildings and operations. It also seeks the use of 100% clean electricity.
- Executive Order 16-07 Building A Modern Work Environment – which seeks to enable a mobile workforce and modern environments resulting in vehicle trip reductions and smaller, space efficient construction to promote flexibility, collaboration, and productivity.
- Executive Order 05-01 Sustainability and Efficiency Goals for State Operations – which requires construction/renovation of building to LEED standards.
- Washington Clean Building Performance Standard – Expansion Law – which subjects all college owned building in excess of 20,000 square feet to reduce energy usage through use of energy management planning and monitoring.

Some examples of how SCC is addressing operational issues include increasing efficiencies in heating and cooling systems by replacing old systems with new clean energy systems, installing high-efficiency water and lighting fixtures, reusing existing buildings, maximizing daylight within buildings, and installing raingardens to manage stormwater on site.

Transportation plays a major role in climate change, and Seattle Central College recognizes the need to address this concern directly through several initiatives, including increasing the number of students living on campus, contributing to vibrant pedestrian-oriented development, and encouraging fewer personal vehicle trips. A Transportation Management Plan is currently in place and will be revisited as part of the pending MIMP application. It identifies strategies to reduce single-occupancy vehicle travel. In addition, parking and traffic studies will also be prepared to analyze potential traffic and parking impacts.

This Master Plan is an effective vehicle to encourage sustainable campus development by addressing potential regulatory barriers to the implementation of appropriate strategies that will allow for the integration of emerging best practices in design and operation with the regulatory purpose and intent of the Major Institution Overlay code.

CHAPTER THREE – CAMPUS GROWTH AND EXPANSION

The section defines anticipated development needs for Seattle Central College. Need was defined via an academic and space planning process provided by the College's Executive team with assistance from the campus Educational Leadership Team. The development elements and boundaries were reviewed and approved by the Board of Trustees.

Background As part of academic and space planning, an academic visioning process was conducted to assist in the development of program, growth, and enrollment goals for the college. The space-planning component of the process used the information gathered during the academic planning effort to review academic space utilization and to project future space needs to support the physical planning recommendations for master plan development.

The process was both comprehensive and collaborative. The Starling Whitehead & Lux Architects team assessed the status of planning and worked with leaders at SCC to verify and validate academic/administrative/service objectives for the future. They facilitated the analysis of existing data pertaining to demographics, programs, enrollments, and facilities. The analysis considered community needs and workforce requirements, as well as recent enrollment trends. SCC's Institutional Research provided pertinent base data and participated actively in the planning process.

Key elements of the process included:

- Articulating future academic objectives to create a proper vision for the college
- Reviewing enrollment projections for the service area for the next ten years based on demographic data and the impact of light rail transit east of access to the campus
- Making recommendations regarding academic changes that will be required to address enrollment and workforce needs
- Review of classroom and laboratory utilization analysis to assess the efficiency and effectiveness of physical academic resources
- Conducting a space needs analysis using guidelines which are applicable to Seattle Central College and supplemented by the experience of the consultant in those areas where specific needs may not be directly addressed by guidelines
- Preparing academic planning information and a space needs analysis report for the Campus Master Plan that combines the key findings from the above analysis

To accomplish the process elements above, the following tasks were performed:

- Project Initiation and Data Collection
- User Group Meetings
- Space Utilization Analysis at the Base Year
- Space Needs Analysis at Base Year and Future Year Enrollment
- Facilitation of Academic Visioning Session
- Presentation and Final Documentation

Master Plan Concept

Proposed Campus

The Master Plan articulates how the physical campus form impacts some of the most important issues and goals that support the college's mission, vision, and values. The physical design contributes to the vitality of "place" by providing students with a sense of belonging and community. The combination of formal and informal spaces allows for interaction and the achievement of academic goals. Specific improvements include a strengthened pedestrian network and a purposeful extension of main pedestrian pathways to the Broadway Business District, the Pike/Pine neighborhood, and to Cal Anderson Park. This network will be improved with pedestrian amenities (benches, bike racks, lighting) and landscaping. The physical campus is enhanced by improvements to entry points and improved wayfinding that reflects the college's desire for an open and accessible campus. This, in turn, will increase the presence and visibility of the college within the immediate community and the City of Seattle.

SCC seeks to provide development in congruence with neighborhood development planning

completed by the Capitol Hill community. This includes planned development per the Seattle Design Guidelines, Capitol Hill Neighborhood Design Guidelines, Capitol Hill Light Rail Station Design Guidelines, and the Pike Pine Neighborhood Design Guidelines. SCC staff and administration participated in these and other neighborhood planning exercises.

The Master Plan also provides multiple options to meet current and future needs for academic space, student services, support space, and college-related community services, creating a framework that is flexible enough to meet the college's evolving needs. Seattle Central College is committed to contributing to a healthy campus and environment by incorporating sustainable strategies in all aspects of site and building design, construction, maintenance, and operation. Several primary sustainability principles have been identified:

- Comprehensively and creatively incorporate sustainable design approaches into the design of all physical campus elements and systems.
- Harmonize the human-built environment with natural systems and processes in such a way that non-renewable natural resources are conserved, and that the natural environment maintains its capacity for healthy growth and regeneration.
- Make sustainable features visible and available as learning and teaching opportunities.
- Endeavor to build structures for permanence, quality, and flexibility.
- Design new and renovation projects to meet or exceed LEED silver standards for green buildings.

Alternatives and Decentralization Options

It is important to note that key to success for many SCC students is:

- Ease of access – Most students rely on public transportation to travel between home, work, and school.
- Access to social, human, and educational services – SCC provides extensive wrap-around services at its main campus.
- Affordability – Keeping the cost of education affordable is an ongoing challenge for SCC. Effective use of operation and capital funding helps keep the cost of tuition down and therefore access to more students.

Development options to accommodate the college's growth in the Capitol Hill neighborhood are difficult and expensive due to limited sites available for acquisition and development; therefore, the college has investigated other options to meet the expected total campus FTE growth, and these are discussed below. It is the college's intent to develop this MIMP with flexibility to adapt to changes in program and enrollment needs without major amendments.

Decentralize Facilities by Expansion at Existing Satellite Sites

Seattle Central College has three existing satellite campuses. Collectively these campuses total approximately 20% of the College's gross square feet of space. Each has been developed to serve unique academic programs where both instruction and related services can be narrow and targeting.

Health Education Center (HEC)

The Health Education Center is located within the historic Pacific Tower on Beacon Hill. The HEC supports a wide range of healthcare related programs, many of which are tied to training opportunities with partner programs in the Pacific Tower complex. The College occupies five floors which total approximately 94,000 gross square feet of space.

- The HEC location is within a larger campus that is operated by the state as the Pacific Hospital Development Authority. As such, any expansion would be limited to other spaces within the complex being vacated; therefore, substantive expansion is very difficult.
- Any program growth in areas outside the current programs would require substantive duplication of college administrative and student services. This would bring unnecessary capital and operational costs to the institution with limited student benefit.
- Access to public transportation includes only a few bus routes, making access a barrier for many students.

Wood Technology Center (WTC)

Located in the Squire Park neighborhood, the WTC provides programs in Cabinetmaking and Architectural Woodworking, Carpentry, Boat Building and Repair, and Pre-Apprenticeship Construction Training. The WTC is largely a new complex completed in 2014. It consists of approximately 67,500 gross square feet of industrial education space and site needs.

- The existing site is fully built out with construction that maximized the existing land-use regulations. Any further development would need new site acquisitions.
- The WTC is in a predominantly residential neighborhood (an area of new multi-family and existing single-family housing). Substantive site acquisition would be very difficult to achieve in a timely manner unless the college sought to assert imminent domain.
- Any program growth in areas outside the current programs would require substantive duplication of college administrative and student services. This would bring unnecessary capital and operational costs to the institution with limited student benefit.
- Access to public transportation includes only a few bus routes and parking is limited.

Seattle Maritime Academy (SMA)

A full campus revitalization of the SMA was completed in 2017. Located on the Ship Canal adjacent to the Ballard Bridge and Fisherman's Terminal, the SMA supports the maritime industry with instruction in Marine Deck and Marine Engineering Technology. SMA consists of two buildings totaling around 31,000 gross square feet.

- The existing site is fully developed to existing land-use regulations. Any further development would need new site acquisitions.
- The SMA is in one of Seattle's limited industrial zones with waterfront access. The scarcity of this type of land within the city would make land acquisition very difficult.
- Existing land use codes restrict development in the area of non-maritime related business and industrial development. College growth in other than maritime fields would be counter to land use goals of the area.
- Any program growth in areas outside the current programs would require substantive duplication of college administrative and student services. This would bring unnecessary capital and operational costs to the institution with limited student benefit.
- Access to public transportation includes only a few bus routes and parking, other than street available parking, does not exist.

Change Breadth of Programs Offered

If expansion or re-vitalization of campus is not permitted, the college will need to reduce program offerings to increase capacity in other higher demand programs. This would mean increasing specialization education and a reduction in offering for Basic Skills instruction. This is not a viable option considering the College's Mission.

Alternative to Do Nothing

To do nothing would mean the college could not accommodate expansion of enrollment. Considering that the state community college system is a large supplier of education for workforce needs, college transfer, and basic skills education, any loss of access would be a detriment to state and city residents. Further, college enrollment spikes during times of economic recession and the lack of ability to address enrollment at times of the greatest need further hurts our communities and, in particular, those from disadvantaged situations common to many seeking an education at Seattle Central College.

Street Vacation Seattle Central College is not seeking any street vacations as part of the MIMP.

Site Disposition SCC property is owned by the State of Washington, as such, If the above efforts do not come to fruition, and the college elects to proceed with the sale/disposition of any parcels or buildings, it must be done in compliance with House Bill (HB) 2382. A summary of the bill is as follows:

Disposal of Surplus Property for Public Benefit

Any state or local agency with authority to dispose of surplus property may transfer property to any public, private, or nongovernmental body on any terms agreeable to the parties, including a no-cost transfer, if the property is used for a public benefit. Public benefit means affordable housing development, or related facilities, for households at or below 80 percent of the local adjusted median income. Such a transfer must include a requirement that the property will be used for a designated public benefit, as well as remedies if the property is not used for the designated purpose. Each government entity using the authority to dispose of public property must enact rules.

The authority to dispose of surplus property for public benefit is discretionary and may be used as an alternative to existing authority, but does not apply to state forest lands, common school lands, or other lands subject to legal restrictions. Such authority is expressly added as an alternative to the authority of the WSP and city governments to sell surplus property at fair market value. The disposal of surplus lands for public benefit is deemed a lawful purpose for any state or local agency that keeps accounts on an enterprise fund, and must be consistent with any applicable, local comprehensive plan. The disposal of property for public benefit is expressly exempted from the local government accounting requirement that intra-agency property transfers must be paid for at true and full value.

To consolidate campus facilities to the greatest extent reasonable and to remove older buildings which under-perform as academic spaces, SCC recently completed the following site/building dispositions:

Broadway Café and Atlas Building

The college recently negotiated the release of these parcels to Community Roots Housing (CRH). As part of the agreement, CRH has agreed to develop Pride Place, a LGBTQ affordable housing development in partnership with leaders from LGBTQ and health organizations. GenPride, a nonprofit organization, will provide services for an LGBTQ-focused senior community and health center.

South Annex and International Programs buildings

The college recently negotiated the release of these parcels to Community Roots Housing (CRH). As part of the agreement, CRH has agreed to develop Youth Care – South Annex. The development, in collaboration with Youth Care, will include an employment and education academy and up to 87 affordable apartments, including housing for homeless youth and units affordable to people making 30% to 50% of the Area Median Income.

Site Acquisition

Sound Transit Site D

Seattle Central College has recently acquired a 10,383 square foot parcel from Sound Transit located immediately south of the Capitol Hill Station's southwest entrance.

Presbyterian Church Properties

If they become available, the college seeks to acquire parcels west of Harvard Avenue currently owned by Presbyterian Church for future, long term space needs.

Boylston Properties

If they become available, the college seeks to acquire parcels south of E Olive Street and west of Harvard Avenue for future, long-term space needs.


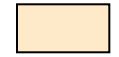




Existing Campus Development

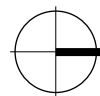
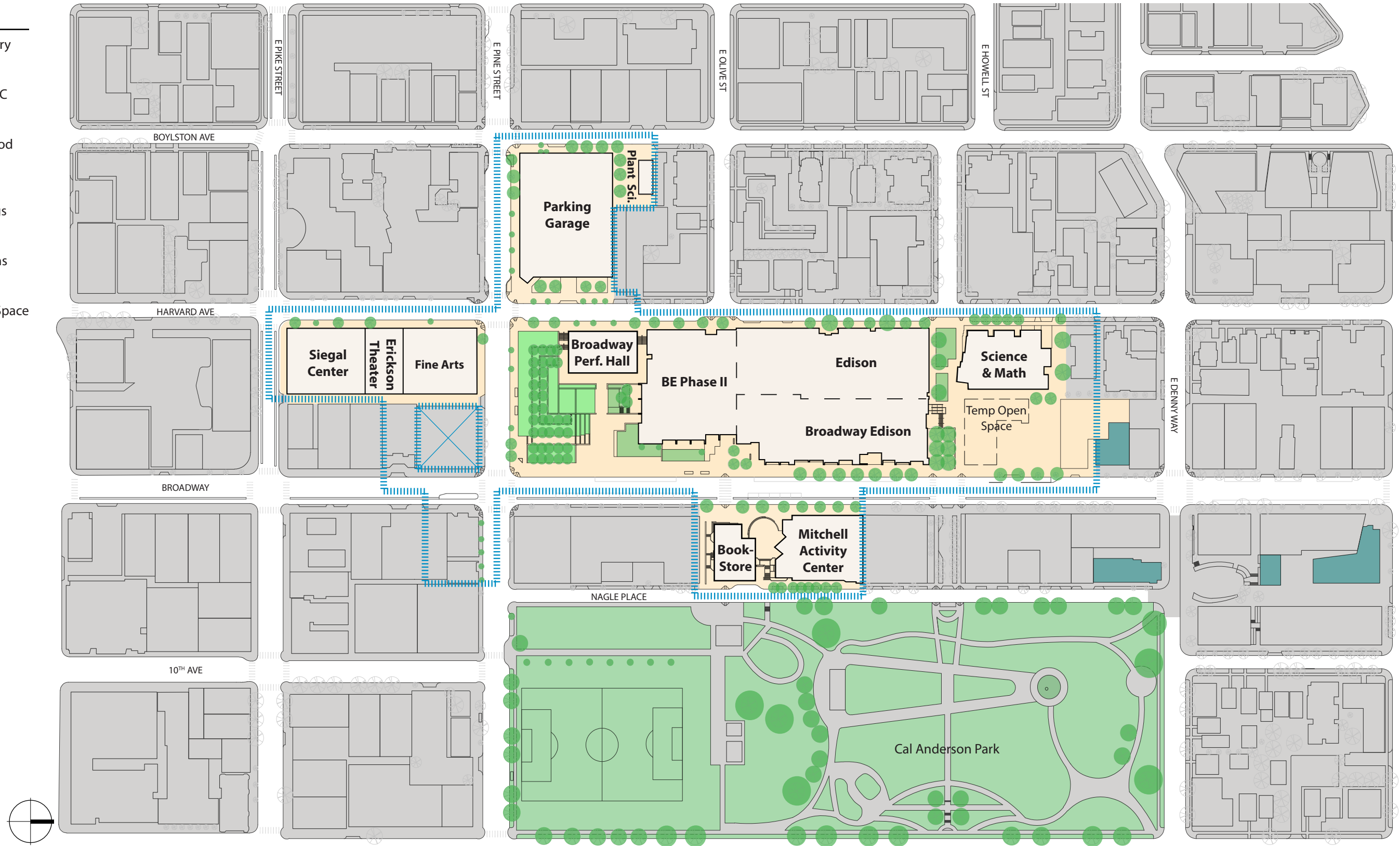
The existing campus site plan - *See Figure 3-1.1 – Existing Campus and Major Institution Overlay (MIO) District* shows all existing buildings owned by Seattle Central College. This totals 12 buildings and represents a combined 745,036 gross square feet:

Building	Gross Square Feet (GSF)
Edison Technical Building	130,527
BE Phase I	175,568
BE Phase II	125,863
Broadway Performance Hall	41,174
Science and Math (parking area excluded)	69,159
Mitchell Activity Center	65,921
Bookstore/Student Leadership Building	13,594
Plant Sciences Lab	2,378
Siegal Center	43,774
Erickson Theater	7,973
Fine Arts Building	66,814
Parking Garage (parking area excluded)	2,291
Total Existing Development	745,036

FIGURE 3-1.1 – EXISTING CAMPUS AND MAJOR INSTITUTION OVERLAY (MIO) DISTRICT

LEGEND

-  Existing MIO Boundary
-  Parcels Owned by SCC
-  Existing Neighborhood Buildings
-  Existing SCC Buildings
-  Sound Transit Stations
-  Park & Public Green Space



Planned Project Development

The City of Seattle Major Institution Land Use and Zoning code defines Planned Projects as development which the Major Institution has definite plans to construct. The Planned Projects shown on the following pages are projects that SCC is expected to complete in the next 10-15 years. These projects will address the following college needs:

- Expand campus resources for instruction and student support to serve enrollment expansion (to 85% of state identified space needs)
- Add student housing to increase economic accessibility, support student retention and completion, and support international student enrollment
- Strengthen academic core of campus with state-of-the-art instructional facilities dedicated to high demand fields where enrollment is expected to spike
- Re-envision student services to increase effective delivery of support
- Reallocate space in underutilized facilities to maximize their effective use
- Secure state-provided capital funding for renovation of ineffective space
- Enhance student instruction in ABE (Adult Basic Education), ESL (English as a Second Language), etc.
- Expand the College Library
- Create a large gathering and meeting space for campus community events
- Provide student life facilities and amenities
- Create a student/community arrival gateway
- Enhance pedestrian movement through and around campus
- Create a safe campus environment for students and the community

The scope of these projects is limited to development of currently owned parcels and those currently in the process of acquisition (Sound Transit Site D). The development indicated below equates to approximately 77,872 new assignable square feet of space. This approximates 85% of the need identified by a Spaces Needs Analysis for the target enrollment of 7,508 per the SBCTC Capital Asset Model (CAM). **See Figure 3-1.2 – Planned Project Development** for graphic depiction of the following projects.

Planned Projects Summary

The following projects total an **additional 352,792 gross square feet** of space added to campus (excluding parking structures):

Project	Replacement GSF	Renovation GSF	Growth GSF	Change to ASF (CAM)
Student Housing	4,669	-	180,386	-3,636
ITEC	-	-	140,000	45,000*
Broadway Achievement Center	-	41,174	2,406	18,508
Student Union	-	20,000**	30,000	18,000
TOTALS	4,669	61,174	352,792	77,872







* 50% of the Information Technology Education Center space is for the SCC and 50% for Partners. Partner space is excluded from CAM ASF.

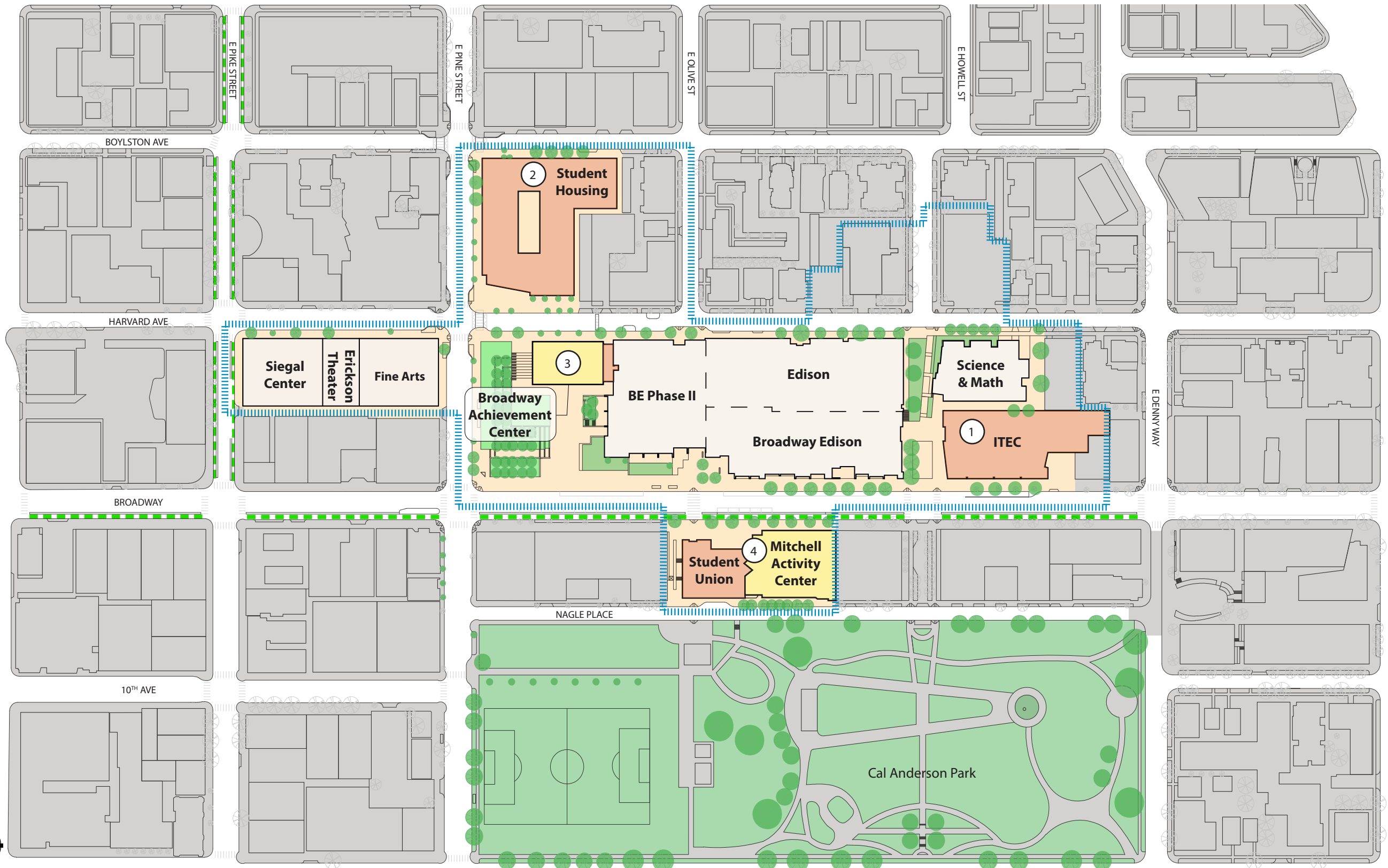
** This includes space in the Mitchell Activity Center (MAC) that will be renovated in this project.

FIGURE 3-1.2 – PLANNED PROJECT DEVELOPMENT

LEGEND

- 1 **Information Technology Education Center - ITEC**
New construction for Student Services and Academic programs. Expected to included space for industry partners.
- 2 **Student Housing**
New construction of student housing (500 beds +/-) above a renovated Parking Garage.
- 3 **Broadway Achievement Center**
Renovation of the existing Broadway Performance Hall for expansion of Library, Student Support, and Gathering Spaces.
- 4 **Student Union**
Renovation and expansion of the existing MAC/Bookstore for creation of a new combined Student Union.

-  SCC Existing Building
-  New Construction/Expansion
-  Renovation
-  Neighborhood Building
-  Park & Public Green Space
-  Existing Protected Bike Lane



Planned Projects

Information Technology Education Center (ITEC)

This project is planned as a major new academic building located on the site of the former North Plaza Building and the acquired Sound Transit Site D. The project scope is envisioned as a six-story structure consisting of three floors of college uses (anticipated to be Student Services, technology classrooms and labs, and general instructional space) and three floors of leased space to college-related partners (District Offices, Industry Organizations, Partnership Companies, etc.). In addition, the project will include District Energy Plant components and underground parking. A direct underground connection to the existing West Portal of the Capitol Hill Link station will be studied in the early design and planning phases of this project.

Requirements for effective learning environments have evolved and today require increased floor-to-floor heights (to accommodate use of indirect lighting, penetration of daylighting deeper into buildings, and mechanical ventilation requirements, etc.). This is particularly true for instruction in technical and STEM-related fields of study which require complex mechanical, electrical, and plumbing systems to support unique laboratory environments.

Building Height	95 feet	
Vehicle Parking Stalls	198	
Bicycle Parking		
Long-Term Bicycle Spaces	25 minimum	
Short-Term Bicycle Spaces	25 minimum	
Project Gross Square Feet		
	Parking Structure	= 62,224
	College Academic Space	=70,000
	<u>College Partner Space</u>	<u>=70,000</u>
		Total = 202,224 gross square feet
Net Added Campus Gross Square Feet	140,000 gross square feet	
	Excludes below grade parking structure	

Student Housing

When students live on campus, it increases opportunities for meaningful interaction with other students as well as college staff and faculty. It also affords students full participation in the social experience of college life. Students living on campus spend less money and time on transportation and are immersed in the campus culture. This is critical for student success and retention. Reduced driving by students also decreases demand for fossil fuel consumption, which in turn reduces the college's carbon footprint. Reduced driving also decreases demand for parking on neighborhood streets.

The college seeks to develop a 500 bed (+/-) student housing complex above a re-built parking garage on the site of the existing campus parking structure. This project includes providing public-facing retail spaces along the street frontages of E Pine Street and Boylston Avenue.

Building Height	90 feet	
Vehicle Parking Stalls	Existing garage includes 510 parking stalls. As a result of the project, the revised garage will include 261 stalls -- a net loss of 249 stalls.	
Bicycle Parking		
Long-Term Bicycle Spaces	375 minimum	
Short-Term Bicycle Spaces	15 minimum	
Project Gross Square Feet		
	Parking Structure	= 122,573
	Retail/Amenities tied to College mission	= 6,055
	<u>Student Housing</u>	<u>=179,000</u>
		Total = 307,628 gross square feet
Net Added Campus Gross Square Feet	185,055 gross square feet	
	(Excluding the demolished Greenhouse, parking structures, and the existing retail space in the existing parking garage)	

Broadway Achievement Center (formerly the Broadway Performance Hall)

The proposed Broadway Achievement Center (BAC) project will fully renovate the existing Broadway Performance Hall as a revitalized facility serving the college with Basic Skills instruction spaces, a Library expansion, and a new campus Auditorium. Added space will be limited to a new connection to the existing Broadway Edison Complex.

Building Height	All construction will be contained below the existing BPH roofline	
Vehicle Parking Stalls	None exist, and none proposed	
Bicycle Parking		
Long-Term Bicycle Spaces	20 minimum	
Short-Term Bicycle Spaces	20 minimum	
Project Gross Square Feet		
	Renovation	=41,174
	<u>New connection to BE Complex</u>	<u>= 2,406</u>
		Total = 43,580 gross square feet
Net Added Campus Gross Square Feet	2,406 gross square feet	

Student Union (formerly the College Bookstore)

The college intends renovation/expansion of the existing Mitchell Activity Center (MAC)/Student Leadership Building (SLB) complex. Limited renovations are expected in the MAC. The SLB will be fully renovated and expanded with potentially an additional floor. The resulting complex will create a new Student Union with space for student life, fitness, and wellness functions.

Building Height	60 feet	
Vehicle Parking Stalls	No existing and none proposed	
Bicycle Parking		
Long-Term Bicycle Spaces	36 minimum	
Short-Term Bicycle Spaces	18 minimum	
Project Gross Square Feet	Renovation	=20,000
	SLB Addition	= 30,000
		Total = 50,000 gross square feet
Net Added Campus Gross Square Feet	30,000 gross square feet	

Potential Project Development

The Potential Projects depicted on the following pages show campus development more than 15 years from now. The purpose of these projects is to provide Seattle Central College with development flexibility to serve unexpected needs not currently envisioned. Pending available funding and successful site acquisition, the College seeks to complete the following Potential Projects that could address the following:

- Expand campus resources for instruction and student support to serve full enrollment of 7,508 (to 100% of state identified space needs)
- Provide additional housing for students or college staff to support economic accessibility, support student/staff retention
- Secure state-provided capital funding for campus expansion to support growth needs
- Strengthen academic core of campus with state-of-the-art instructional facilities dedicated to high demand fields where enrollment is expected to spike
- Strengthen student services to increase effective delivery of support
- Develop and strengthen pedestrian movement through and along campus edges at Harvard and East Howell streets

See Figure 3-1.3 – Potential Project Development for graphic depiction of the following projects.

Potential Projects Summary

The following projects total an additional 115,000 gross square feet of space added to campus:

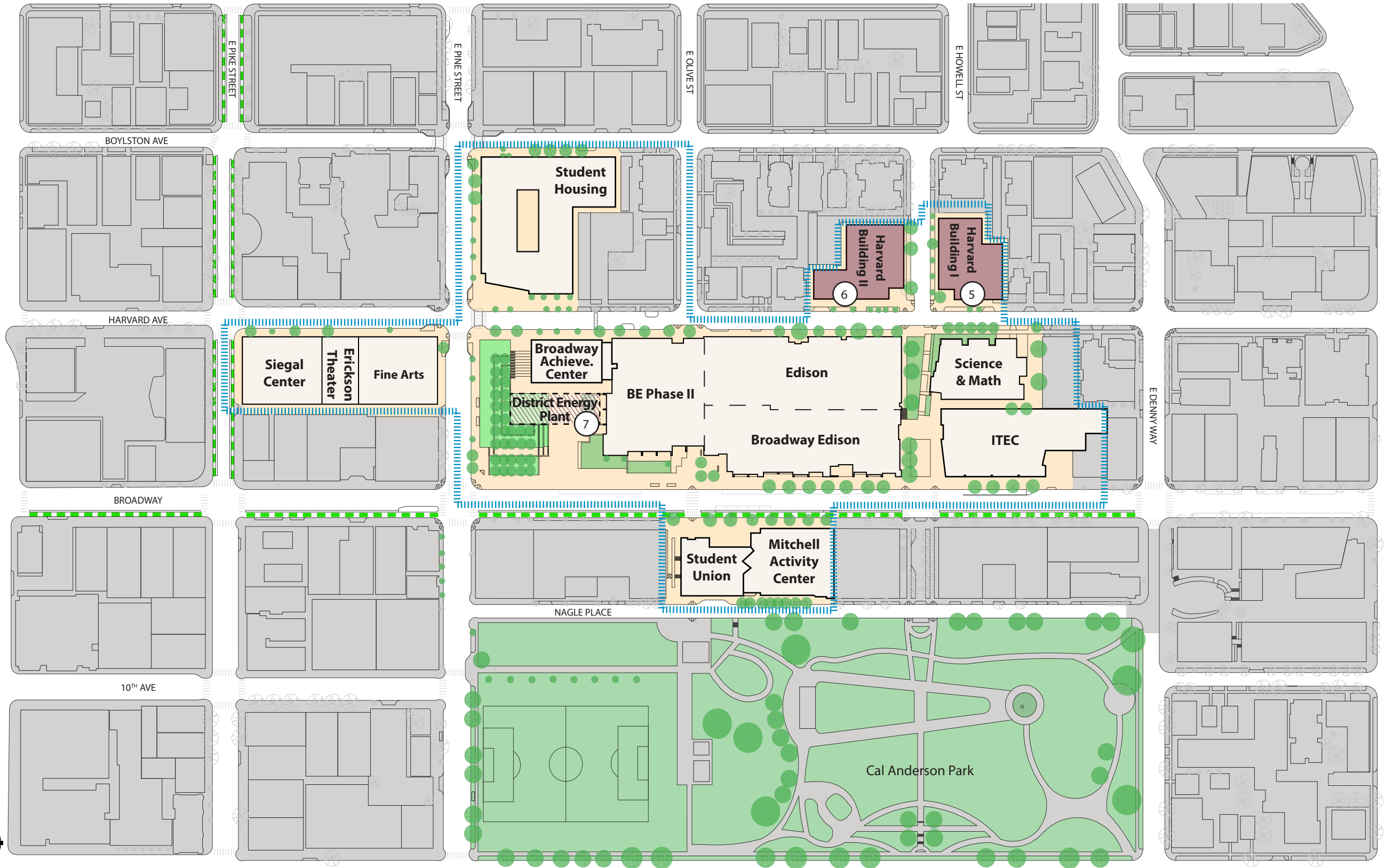
Project	Replacement GSF	Renovation GSF	Growth GSF	Change to ASF (CAM)
Harvard Building I	-	-	50,000	30,000
Harvard Building II	-	-	50,000	30,000
District Energy Plant	-	-	15,000	0
TOTALS	-	-	115,000	60,000

FIGURE 3-1.3 – POTENTIAL PROJECT DEVELOPMENT

LEGEND

- 5 **Harvard Building I**
New construction for academic spaces and college administrative functions.
- 6 **Harvard Building II**
New construction for academic spaces and college administrative functions.
- 7 **District Energy Plant**
New construction of an underground, low-carbon thermal energy plant.

- SCC Existing Building
- New Construction/Expansion
- Renovation
- Neighborhood Building
- Park & Public Green Space
- Existing Protected Bike Lane



Potential Projects

Harvard Building I

A new four-story, 50,000 gsf building for campus space needs. This building will be located on the site of an existing parking lot. SCC does not own the parcel for the project. Specific programs for this location have not yet been identified.

Building Height	80 feet
Vehicle Parking Stalls	None
Bicycle Parking	To be Determined
Net Added Campus Gross Square Feet	50,000 gross square feet

Harvard Building II

A new four-story, 50,000 gsf building for future campus space needs. This project is planned for the existing Presbyterian Church parcel. SCC does not own the parcel for the project. Specific programs for this location have not yet been identified.

Building Height	80 feet
Vehicle Parking Stalls	None
Bicycle Parking	To be Determined
Net Added Campus Gross Square Feet	50,000 gross square feet

District Energy Plant

A new below-grade District Energy Plant of up to 15,000 gross square feet may be proposed to meet campus energy needs. The project will occur if the college is able to secure a funding source that will permit the conversion of existing campus energy systems (mechanical and electrical) to a more sustainable and efficient central utility system. If there is need and funding, the District Energy Plant may also be able to offer services to the surrounding community. This project is planned to be located below the South Plaza. The project is not anticipated to impact the use of the South Plaza open space.

Building Height	30 feet below grade (of the existing south plaza) Limited above grade building elements may be required. (Stair access, air intake and exhaust, etc.)
Vehicle Parking Stalls	None
Bicycle Parking	None
Net Added Campus Gross Square Feet	15,000 gross square feet

Access to Campus

Access to campus is provided by a variety of sources. The following mode splits are as reported by the 2019 survey provided as part of SCC's Transportation Management Program.

	Public Transit	Bike/Walk	Automobile	Other
Students	66%	12%	19%	3%
Staff	44%	7%	43%	6%

SCC proposes improvements to campus arrival points as part Planned projects. Proposed improvements are noted below. A more detailed explanation of the design approach to each location is included in the Design Guidelines section of Chapter 4.

Pedestrian Access

Pedestrian access to the campus occurs along all campus boundaries but is particularly heavy at the north end due to the Capitol Hill Sound Transit Station and at the south end due to the SCC-owned Harvard Parking Garage at Harvard and E Pine; bus stops outside the Egyptian Theater on E Pine between Harvard and Broadway; bus stops along Broadway; and from the residential neighborhood west of campus.

The proposed plan calls for maintaining all existing campus access points and embracing the Sound Transit and Seattle Streetcar stations with District Gateway Enhancements. This enhanced access will be at the northern end of campus via the Sound Transit station at Broadway and E Denny Way, and the adjacent streetcar stop at Broadway between E Howell Street and E Denny Way. This location will be marked by a new gateway development. (See below)

Pedestrian Improvements

In addition to the E Pine and Harvard Plaza at Student Housing and the District Gateway Enhancements on Broadway between E Howell and E Denny noted above, the following projects will include improvement to major pedestrian pathways that serve campus.

Student Housing:

Pedestrian improvements will be provided along E Pine Street between Boylston Ave and Harvard Ave, culminating at the E Pine and Harvard Plaza. Improvements will also be included at Boylston and Harvard. The college will work future design teams and SDOT (along with other City of Seattle jurisdictions) to develop and implement traffic calming measures on Harvard Ave between E Pine Street and E Olive Street. Measures may include raised crosswalks, chicanes, bollards, and/or cobblestones or textured paving.

ITEC Building:

Pedestrian improvement along Broadway between the Sound Transit Station and the E Howell Street Passageway will be provided. These improvements will link to the Seattle Streetcar station. Additional open space improvements will be made to the east half of the E Howell Street Passageway and, if funding allows improvements to the west half.

See Chapter Four, Section 4 – Design Guidelines, and Chapter Five – Campus and Community Context for more information on pedestrian improvements.

Vehicular Access

There is currently one primary vehicular arrival point on campus – the SCC Harvard Parking Garage at the corner of Harvard and E Pine. Other parking areas on campus are limited and do not constitute a significant amount of traffic or arrivals.

As SCC is an urban campus amid the vibrant Capitol Hill community, access to and through campus is largely by the greater Seattle community at large. Primary vehicle traffic occurs along Broadway and E Pine Streets. Harvard traffic is largely limited to localized neighborhood use, campus ADA parking, and campus services loading zones.

The proposed plan calls for moving vehicle access to the remodeled Harvard Parking Garage from Harvard Ave to Boylston Ave. This change will decrease the quantity of cars using Harvard and improve pedestrian safety (along with other streetscape and frontage improvements noted in this MIMP). Planned parking to be included with the ITEC Building will be accessed from Harvard Ave near E Howell Street via a connection through the Science & Math garage.

Vehicular Improvements

There are no significant improvements proposed for those arriving by car. The parking garage associated with the ITEC building will include visitor parking that may provide direct access to the building's first floor which is anticipated to include services for those visiting campus for the purposes of enrollment. The design of the parking facilities will be further developed when the design of ITEC begins.

Service and Delivery Access

The College has centralized major service functions – deliveries, waste collection, loading berths – for campus in the existing Edison building. These services are accessed from Harvard Ave, just north of the E Olive St intersection.

Service and Delivery Access Improvements

There are no significant improvements proposed for central service and delivery access. As individual buildings are developed, service access will be included as necessary.

Vehicle Parking

Seattle Central College currently provides 608 parking stalls. The primary location is the college Parking Garage which includes 510 total stalls. The remainder are interspersed around campus in a variety of surface lots and garages – **See Figure 3-1.4 – Existing Transportation and Vehicle Parking.**

The proposed plan intends to lower the existing capacity from 608 down to 494. The existing vehicle parking at the SAM garage will remain as is (35 stalls). The remainder will be split into two principal locations, one at each end of campus. The existing parking garage will be reconfigured as part of the planned Student Housing project. As a result, parking stalls will be reduced by around 50%. This reduction will be offset by a new below-grade parking structure at the north end of campus as part of the planned ITEC project.

While anticipated enrollment growth will bring additional people to campus, the objective of the MIMP will be to meet parking demand by increasing the number of resident students; decrease commuter student reliance on single occupant vehicles; and increase student use of Sound Transit through an effective Transportation Management Program (TMP).

Vehicle Parking Improvements

The planned growth included in this MIMP anticipates up to 7500 student FTE's. An objective of the master plan is to maximize alternative transportation uses other than by single occupant vehicle. SCC's Transportation Management Plan incorporates significant efforts to reduce parking needs on campus through incentives to faculty, staff, and student to use other options.

The Transportation Management Plan (TMP) will address in detail the measures to be implemented. In lieu of parking and access requirements established by the underlying zones, the TMP, approved as part of the Major Institution Master Plan, will establish parking requirement.

The Seattle Community College campus currently provides approximately 608 parking stalls. This MIMP proposes a reconfiguration of parking locations and a total reduction of 114 stalls to a new total of 494. This represents a proposed maximum parking supply. The College will confirm specific parking needs when project planning and design commence and may reduce parking stall counts where feasible. New parking facilities built under this MIMP will provide electric vehicle charging infrastructure in accordance with WAC 51-50-0429. **See Figure 3-1.5 – Proposed Transportation and Vehicle Parking.**

Location	Existing	Proposed	Access/Use/Changes
SCC Garage	510	-	The existing garage will be removed and replaced with a garage in the Student Housing/garage project.
Planned Project Student Housing	-	261	A partially-below grade garage. Will include parking for electric vehicle charging, accessibility, and carpool. Access will be from Boylston Avenue. Garage may also be available for public during non-peak times.
North Plaza Lot	37	-	Site will be redeveloped for the ITEC Building/garage.
Planned Project ITEC Building	-	198	A below grade garage. Will include parking for visitors, electric vehicle charging, accessibility, and carpool. If technically viable, access will be from Harvard Avenue via a connection through the existing SAM garage. Garage may also be available for public during non-peak times
SAM Garage	35	35	Access is from Harvard Avenue. Faculty and staff parking
South Annex Lot	26	0	This site was transferred to Community Roots Housing
TOTAL	608	494	A net reduction of 114 parking stalls.

Transit

The Seattle Central Campus is well served by public transit. It has direct connections to the Sound Transit, King County Metro, and the Seattle Streetcar systems. The proposed MIMP does not propose any transit access improvements.

Sound Transit's Link Capitol Hill station makes campus easily accessible from Angle Lake in the south, to Northgate in the north. In the next few years, system expansions will extend to Federal Way in the south, Lynnwood in the North, and east to Bellevue and Redmond. SCC is directly linked to the University of Washington with stations (Husky Stadium and U District).

KC Metro serves the campus with eight different bus lines. Bus stops for these lines are within two blocks of SCC's main building entrance.

The Seattle Streetcar links the campus with Downtown, several south neighborhoods, and First Hill (Pioneer Square, Japantown, Chinatown, Little Saigon, Yesler Terrace and First Hill). It also connects the College with three other Major Institutions; Seattle University, Harborview Medical Center, and Swedish Medical Center First Hill

Proposed Improvements for those arriving by Transit

As part of the college's development program, the following improvements which will support those who come to campus by Public Transit will include:

E Pine and Harvard Plaza:

As part of the Student Housing project a proposed pedestrian crossing and new plaza development will be developed at E Pine and Harvard. The sidewalk area adjacent to the KC Metro stop on the north side of E Pine between Broadway and Harvard will be improved and integrated into plaza development.

Campus Entry Plaza:

As part of the ITEC building's development, the College will create a new entry plaza immediately adjacent to the Sound Transit station. The plaza will serve as a major new entry gateway to the campus both externally and as entry point to the building and campus internal circulation network.

FIGURE 3-1.4 – EXISTING TRANSPORTATION AND VEHICLE PARKING

LEGEND

- 1 Parking Garage (Harvard)
510 vehicle spaces
- 2 SAM Garage
35 vehicle spaces
- 3 North Plaza Lot
37 vehicle spaces
- 4 South Annex Lot
26 vehicle spaces
(transferred to Community Roots in 2020)

- Proposed MIO Boundary
- Parcels Owned by SCC
- Existing SCC Buildings
- Existing Neighborhood Buildings
- Existing Protected Bike Lane
- Sound Transit Stations
- ⋮ Bus Station
- ⋮ Streetcar Station
- Park & Public Green Space

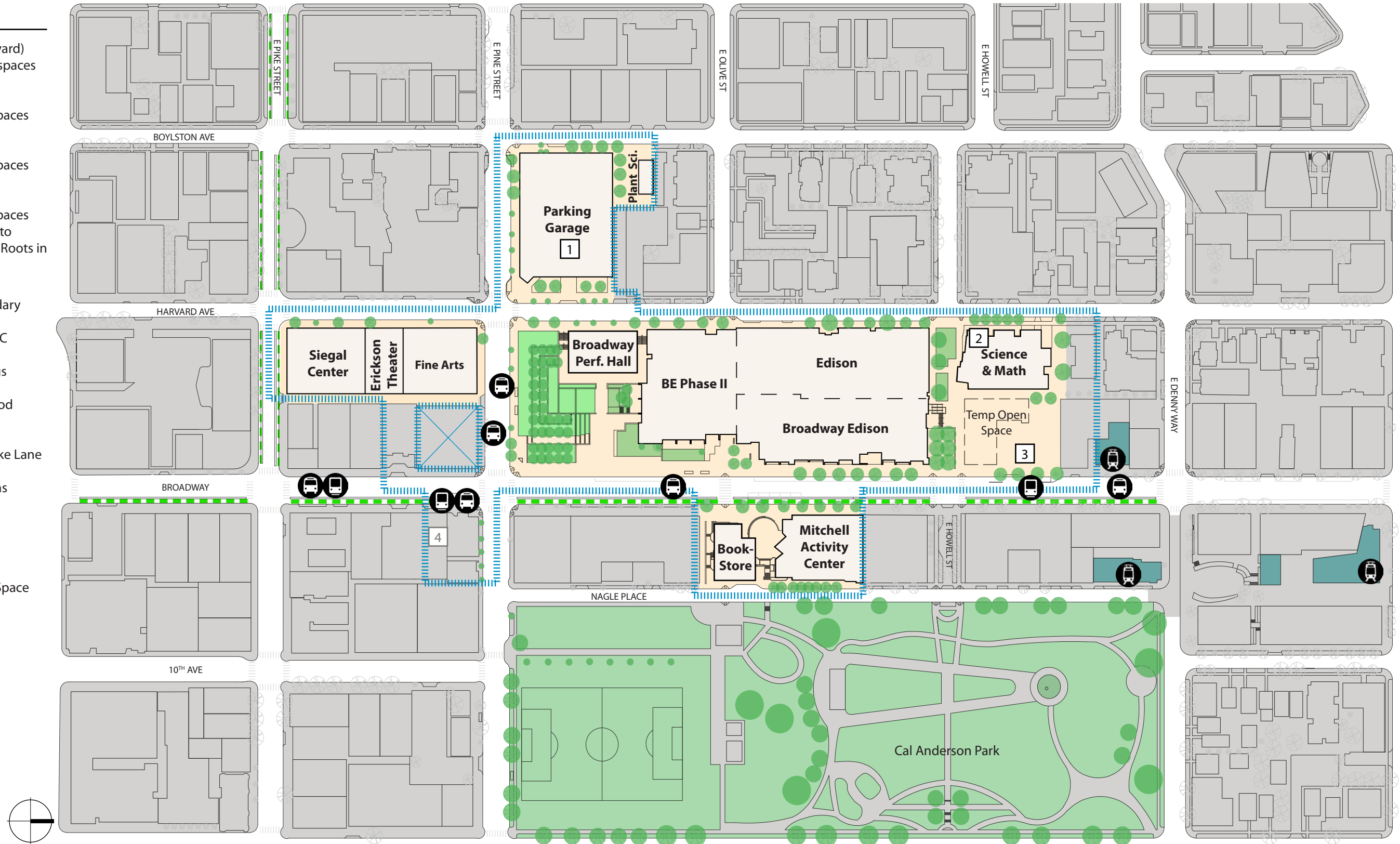
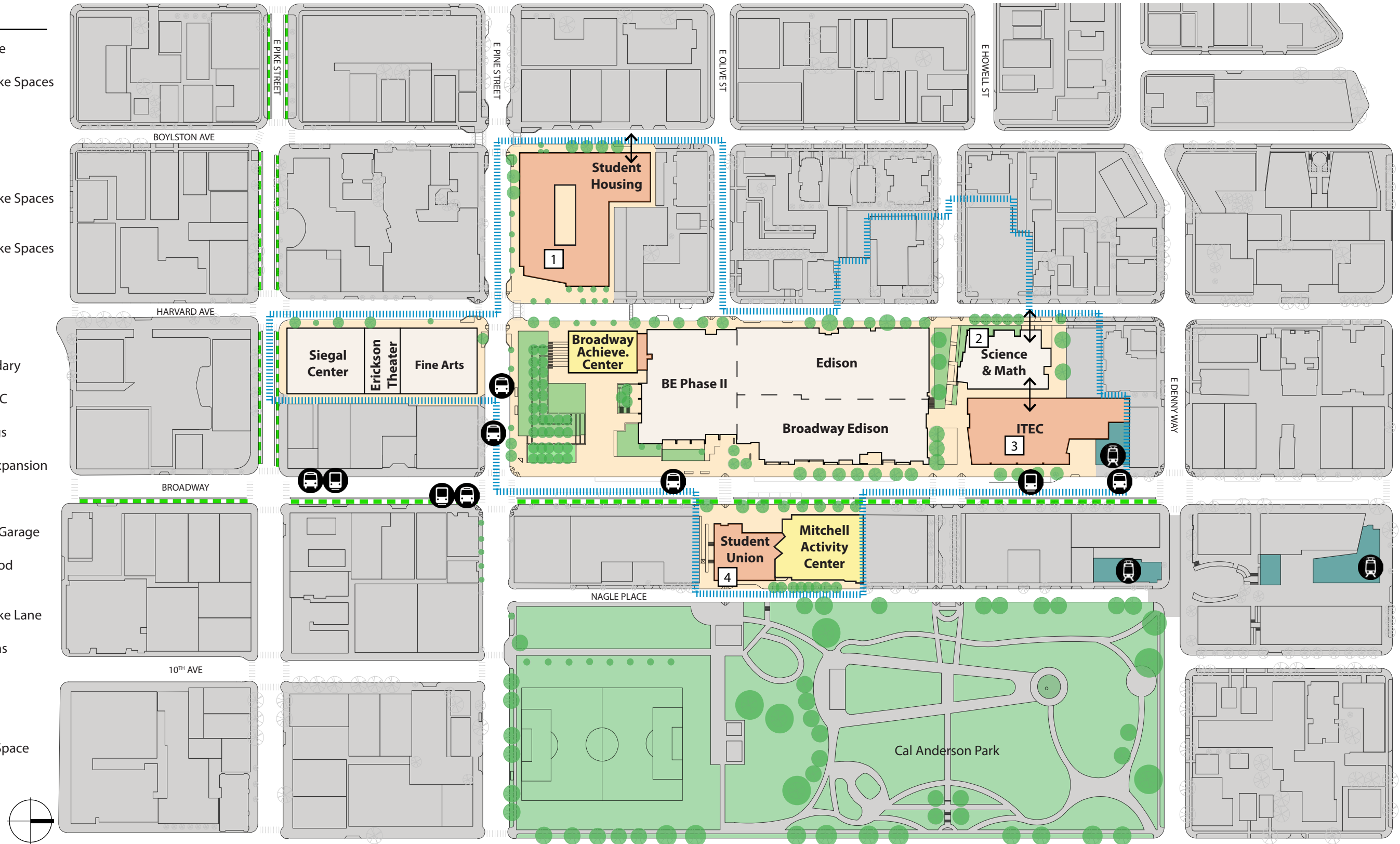


FIGURE 3-1.5 – PROPOSED TRANSPORTATION AND VEHICLE PARKING

LEGEND

- 1 Student Housing Garage
261 Vehicle Spaces
Long & Short-Term Bike Spaces
- 2 SAM Garage
35 Vehicle Spaces
- 3 ITEC Garage
198 Vehicle Spaces
Long & Short-Term Bike Spaces
- 4 Student Union/MAC
Long & Short-Term Bike Spaces

- Proposed MIO Boundary
- Parcels Owned by SCC
- Existing SCC Buildings
- New Construction/Expansion
- Renovation
- Vehicle Entry/Exit to Garage
- Existing Neighborhood Buildings
- Existing Protected Bike Lane
- Sound Transit Stations
- Bus Station
- Streetcar Station
- Park & Public Green Space



Bicycle Access and Facilities

Bicycle access to campus is currently served by the three existing bike lanes:

- North South Access – Broadway. A protected bike lane that runs the full length of campus and connects to Sound Transit Link, Seattle Streetcar, and KC Metro.
- East-West – E Pike Street. A protected bike lane that runs from downtown to Broadway.
- East-West – E Pine Street. A painted bike lane that runs from downtown to Broadway. The SDOT Seattle Bicycle Master Plan 2021-2024, anticipated further improvement to this bike lane.

See Figure 3-1.6 -Existing Transportation and Bicycle Parking

SCC currently provides secured bicycle storage (long-term parking) in the Harvard Parking Garage for use by SCC students, faculty, and staff. Short-term bicycle parking is provided at various locations throughout campus and is available for public use.

Proposed Bicycle Access and Facilities Improvements

See Chapter 4 Section 6 – Bicycle Parking and Facilities for information on proposed bicycle improvements.

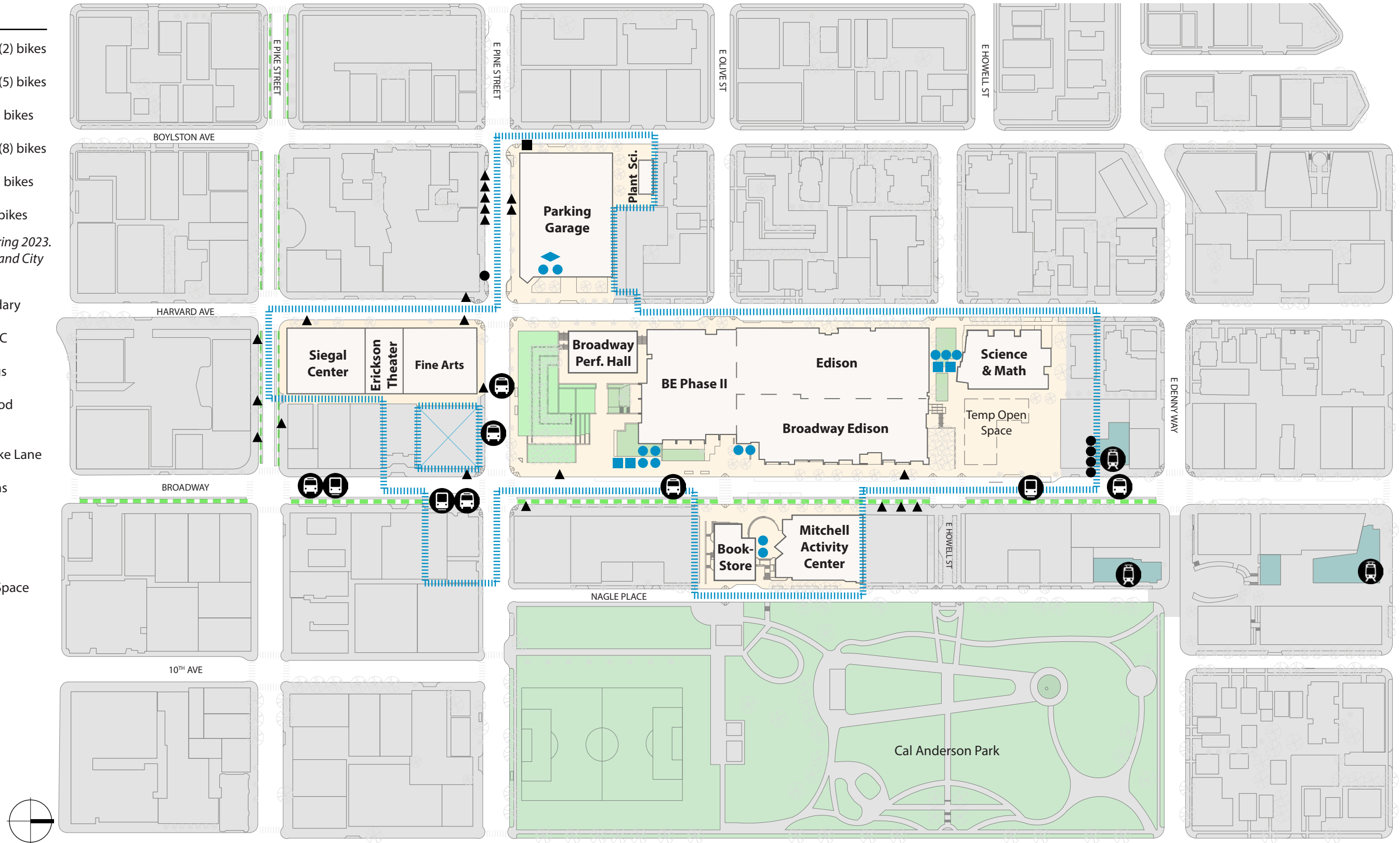
FIGURE 3-1.6 – EXISTING TRANSPORTATION AND BICYCLE PARKING

LEGEND

- ▲ Public short-term rack for (2) bikes
- Public short-term rack for (5) bikes
- SCC short-term rack for (5) bikes
- Public short-term rack for (8) bikes
- SCC short-term rack for (8) bikes
- ◆ SCC secure locker for (10) bikes

Bike rack survey done in Spring 2023. Based on site observations and City of Seattle GIS data.

- ▬▬▬▬▬▬ Proposed MIO Boundary
- Parcels Owned by SCC
- Existing SCC Buildings
- Existing Neighborhood Buildings
- ▬▬▬ Existing Protected Bike Lane
- 🚊 Sound Transit Stations
- 🚌 Bus Station
- 🚋 Streetcar Station
- 🌳 Park & Public Green Space



CHAPTER 4 – DESIGN GUIDELINES AND DEVELOPMENT STANDARDS

SECTION 1 – INTRODUCTION

The development standards component in this adopted master plan shall become the applicable regulations for physical development of Major Institution uses within the MIO District. These development standards shall supersede the development standards of the underlying zone. Where standards established in the underlying zone have not been modified by the master plan, the underlying zone standards shall continue to apply. This section describes the development standards that will apply to Seattle Central College for the duration of this MIMP. As this master plan represents an anticipated 20-year time horizon for the physical development of campus, many of the details are conceptual at this point. For this master plan to be successful, it is necessary to balance the rigor of specific requirements with the flexibility to address future needs as new conditions arise.

For standards that are measured, such as height and density, an explanation of the method used to calculate these can be found in Appendix A - Definitions section.

General Requirements

Per SMC 23.69.020, the following development standards are common to all Major Institutions:

- Major Institution uses shall be subject to the development standards for institutions of the underlying zone in which they are located, except for the dispersion requirements of the underlying zoning for institutions.
- Development standards for Major Institution uses within the Major Institution Overlay District, except the provisions of [Chapter 23.52](#), may be modified through adoption of a Major Institution Master Plan according to the provisions established in Subchapter VI, Part 2 of this chapter.
- Maximum structure heights for structures containing Major Institution uses may be allowed up to the limits established pursuant to [Section 23.69.004](#) through the adoption of a master plan for the Major Institution. A rezone shall be required to increase maximum structure height limits above levels established pursuant to [Section 23.69.004](#).
- The demolition of structures containing residential uses which are not Major Institution uses shall be prohibited if the demolition is intended to provide a parking lot or structure to accommodate nonrequired parking or to reduce a parking deficit.
- When a pedestrian designation in a commercial zone occurs along a boundary or within a campus, the blank facade standards of the underlying zoning shall apply.

Physical Planning Objectives

The Master Plan established a series of physical objectives to be achieved during the duration of this Master Plan:

- Plan for main campus enrollment of approximately 7,500 FTE (Full Time Equivalent) and total campus enrollment of approximately 8,150 FTE.
- Campus development should look to consolidate primary academic and student services functions on or immediately adjacent to the Broadway Edison Complex of buildings.
- The college will look to leverage or replace under-utilized and expensive buildings/sites located south of E Pine Street.
- Plan for new construction projects, to the greatest extent possible, to be developed via the SBCTC funding mechanisms for growth, renovation, and replacement projects.
- Plan for new construction projects, when SBCTC funding is not available in a timely manner, to be developed via public/private partnerships that seek to maximize the use of existing college resources without sacrificing the college's long-term viability.
- Propose renovation projects where opportunities exist to transform outdated instruction and service spaces into new spaces designed to serve today's students.
- Pursue renovation projects of highly under-utilized facilities to meet newer high demand needs.
- Seek campus infrastructure improvements including parking, major utilities, and a District Energy Plant.
- Initiate campus environmental upgrades, to enhance the physical environment for students, the community, and its visitors. Improvements will be tied to major projects to assist in funding.
- Actively engage with the greater Capitol Hill community to integrate SCC planning with other community driven plans to achieve mutual common benefits.

SECTION 2 – ZONING

The following two sections show the current and proposed zoning context of the SCC campus and its immediate environs. The diagrams include the MIO (major institution overlay) boundary and designations, the underlying zones, and other overlay districts. Also shown are the extents of Seattle Central College parcel ownership.

The requirements of the underlying zones can be found in the Seattle Municipal Code (SMC). NC (Neighborhood Commercial) is found in section 23.47A and MR (Midrise) in section 23.45. Requirements and development standards for MIO are governed by SMC 23.69.

2001 MIMP Boundary and Zoning

See Figure 4-2.1 – Existing Land Use Designations & Overlays for current zoning within and around the 2001 MIO. Seattle Central College's 2001 MIO Boundary was defined primarily by the parcels owned at the time of the MIMP application.

Area within MIO boundary (exclusive of ROW/streets). Data from King County Assessor 2020		
SCC owned parcels	= 419,127 square feet	=96.3%
Non-owned parcels	= 16,060 square feet	=3.7%
Total Areas of existing MIO	= 435,187 square feet	(=9.99 Acres)

The existing MIO boundary is split into two MIO Zoning designations. Parcels north of E Pine Street are designated MIO-105, and parcels south of E Pine are MIO-65.

Existing Underlying Zones

The predominate underlying zone of the MIO is NC3P-75. There are two exceptions: parcels that front Broadway Avenue north of E Pine Street are zoned NC3P-55 and the parcel housing the college greenhouse is MR.

Existing Overlay Districts and Urban Village

The Capitol Hill Station Overlay District encompasses all parcels north of East Olive Street and the parcel housing the college greenhouse.

The Pike/Pine Urban Village encompasses all parcels south of East Olive Street.

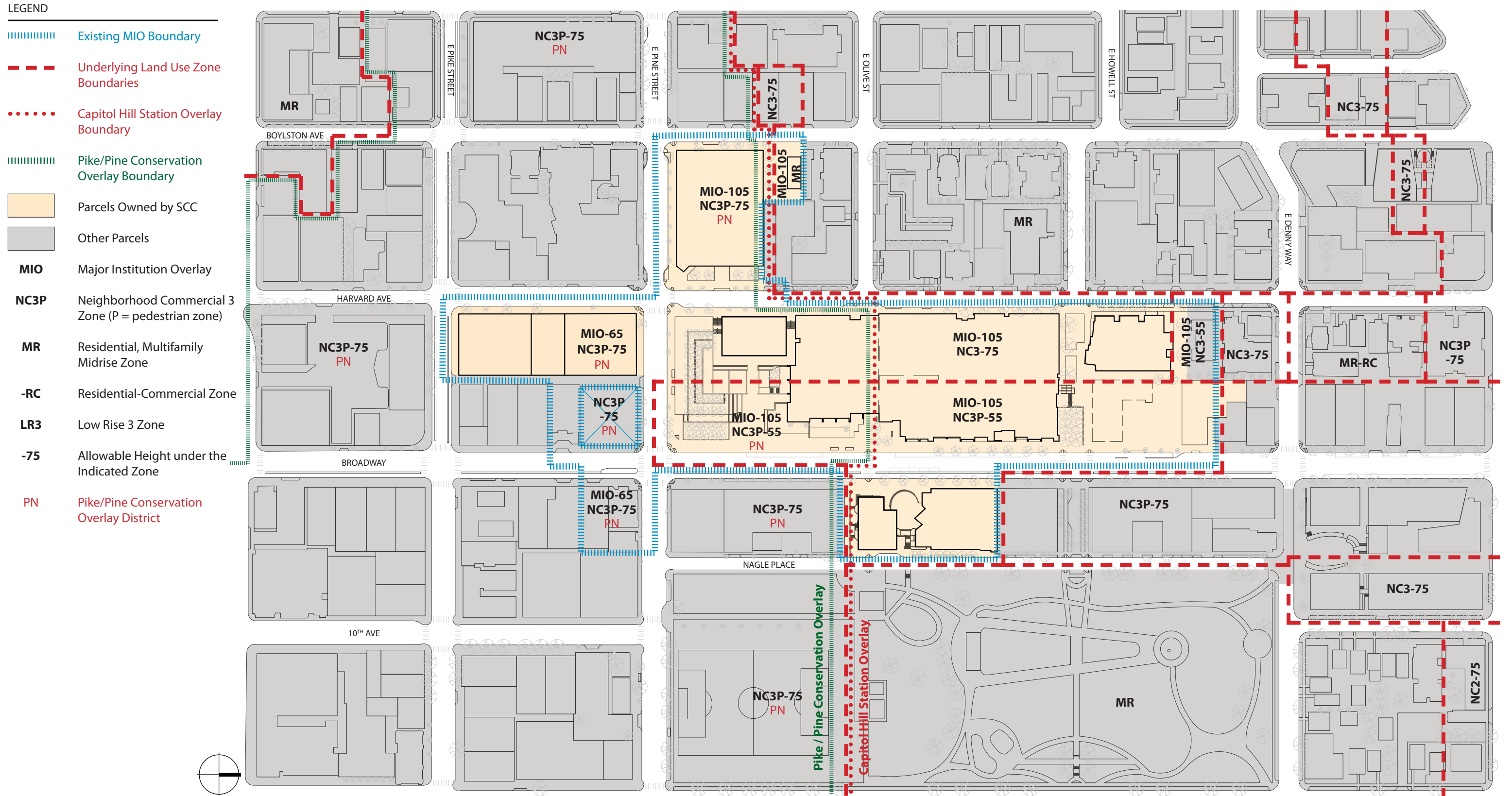
The Capitol Hill Urban Center Village encompasses all parcels north to East Olive Street.

A Pedestrian Overlay Zone encompasses all parcels on E Pike, E Pine, Broadway, and Nagle. Partial streets in this overlay include the south half of Boylston between E Olive and E Pine, and Harvard between E Olive and E Pike.

Site Disposition and Acquisition

Since approval of the 2001 MIMP, SCC has completed or is in the process of completing several parcel transactions. The following Proposed MIMP Boundary was proposed to acknowledge the transactions which have occurred or will occur as well as planning for potential future acquisition opportunities. Seattle Central does not anticipate any parcel dispositions as part of this MIMP.

FIGURE 4-2.1 – EXISTING LAND USE DESIGNATIONS & OVERLAYS



Proposed MIMP Boundary and Zoning

See **Figure 4-2.2 – Proposed Major Institution Overlay (MIO) District** which depicts the proposed boundary revisions and the underlying zones. The proposed MIO boundary includes five changes to the 2001 MIO boundary.

Three align the boundary with recent changes in SCC parcel ownership:

- **Sound Transit Site D** – The college is currently negotiating the acquisition of Sound Transit Site D. In addition, the college is negotiating the use of air-rights above the station for the purpose of façade enhancements (glazing and materials) abutting the transit station.
- **Broadway Café (aka Eldridge Tire)** – This parcel is being removed as it is currently in the process of disposition to Community Roots Housing.
- **South Annex (aka Booth Building) and International Program buildings** – These parcels are being removed as they are currently in the process of disposition to Community Roots Housing.

Two boundary expansions are proposed. These expansions are all parcels not currently owned by the College.

- **Boylston Properties** – If parcels become available, the college seeks opportunities to acquire parcels south of Boylston and west of Harvard Avenue for future, long term space needs. This includes three parcels: The Porter and Lenawee apartment buildings, and 713 East Olive.
- **Westminster Presbyterian Church Properties** – The college and the Presbyterian Church have mutual interest in the college acquisition of parcels west of Harvard currently owned by the church for future, long term space needs. This includes the church building at 1727 Harvard Avenue East and two parking lots located at 1700 and 1807 Harvard Avenue.

All Planned and Potential Projects are proposed on SCC owned parcels.

<u>Area within Proposed MIO boundary (exclusive of ROW/streets. Data from King County Assessor 2020)</u>		
SCC owned parcels	= 406,950 square feet	= 81.9%
Non-owned parcels	= 90,048 square feet	= 18.1%
Total Proposed MIO Boundary	= 496,998 square feet	
	(=11.41 Acres)	

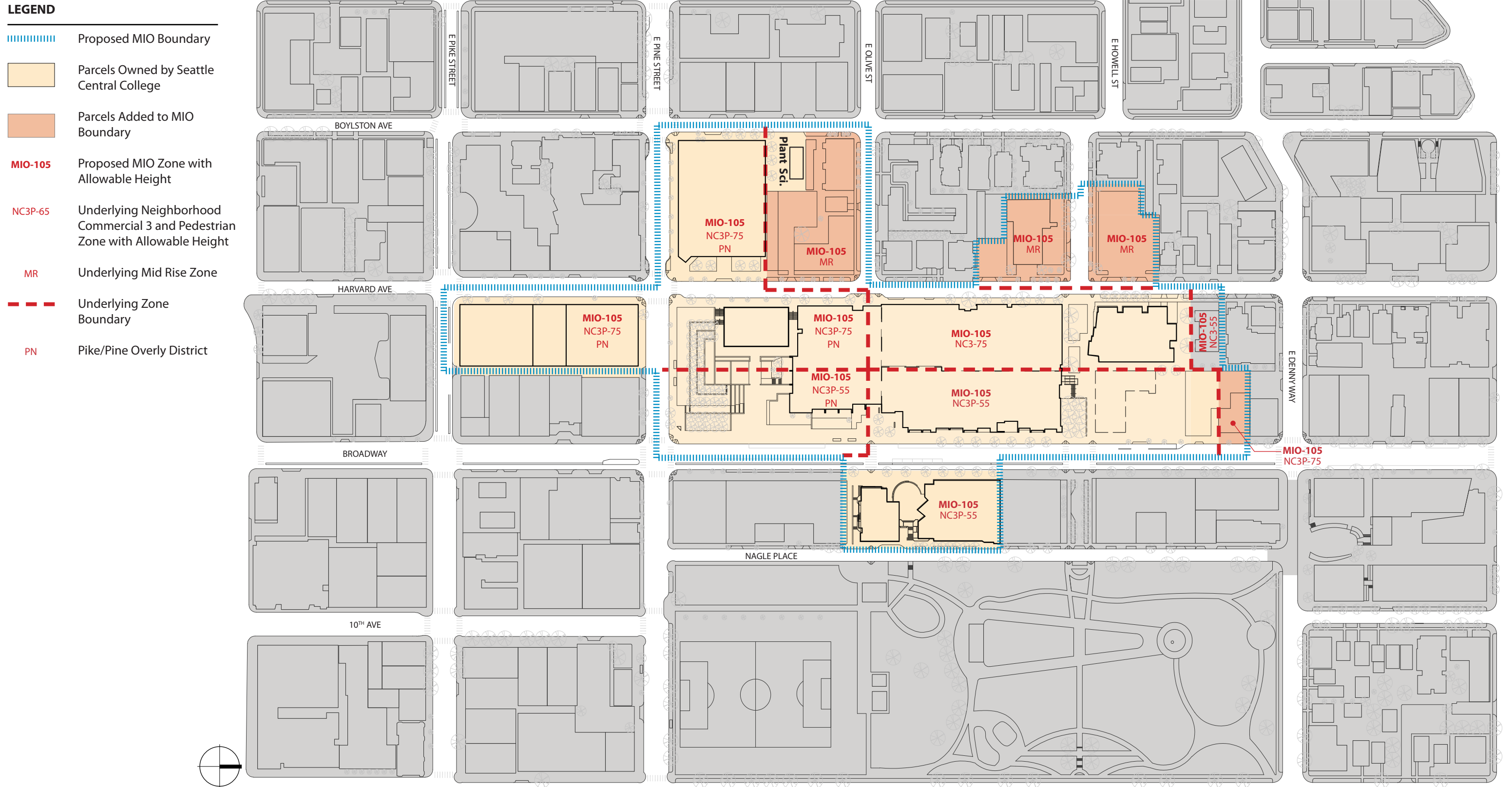
Proposed MIO Zoning

The proposed MIO seeks a zoning designation of MIO-105 for all parcels within the MIO boundary. The proposed designation exceeds the height allowed by the underlying zones and is intended to allow long-term concentration of the institution with minimal needs for neighborhood encroachment.

Underlying Zones

This master plan proposes no changes to any underlying zoning within the proposed MIMP boundary.

FIGURE 4-2.2 – PROPOSED MAJOR INSTITUTION OVERLAY (MIO) DISTRICT



SECTION 3 – DEVELOPMENT STANDARDS

Overlay Districts

As stated previously, the Seattle Central College campus falls within several overlay districts. These overlay districts have their own development standards in the Seattle Municipal Code. Those districts are the Capitol Hill Station Overlay District, the Pike/Pine Conservation Overlay District, and the Capitol Hill Urban Center Village.

Proposed Overlay District Standards

Design Guidelines from the various overlay districts have been reviewed and integrated into this MIMP so that future development reflects the goals of the surrounding neighborhoods. See Chapter 4 Section 4 – Design Guidelines.

Projects developed by Seattle Central College under this MIMP will not follow the Development Standards of the aforementioned overlay districts nor other overlay districts.

Setbacks

Per SMC 23.69.030.C.3.a

The development standards component of a master plan shall include the structure setbacks along public rights-of-way and at the boundary of the MIO District.

Setback standards are established in SMC 23.47A.014 for Neighborhood Commercial (NC) zones and in SMC 23.45.518 for Residential Multifamily Midrise (MR) zones. Setback and separation standards within the proposed MIO Boundary are modified as follows:

Proposed Setback Standard

- There are no minimum setbacks required between SCC owned parcels.
- There are no minimum setbacks along the edges of SCC properties abutting streets except as noted below.
- Where SCC parcels abut Neighborhood Commercial (NC), and Residential (MR) zoned lots, the following setbacks will apply.

Location	Building Height	Minimum Setback	Setback at Underlying MR/NC3P
Front lot lines	< 13'	0'	0'
	13- 65'	0'	0'
	> 65'	10'	*
Side and Rear lot lines	< 13'	0'	0'
	13 - 65'	10'	10'
	> 65'	1'/10' additional height	1'/10' additional height

* = Upper-level setback requirements for street-facing facades

Exceptions:

Locations	Minimum Setback
Broadway Street – west	Match minimum existing setback of BE Complex
Broadway Street – east	Match existing setback of Mitchell Activity Center
E Pine Street – north	Match existing setback of Parking Garage
All side lot lines abutting Resid./MR	15' triangle at all lot abutments

Projections into required setbacks:

At all frontages – where canopies are provided for the purpose of providing pedestrian cover from weather, they shall be excluded in setback calculations. Canopies extending into the ROW are subject to approval by Authorities Having Jurisdiction (AHJ)

Projections allowed in required setbacks:

- Ramps or other devices necessary for access for the disabled and elderly, which meet Seattle Building Code, Chapter 11, are permitted in required setbacks.
- Fences, bulkheads, freestanding walls, and other similar structures
 - Fences, freestanding walls, and other similar structures 6 feet or less in height above existing or finished grade, whichever is lower, are permitted in required setbacks. The 6-foot height may be averaged along sloping grade for each 6-foot-long segment of the fence, but in no case may any portion of the fence exceed 8 feet.
 - Bulkheads and retaining walls used to raise grade may be placed in any required setback when limited to 6 feet in height, measured above existing grade. A guardrail no higher than 42 inches may be placed on top of a bulkhead or retaining wall existing as of September 30, 1994. If a fence is placed on top of a new bulkhead or retaining wall, the maximum combined height is limited to 9.5 feet.
 - Bulkheads and retaining walls used to protect a cut into existing grade may not exceed the minimum height necessary to support the cut or 6 feet, whichever is greater. When the bulkhead is measured from the low side and it exceeds 6 feet, an open guardrail of no more than 42 inches meeting Building Code requirements may be placed on top of the bulkhead or retaining wall. A fence must be set back a minimum of 3 feet from such a bulkhead or retaining wall.
- Dumpsters and other trash receptacles, except for trash compactors, located outside of structures are not permitted within 10 feet of any lot line that abuts a residential zone and must be screened per the provisions of [Section 23.47A.016](#).
- Green stormwater infrastructure (GSI) features are allowed without setback restrictions if:
 - Each above-grade GSI feature is less than 4.5 feet tall, excluding piping.
 - Each above-grade GSI feature is less than 4 feet wide; and
 - The total storage capacity of all above-grade GSI features is no greater than 600 gallons.
- Above-grade GSI features larger than what is allowed in subsection 23.47A.014.G.9 are allowed within a required setback if:
 - Above-grade GSI features do not exceed ten percent coverage of any one setback area.
 - No portion of an above-grade GSI feature is located closer than 2.5 feet from a side lot line.
 - No portion of an above-grade GSI feature projects more than 5 feet into a front or rear setback area; and
 - Above-grade GSI features meet all applicable Building Code and Plumbing Code requirements.

Façade Modulation/Articulation Standard

Façade Modulation standards are established in SMC 23.47A.014.D for Neighborhood Commercial (NC) zones. Façade Modulation/Articulation standards are established in SMC 23.45.529 for Residential Multifamily Midrise (MR) zones. Façade Modulation standards within the proposed MIO Boundary are modified as follows:

Proposed Façade Modulation Standard

For facades facing a Pedestrian Zone and have a width of more than 200', at least one portion of the structure 30 feet or greater in width must be setback 20 feet from the property line. The setback area shall provide publicly accessible open space and/or green space or shall provide outdoor area for retail or commercial use.

Structure Height Limits

Per SMC 23.69.030.C.3.b

The development standards component of a master plan shall include height limits per SMC 23.69.004

Structure height limit standards are established in SMC 23.47A.012 for Neighborhood Commercial (NC) zones and in SMC 23.45.514 for Residential Multifamily Midrise (MR) zones. Structure height standards within the proposed MIO Boundary are modified as follows:

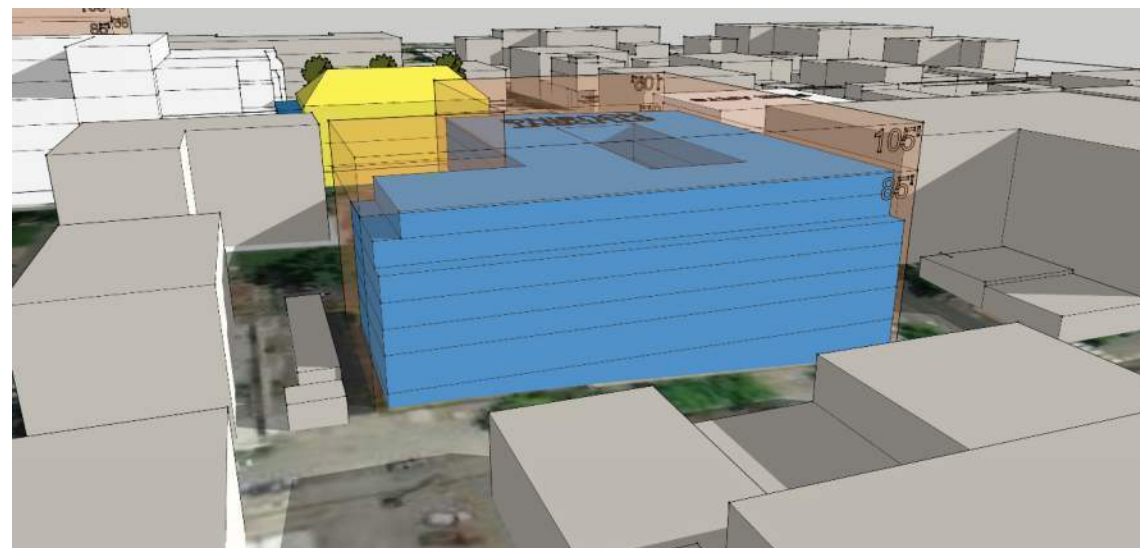
Proposed Structure Height Limit Standard

The maximum height limit of 105 feet shall apply across the entire MIO District. The height limit would have the standard exceptions allowed as part of the commercial zoning district as well as use of the standard height measurement techniques defined by the current zoning code unless specifically altered by this MIMP.

Existing and proposed SCC structures are developed, or proposed to be developed, to the maximum 105' height limit (unless noted otherwise), rather than requiring future expansion horizontally into the neighborhood. The height limit proposed retains the previously 2001 MIMP approved limit of 105' (approved for parcels north of E Pine Street) to preserve the ability to allow intensified institutional development consistent with the Major Institutional Policies. Any future project that has a proposed height beyond the height of the project discussed in the MIMP would be subject to review and approval by the IAC and/or a master plan amendment pursuant to City policies.

The following summary shows structure heights proposed for projects included in this MIMP:

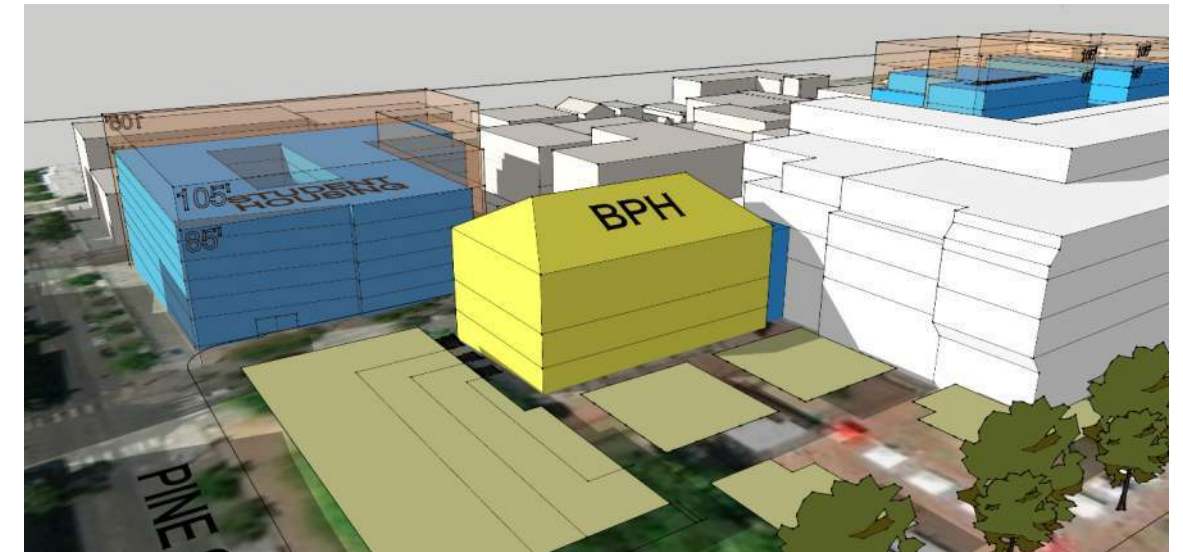
Project – Stories	Proposed Height	Allowable Height by Underlying Zone	Max MIO Height
Student Housing – 6 stories	90'	75' / 85'	105'
ITEC – 6 stories	95'	55' / 75'	105'
Broadway Achieve. Ctr. - N/A	N/A - No Change	75'	105'
Student Center – 3 stories	60'	55'	105'
Harvard I – 5 stories	80'	85'	105'
Harvard II – 5 stories	80'	85'	105'



Student Housing – Six stories and approximately 90 feet



ITEC – Six stories and approximately 95 feet



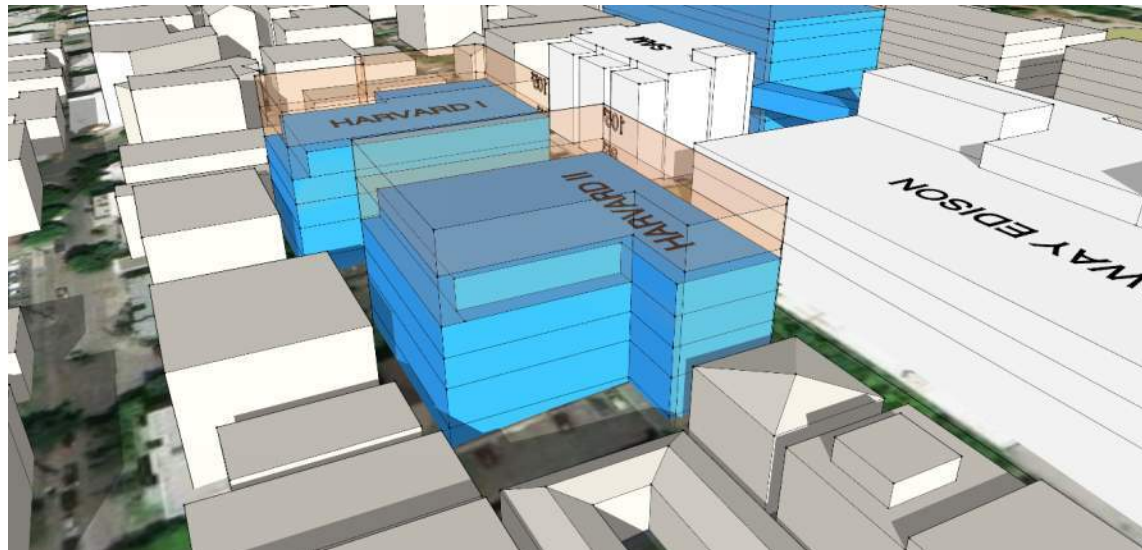
Broadway Achievement Center – Interior renovation, no substantive change to height or bulk



Student Center – Three stories and approximately 60 feet



Harvard I – Five stories and approximately 80 feet



Harvard II – Five stories and approximately 80 feet

Lot Coverage *Per SMC 23.69.030.C.3.c*
The development standards component of a master plan shall include Lot Coverage for the entire MIO District.

Proposed Lot Coverage Standard

Lot coverage by above grade structures will not exceed 80% for the entire campus area. The lot coverage shall be calculated over the parcels owned by Seattle Central College in the MIO District and shall not apply individually to the building sites, parcels, etc.

Current lot coverage ranges from 15-100% by individual building sites with the total average for the entire MIO District Estimated at 67%. As an urban campus, densification is expected and planned. The re-development of the North Plaza area would be the primary addition to overall campus lot coverage. If this area were 100% covered, the total campus average would reach about 75% lot coverage. Other possible lot coverage by building additions and the level of accuracy of the conceptual site/building statistics suggest that the 80% maximum lot coverage standard is appropriate.

Note: The underlying zone has no lot coverage or open space standard for non-residential uses. Thus, buildings could cover 100% of their sites.

Floor Area Ratio (FAR)

Floor Area Ratio (FAR) standards are established in SMC 23.47A.013 for Neighborhood Commercial (NC) zones and in SMC 23.45.510 for Residential Multifamily Midrise (MR) zones. Floor Area Ratio standards within the proposed MIO Boundary are modified as follows:

Proposed Floor Area Ratio (FAR) Standard

Floor Area Ratio (FAR) will not exceed 2.50 for the entire campus area. The FAR shall be calculated over the parcels owned by Seattle Central College in the MIO District and shall not apply individually to the building sites, parcels, etc.

The total amount of campus development is described by the amount of building and by a floor area ratio (FAR) comparing building with site area. The basis for the floor area ratio calculation is summarized in the table below. Structured parking and below grade space is excluded from this FAR calculation.

2001 MIMP Allowable FAR	2.10	
Existing Campus FAR	1.55	
FAR after Planned and Potential Projects	2.28	(SCC Owned and Harvard I and II parcels)
Proposed MIMP FAR	2.50	
Underlying Zone FAR	5.5, 5.75	(NC3P-75, MR)

Existing Campus Density (Floor Area Ratio – FAR) Calculation

Building	Total Building Area	Less Below Grade Area	Area used in FAR Calculation
South Annex*	17,333	3,142	0*
International Programs*	4,632	-	0*
Siegal Center	43,774	9,163	34,611
Erickson Theater	7,973	-	7,973
Fine Arts Building	66,814	16,776	50,038
Atlas Building (outside MIMP boundary) *	-	-	-
Broadway Café*	1,040	-	0*
Parking Garage	2,291	-	2,291
Plant Sciences Lab	2,378	-	2,378
Edison Technical Building	130,527	-	130,527
BE Phase I Building	175,568	12,373	163,195
BE Phase II Building	125,863	-	125,863
Broadway Performance Hall	41,174	7,219	33,955
Science and Math Building	69,159	-	69,159
College Bookstore	13,594	-	13,594
Mitchell Activity Center	65,921	25,232	40,689
Total Existing			674,273
Land Area of Existing MIMP Boundary			435,187
Existing Campus FAR			1.55

* = buildings removed from inventory

Planned and Potential Campus Density (Floor Area Ratio – FAR) Calculation

Building	Total Building Area	Less Below Grade Area	Area used in FAR Calculation
Student Housing	185,055	-	185,055
ITEC	140,000	-	140,000
Broadway Achievement Center	2,406	-	2,406
Student Union	30,000	-	30,000
Harvard I	50,000	-	50,000
Harvard II	50,000	-	50,000
District Energy Plant	15,000	15,000	-
Subtotal Planned & Potential Projects			457,461
Total GSF of Existing + Planned + Potential Projects			1,131,734
Land Area of Proposed MIMP Boundary			496,998
Existing + Planned + Potential Projects Campus FAR			2.28

Vehicle Parking See SMC 23.54.016 for Major Institution parking requirements.

Landscaping *Per SMC 23.69.030.C.3.d*
The development standards component of a master plan shall include landscaping.

Landscaping and Screening standards are established in SMC 23.47A.016 for Neighborhood Commercial (NC) zones and in SMC 23.45.524 for Residential Multifamily Midrise (MR) zones. Landscape standards within the proposed MIO Boundary are modified as follows:

Proposed Landscape Standard

The intent of campus landscaping is to soften the built environment while not creating safety/security or maintenance concerns. There are no specific landscaping standards proposed as they relate to quantity of landscape area. This standard proposes that landscape area be incorporated into the Open Space Standard noted below. The location and configuration of the landscaped space may change over time.

Landscaping design elements are incorporated into several Design Guidelines included later in this chapter. These design guidelines will be applied as part of individual building and/or improvement projects.

In addition to the major landscaped open spaces noted below, landscaping will be included and complement project development in rights-of-way in the form of street trees, green stormwater development, and plantings in pedestrian circulation spaces. Landscaping in ROW will be designed in accordance with SDOT standards.

The College manages its facilities and grounds as recommended by the Association of Physical Plan Administrators (APPA). The APPA provides standards for the performance of ground maintenance functions based on both a land use inventory of the scope and nature of the grounds to be cared for and a determination of the standards of care. SCC will continue to use standards provided by the APPA or other applicable associations for the care and maintenance of campus environs.

Proposed Setback Landscaping Standard:

A minimum of Fifty percent of all total site setback area provided, regardless of minimum requirements shall be landscaped.

Proposed Tree Planning and Preservation

The SCC Campus features high value trees, namely the tree grove adjacent to the South Plaza. Any modifications proposed to the South Plaza and the associated tree grove will balance prioritizing preservation of the individual trees and the grove with improving the quality and usability of the South Plaza and tree grove. Proposed modifications to all areas of campus that impact existing trees will balance preservation of trees with creating quality public space for the College and community.

When planning and designing of landscape features is undertaken, tree species selection will be reviewed. As of 2022, the most common tree species on campus are London Planetree (*Platanus x acerifolia*), Pin Oak (*Quercus palustris*), Red Oak (*Quercus rubra*), and Sweetgum (*Liquidambar styraciflua*). These four species make up approximately 44% of the trees within the proposed MIO district. These species should be avoided, and new species will be reviewed and selected to increase diversity throughout the campus.

All existing trees (including Heritage and Exceptional trees) on sites slated for development will be surveyed prior to any development action. Retention of all trees will be considered throughout the design process to ensure that trees with high retention value can be protected. All pruning required for construction clearance should be performed by an ISA Certified arborist and conform to current ANSI 300 standards (or prevailing standards at the time of work).

Open Space *Per SMC 23.69.030.C.3.e*

The development standards component of a master plan shall include the percentage of MIO District to remain in open space.

The urban nature of SCC's campus environs, the way it is used by its students and the community at large, puts importance on the use and effectiveness of the open areas rather than the quantity. This was evidenced through the numerous discussions and charrettes with the Development Advisory Committee. During those discussions, there was general agreement that the quantities of Open Space (and green space) was appropriate and that changes to the amount of space was not necessary. What was clearly agreed was that the quality of the open areas needed consideration and improvement.

Existing Open Space

On existing SCC owned/developed parcels (January 2021)

Building Footprints	63%	
Open/Green Space	31%	includes all softscape and hardscape spaces (sidewalks, lawns, planted areas, plazas, etc.).
Surface Parking	6%	

Existing Open/green Spaces on campus include:

- South Plaza/South Green - on the corner of E Pine St and Broadway
- E Howell St Passage – a previously vacated street that connects Broadway to Harvard
- Broadway Edison Complex/MAC Student Center entrance areas - mid-block on Broadway.

A temporary open space exists on the site of the former North Plaza building on Broadway, east of Science and Math. This temporary open space aligns with the footprint of the planned ITEC project; therefore, it will be removed when construction of the ITEC project commences.

Proposed Open Space Standard

A minimum of 30% of SCC owned parcels within the MIO District boundary shall be preserved as Open/Green space. Applicable space shall be defined as any of the following: lawns, planting beds, plazas, and walkways. It will also include elevated (i.e., rooftop) plazas and green roof areas if made available for public use. This standard shall not apply to individual parcels, but will be distributed over the entirety of college-owned parcels within the MIO District. Neither individual parcels nor the entirety of college-owned parcels shall be subject to a minimum Green Factor score.

SCC will maintain and improve the Existing Open spaces identified above. Description of proposed improvements will be provided as defined elsewhere in this Master Plan document.

Street Level Development Standards and Uses

Per SMC 23.69.008.C.3:

Where the underlying zone is commercial, uses at street level shall complement uses in the surrounding commercial area and be located in a manner that provides continuity to the commercial street front. Where the underlying zoning is a pedestrian-designated zone, the provisions of Section 23.47A.005 governing street-level uses shall apply.

Per SMC 23.69.020.E:

When a pedestrian designation in a commercial zone occurs along a boundary or within a campus, the blank facade standards of the underlying zoning shall apply.

Street Level Use standards are established in SMC 23.47A.005 for Neighborhood Commercial (NC) zones. Street Level Development standards are established in SMC 23.47A.008 for Neighborhood Commercial (NC) zones. Street Level Use and Development standards within the proposed MIO Boundary are modified as follows:

Proposed Standard for any development fronting Broadway Avenue:

- The floor-to-floor height at the ground floor of the building shall be between 15 and 20 feet high and shall be recessed from the property line to align with the face of the existing Broadway Edison Building to provide for an expanded sidewalk area. This expanded sidewalk area may include green spaces, bicycle parking, pedestrian seating, and other pedestrian oriented amenities.
- Weather projection shall be provided along Broadway. The weather protection shall be six to eight feet deep over the public right-of-way and shall be constructed between 12 and 20 feet.

Height and Scale Transition

Per SMC 23.69.030.C.4.a

The Major Institution may choose, or the Director may require the Major Institution to address the Transition in height and scale between developments within the MIO District and development in the surrounding area.

The transition in height and scale between SCC development and the surrounding neighborhood will be achieved by other standards for height, setback, and landscaping/open space. No further standards are proposed or will apply.

The other proposed standards that establish lot coverage, density (floor area ratio), and open space limits effectively create a building transition between the zone's height differences. For example, there are no lot coverage limits in the underlying commercial and residential zone. SCC proposes an institutional lot coverage limit of 80%. The site coverage limit will reduce the institution building "footprints" and create building separations. There are no density limits in the underlying commercial and residential zones.

Historic Preservation Review, Policies and Practices

Per SMC 23.69.030.C.4.d

The Major Institution may choose, or the Director may require the Major Institution to address Preservation of historic structures which are designate on Federal, State or local registers.

Seattle Central College is a state institution of higher education and a member of the Community and Technical College state agency. Pursuant to RCW 36.70A.103 and .200, "State agencies shall comply with the local...development regulations and amendments thereto adopted pursuant to this chapter," but "[n]o local...development regulation may preclude the siting of essential public facilities," including "state education facilities."

Seattle Central College provides responsible and proactive stewardship of its campus assets through preservation of its historic and cultural resources and a managed strategy of property development. Campus planning and historic preservation provide the context for campus development in the future. The College regards building preservation, reuse, and rehabilitation as a continuum with new construction undertaken when other options are not reasonably feasible. The College's physical setting seeks to satisfy academic, social, and cultural requirements of students, faculty, and staff consistent with its primary mission.

The master plan creates a balanced approach to future growth on campus by adopting a compact, high-density approach to development that enables the preservation of historic campus assets, the creation of new public spaces, and an integrated pedestrian and community network. **As part of any project development, where an existing resource is eligible for the Nomination process, The College will work with the City and State to complete a Historic and Cultural Resources Assessment (HRA) that shall be a common reference material for historic preservation implementation.**

Seattle Central College Process related to potential development of Landmarks

Landmark status does not preclude all changes to a property. If a building is designated as a City of Seattle landmark, changes to the designated features of the building will be reviewed by the Landmarks Preservation Board as a part of the Certificate of Approval process. The Landmarks Preservation Board Reviews Certificates of Approval to ensure that change is managed in a way that respects the historical significance of the designated landmark.

Pursuant to the College's Lead Agency SEPA policies, the College will, as established in the Seattle Municipal Code (SMC), submit a landmark nomination application to the Landmarks Preservation Board in advance of the MUP process. It is the college's intention to continue to comply with the City's Landmarks Preservation Ordinance, SMC 25.12, to respect the character of historic structures as a complement to new development. No existing buildings are currently designated landmarks.

As Seattle Central College moves forward with any Master Use Permit (MUP) applications for development that would include the demolition or substantial alteration to any building 25 years or older and/or public comment suggests that the building is historic, a referral will be made to the City's Historic Preservation Office and the nomination process will be executed.



Fine Arts Building – also known as the Egyptian Theater. The building was originally a Masonic Lodge built in 1915, and remodeled in 2004



Siegal Center- Originally constructed as part of the Eldridge Tire Company collection of building in 1912, Seattle Central College renovated and occupied the building in 1990.



Broadway Performance Hall - A reconstruction done in 1978 on the site of the original Broadway High School which was original constructed in 1910 and demolished in 1976.



Broadway Edison Complex - A collection of buildings constructed at various times between 1921 and 1973. Including:

- Edison Technical South - Opened in 1921 with a third-floor addition in 1930.
- Edison Technical Central – Opened in 1942.
- Edison Technical North – Opened in 1949.
- Broadway Edison Phase I - Opened in 1973.
- Broadway Edison Phase II - Opened in 1976.

Existing Seattle Central Buildings Eligible for Landmarks Nomination

The existing Seattle Central Campus does not include any existing Landmark-designated structures, nor are there any within the proposed MIO boundary. There are, however, several structures that are eligible due to their age and the regulations of the City of Seattle for the nomination process. **See Figure 4-3.1 – Area Buildings Eligible for Landmarks Nomination** for buildings eligible for nomination within the proposed MIO Boundary:

The College shall preserve the historic character of the north and west facades and the lobby of the Masonic Temple Building (also known as the Egyptian Theater). It is expected that the Egyptian Theater, if nominated for Landmark Preservation, would be determined by the City of Seattle to be a significant structure, and be granted Landmark status.

There are additional structures (residential and religious) inside the proposed MIO boundary that would also be eligible. Since none of these parcels are currently owned by the College, further investigation has not been conducted.

FIGURE 4-3.1 – AREA BUILDINGS ELIGIBLE FOR LANDMARKS NOMINATION

LEGEND

Proposed MIO Boundary

Pike / Pine Conservation District

Seattle Central College buildings in excess of 25 years old

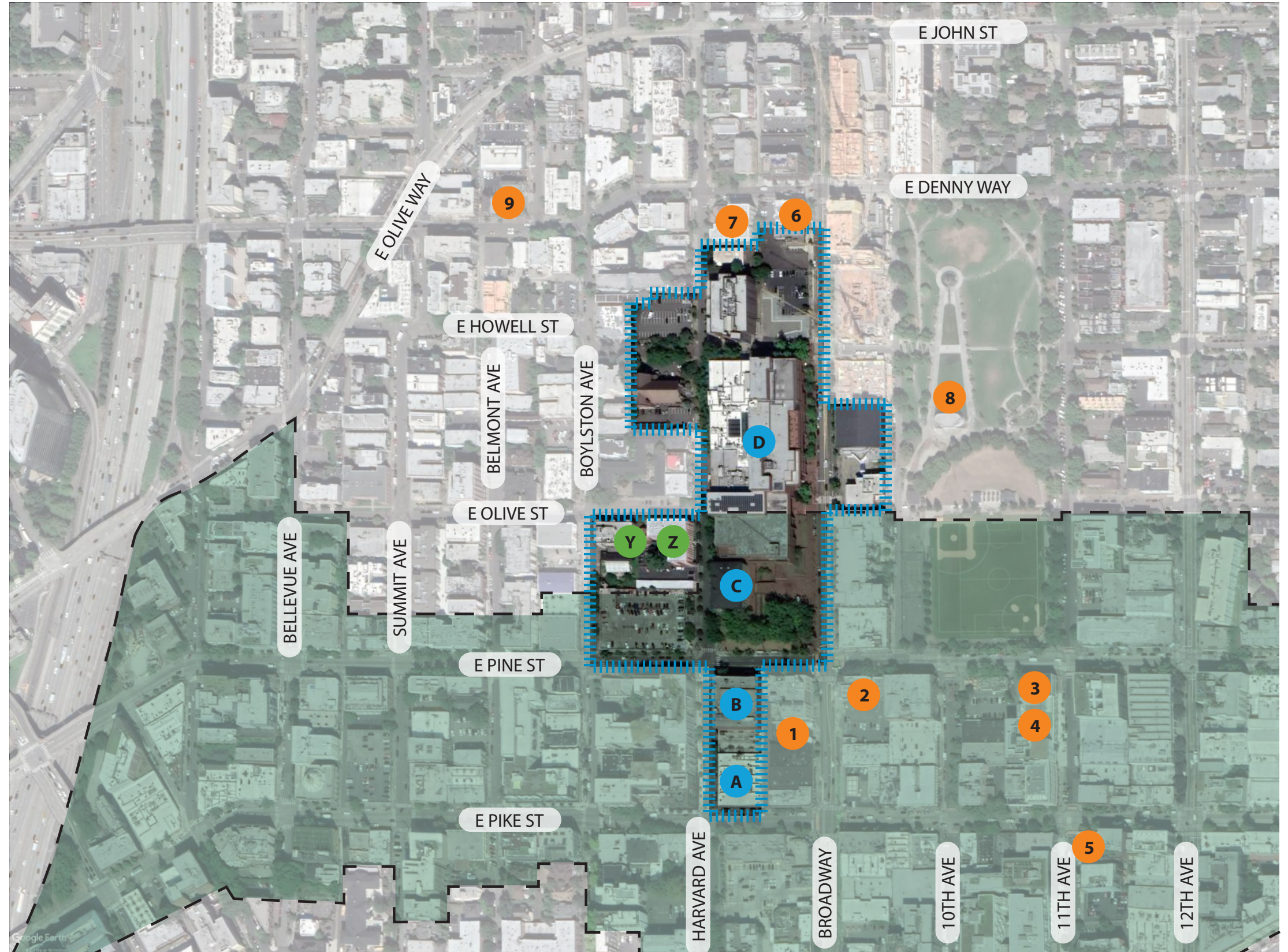
- A** Siegal Center (formerly Eldridge Motors)
Constructed 1912. Renovation ????
Noted on City survey as not eligible for Landmark due to extent of previous alteration
- B** Fine Arts Building (Masonic Temple)
Constructed 1915. Renovation 2004
- C** Broadway Performance Hall (Broadway High School)
Constructed 1910. Demolition / Renovation 1978
- D** Broadway Edison Complex
Edison Technical - Constructed 1923-25. Reno 1973
Broadway Edison Phase I - Constructed 1976
Broadway Edison Phase II - Constructed 1978

Buildings within MIO Boundary noted by City of Seattle survey as eligible for Landmark

- Y** 713 E. Olive Street - Constructed 1902
- Z** Lenawee Apartments - Constructed 1940

Existing Landmark buildings in proximity to SCC MIO

- 1** Eldridge Tire Company - Constructed 1925
- 2** Booth Building - Constructed 1906
- 3** White Motor Company Building - Constructed 1918
- 4** Kelly Springfield Motor Truck Company
Constructed 1916
- 5** 1101 E. Pike Street - Constructed 1916
- 6** Avon Apartments / Capitol Crest Apartments
Constructed 1905
- 7** Pantages House - Constructed 1906
- 8** Lincoln Reservoir - Constructed 1889
- 9** Ward House - Constructed 1882



Application of Best Practices for understanding the cultural context of Seattle Central College
 The following table outlines the identified best practices for historic preservation in master planning for campuses.

Best Practice	Completed	In process as part of EIS	Long term goal for college	Long term goals in partnership w/community
1. Outline goals for preservation: Establish a larger framework for the preservation efforts. Establish a context statement regarding the history of the school and the values that will guide the preservation efforts. A long-term goal would be a nuanced context statement developed in partnership with the community and non-profits, faculty, and students from best practice 7 and 9 along with the administration, campus architect, and citizen advisory council.	Outline history established with narrative	Begin outlining college values	Continue to refine long term goals for preservation	Continue developing historic and cultural context statement in partnership with community
2. Understand the cultural landscape of the school. As an urban institution, SCC has different challenges than a rural institution. Although Preservation Brief 36 regarding cultural landscapes may not be 100% applicable, it can be consulted for guidance.		Begin landscape survey and analysis	Continue to develop survey and context	Continue developing context
3. Conduct an inventory of every parcel under ownership by SCC to identify the build date, architect, landscape architect, contractor, and any significant events or associated persons with the property, and any public art located on the property. This inventory should be considered a work in progress and can be updated periodically, at a set date or when properties change ownership.		Include windshield survey in master plan	Continue to develop and update survey	
4. Create or collate building condition surveys for each identified contributing resource.	Condition surveys created	Assemble documents for identified eligible buildings	Add to collection when new buildings acquired and identified as eligible	
5. Pursue local landmark or national register designations for those buildings that meet the criteria. This is a long-term goal that may occur over decades.			Pursue at appropriate time	Partner where appropriate
6. Appoint a preservation officer. For SCC, this person would not have a stand-alone position but would appropriately be the campus architect.		Identify in master plan		
7. Use faculty and students for campus engagement. This may involve creating a multidisciplinary class for Art/English/History credit to explore issues of campus architecture, identity, and history.			Long term goal	
8. Incorporate historic preservation into the maintenance plan. All buildings identified as eligible for preservation should have maintenance staff trained for best practices in preservation and familiar with the appropriate preservation briefs and technical memos for the materials on the buildings.		Assemble relevant briefs for identified eligible buildings	Incorporate goals with facilities management	

9. Partnerships with local non-profits. This includes the Capitol Historic Society and Historic Seattle. On mid-century buildings partnership with DoCoMo Mo WEWA may be helpful. The Washington Trust for Historic Preservation would have advice on any building with outstanding significance.		Identify local non-profits		Continue to maintain relationships
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Individual Project Review to Ensure Historic Context

While fostering continuous use, improvements and innovations to campus, the College works to ensure that historic significance, value, and association of its assets is preserved for the community, City, and State. To ensure this occurs on a project-by-project basis, the College utilizes a multi-step process for historic preservation review.

To aid the reviewing bodies and further ensure that historic resources are respected, the College prepares a Historic Resources Assessment (HRA) for any project that makes exterior alterations to a building or landscape more than 25 years of age (excluding routine maintenance and repair). The HRA is an attachment to project documentation and is considered by the appropriate decision makers as well as shared with and considered by the project team. The required contents of the HRA are defined further below.

The information and analysis provided in the HRA provides a framework and context to ensure that historical elements of the campus, environmental considerations, and landscape context are preserved, enhanced, and valued. The HRA further ensures that improvements, changes, and modifications to the physical environment may be clearly analyzed and documented.

The College also conducts related processes that ensure consideration of historic resources, including the College's implementation of the State Environmental Policy Act. Through the SEPA process, the College considers the potential impacts of development on historic and cultural resources, including buildings and sites less than 25 years old. SCC's Board of Trustees (BOT) has final review and approval authority for all SEPA determinations as set forth in the Washington Administrative Code which establishes SCC's right for Lead Agency status for SEPA determinations. The BOT reviews the SEPA determination, any HRAs related to the project, and any recommendations from college or other bodies reviewing the project to determine the appropriate action that should be taken to balance all the issues raised by the reviewing bodies.

The Historic Resource Assessment (HRA)

In preparing the HRA, the following information shall be provided to the extent known. Information regarding these considerations may or may not be available or relevant for a proposed development. The HRA shall be appropriately updated as the project evolves prior to final BOT action. For proposed construction that makes exterior alterations to a building or landscape more than 25 years of age or that is adjacent to a building or landscape older than 25 years, information described in the bullets below shall be addressed in the HRA to the extent it is available.

- Age of project building, adjacent buildings, and open spaces
- Information regarding architect, engineers, and contractors (as available) of the original building
- Description of interior and exterior, and site surroundings of the building or campus feature, including the traditional views of the site, if any
- Information regarding the distinctive visible characteristics of an architectural style, or period, or of a method of construction, if any
- Information regarding the roles of the structure, site, and surroundings have played on campus and in the community, if any
- Information regarding the character, interest, or value as part of the development, heritage or cultural characteristics of the campus, city, state, or nation, if any
- Information regarding any association with an historic event with a significant effect upon the campus, community, city, state, or nation, if any
- Information regarding the association with the life of a person important in the history of the campus, city, state, or nation, if any

- Information regarding the association with a significant aspect of the cultural, political, or economic heritage of the campus, community, city, state, or nation, if any
- Information regarding the prominence of the spatial location, contrasts of siting, age, or scale that make it an easily identifiable visual feature of the campus and contribute to the distinctive quality or identity of the campus
- Information regarding the location of the new project, entrances, service, access, and circulation, front/back, bulk, scale, materials, architectural character, profile, open space, and landscape siting, relative to the building or feature older than 50 years, including opportunities to complement the older surroundings and buildings literally or through contrast
- Potential mitigation measures, such as facade treatment, street treatment, and design treatment sympathetic to the historic significance of the development site or adjacent campus feature, if any
- Information in historic resource surveys prepared by outside consultants, if any, and found on the DAHP WISAARD online database
- Seattle Central College is required by the State to submit all projects to the State Department of Archeology and Historic Preservation (DAHP) for review prior to any application for funding. DAHP issues a determination and, if deemed a state resource, mitigation measures.

SECTION 4 – DESIGN GUIDELINES

Introduction The following design guidelines will apply to all projects developed under the approved MIMP. The City of Seattle Land use Code will apply to any requirements not specifically addressed by MIMP. Where any conflicts exist, the MIMP standard will apply.

These campus design guidelines are intended to be supplemental to the Capitol Hill Neighborhood, Pike Pine Neighborhood, and City of Seattle Design Guidelines. *(Guidelines are noted with **Dark Blue italicized text for reference**).* They seek to add additional clarity for projects and improvements developed by Seattle Central College. The development of college properties will benefit and will build on the years of intensive planning efforts the Capitol Hill community has provided previously.

Throughout these guidelines, those noted with the *“Aspirational Guideline”* denotes guidelines that the college seeks to achieve, but for which traditional state funding methods may not support. The college will seek to incorporate these guidelines to the extent possible.

timeline

Explanation of terms:

- Will** The College commits to the guideline as a campus standard requirement.
- Should** The College will strongly encourage the design team to appropriately apply the proposed guideline to the project under development.
- Consider** The college will encourage the design team to appropriately apply a variety of design options that will assist in meeting the broader established guideline.
- Standard** An existing regulatory requirement exists that the college commits to meeting, as a minimum, or exceeding.

Architectural Design and Character:

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

CS2 – Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.

CS3 – Contribute to the architectural character of the neighborhood.

DC2 – Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.

Seattle Central College’s existing campus structures are a collection of diverse buildings. Some constructed originally as academic facilities, some had previous lives as commercial structures and have been renovated to meet academic needs, and others have been acquired by SCC but have had little or no modifications to meet the needs of higher educational functions. As such, there are limited unifying architectural characteristics that tie the campus together visually or physically.

The major issue to be addressed in future development is the best means of conserving the principal assets of the campus while providing for development which respects and improves the existing neighborhood environment at the same time creating a cohesive, unified campus with clear sense of place. Any future development adjacent to, or replacing campus buildings, must reinforce and enhance the college fabric. Consistent design elements **should** be established to provide structure to the college’s street edges, in turn providing improved connectivity between the campus and the surrounding community context.

SCC **will** establish a tradition of design excellence for all future development. While each project will have different characteristics and needs that suggest varying responses, all projects must meet a high level of quality. All projects must respond to context, built form, campus structure and natural beauty. In recognition of the important role SCC will play shaping the character of the surrounding neighborhood, the college **will** continue to inform and involve neighboring community members and groups as major projects are developed. The following are recommended general guidelines to be followed in new development throughout the campus.

General - Campus Wide

Relationship of New Development to Surroundings

Capitol Hill Neighborhood Design Guidelines

PL4.1 – Connections to All Modes: For buildings along corridors that provide direct pedestrian access to light rail station entries and other key transit access points – including: Broadway, 15th, E John St, E Olive St, E Denny Way, E Howell St, E Nagle Place, and 10th Ave below Thomas – locate primary entries to conveniently access transit and consider that secondary entries may also be required to maximize pedestrian access to transit.

- **Consider** the existing or emerging context in order to develop a project, building, and/or landscape/hardscape appropriate to a specific site, the adjacent context, and the college as a whole.
- Valued elements of existing buildings and landscapes/hardscapes **should** be conserved where feasible and their presence enhanced with new development.
- Building design and placement **should** accommodate convenient pedestrian circulation and accessibility.
- Main entrances **will** be clearly identified and relate to the pedestrian circulation system.
- Visual transparency **should** be provided; both from the public ways into campus building; and from the interior out to the community.
- Circulation of all modes of access to a building (including service) **will** not deteriorate the surrounding campus context and open space.
- Building and service facilities **should** be designed to protect adjacent neighbors and open spaces from unpleasant noise, air impurities, or other environmental impacts which preclude use and enjoyment of the area.

Aesthetics

- Building designs **should** represent the highest effective use of public funds and current building technologies.
- Building designs **should** maximize sustainable technologies. i.e., material conservation and reuse, daylighting, sunshades, high performance envelopes, stormwater reuse, energy systems, etc.
- Envelopes **should** be constructed for a 50-year life span.
- Building designs **should** express function in the design concept of a building through form and organization.
- Building designs **should** express the structural rhythm of the structure.
- The College **will** use high quality materials and systems that have an enduring lifecycle, a sense of permanence, and are suitable for a major civic institution.
- At major building entrances, active pedestrian areas **will** be provided between the street frontages and building entrances. Incorporate places of gathering, transition from outside to inside, and protection from weather.
- Entrances to parking garages and service areas **should** use artistic treatments (murals, decorative metalwork, etc) or enhanced landscape treatments to reduce visual impacts
- **Consider** design features that visibly represent and promote the diversity of the Capitol Hill community.
- **Consider** the avoidance of literal interpretations of historically designated buildings when designing new buildings. Additions to existing historically designated buildings may be similar to the existing building.
- Develop detailing that conveys a building's function, contemporary use of technology, and the nature of materials, structure, and systems used. Details **should** also address scale by helping to make the buildings sensitive to pedestrians through providing multiple levels of perception at varying distances.
- Cost-effective, efficient, and easily maintainable facilities **will** be provided.
- The broadest possible spectrum of user disabilities in use of spaces and products **will** be supported.
- Environmental impact **will** be minimized through the development of buildings designed to meet or exceed energy and sustainability standards in accordance with Washington State policy.
- The campus' success and quality depend on buildings and open spaces being conceived in concert. An integrated approach to the design of buildings and open space **will** be encouraged.
- **Aspirational Guideline** – Incorporate bird-friendly design strategies on upper floors of buildings with extensive areas of glass. This can include decorative screens, louvers, or patterns/films integrated into the glass that warn birds before they collide.

Building Materials:

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

DC4 – Use appropriate and high-quality elements and finishes for the building and its open spaces.

- A campus standard material palette **should** be developed to contextually unite all campus buildings. And create a common visual aesthetic. Choose materials that are of a permanent nature, able to age well, and express appropriate craftsmanship in their detailing and application. Material options will vary depending upon the site context.
- Materials **should** be selected that reinforce the pedestrian scale at all locations where pedestrians interact with the building.
- Materials selections **should** favor a warm and natural palette.
- Selected materials **should** discourage graffiti and vandalism.
- Building materials **should** create texture and interest at street level.
- The small red pavers prevalent on the existing campus **should** be avoided and replaced as they create a slipping and tripping hazard.
- Materials and systems **should** be easy to maintain and operate.



Warm brick tones with large expanses of glass create a more modern institutional building – Paccar Hall, University of Washington



Long expanse of brick facade broken up by canted glass protrusions, Eastern Washington University



Gray and white brick create a visual texture, glass with red sunshades span the facade to break-up the massing



Red-brown brick with wood accents above ground level - wood accents reduce the visual weight of the brick



Highlight main building/campus entries with unique material, lighting, signage, colors, etc.



Transparent building entry framed by brick massing

Façade Articulation:

- Existing structures along E Pike, E Pine, and Broadway, generally match the originally platted lots and are characterized by buildings that are 50 – 60 feet wide, or when on two lots are, 100-120 feet in width. The scale of new structures **should** reflect the rhythm of bulk and scale established by this existing context. (I.e., expressions for structural bay spacing in the façade articulation).
- Buildings **should** respond to topography by stepping facades so that floorplates generally match the street grade.
- Large blank walls **should** be avoided.
- High levels of transparency and street activating functions **should** be located at the ground plane.
- Building materials and details that create and articulate building façades that blend with the greater Capitol Hill environment **should** be used. (I.e., the energy of Broadway, the residential character of Harvard, etc.)
- Consider** the use of passive design strategies to reduce building energy use like exterior shading devices.



Historic structures In Pike/Pine show structural bay spacing in facades



Street-level facade and entries step with the street slope

Embrace the desirable characteristics and context of Capitol Hill:

- Projects **should** enhance the character of Broadway, E Pine, and E Pike streets as some of Capitol Hill's most prominent and vibrant public main streets.
- Facades facing Broadway, E Pike, and E Pine streets **should** reinforce the street edge.

Incorporating Art:

Capitol Hill Neighborhood Design Guidelines

CS3.2 – Placemaking: The Capitol Hill Neighborhood is a designated arts and cultural district. Art and culture should reflect the local history and values of the neighborhood and should be well integrated with future developments. Art should be designed for human delight and the celebration of culture, spirit, and place appropriate to its function. Capitol Hill strongly values the intact and positive examples of its physical heritage.

DC2.2 – Integrating Art: Use art to animate the pedestrian realm including blank walls, sidewalks, entrances, walkways, etc. Consider themes and artists that represent the Capitol Hill community.

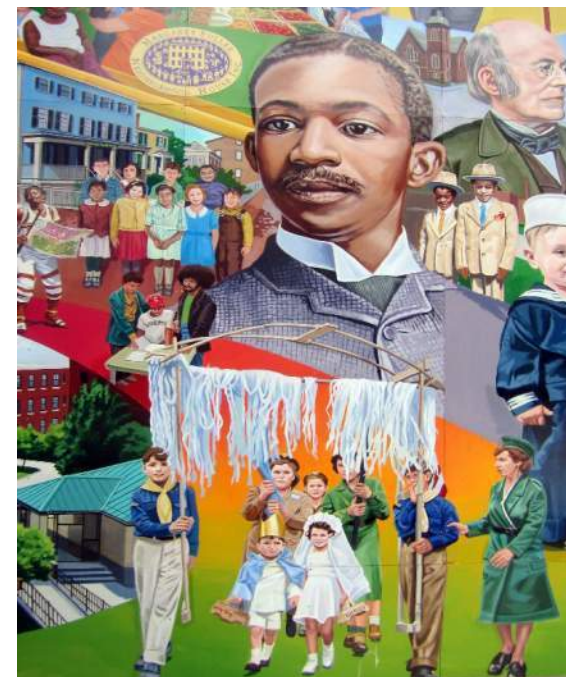
- Provided art **should** match the vibe of Capitol Hill (whimsical, creative, diverse)
- Consider** providing intentional opportunities for the creation of street art.
- The college **will** continue and expand on integrating art and the thinking and work of artists in campus development. Public Art **should** integrate into buildings' architecture.
- Public art **should** be used to punctuate and enrich open space and green space design.



"Park Sculpture" by Charles W. Smith, 1976, SCC Campus



"Wind Cradle" by Ali Baudoin, 1976, SCC Campus



Murals that honor community members and builders



Murals with a template for students to add to.



Temporary and unsolicited art is found in the Capitol Hill Neighborhood. Design new buildings and public spaces to receive art that reflect the context of the neighborhood.



Project Specific Design Guidelines

Broadway Achievement Center (BAC)

- **Aspirational Guideline** – Renovate the southern exterior staircase to be wider/more open to create more visual connections between the South Plaza and Harvard Ave and provide additional site lighting.
- **Aspirational Guideline** – Physically connect the BAC to Broadway Edison Phase II and close off the northern exterior staircase to/from Harvard Ave.

ITEC Building

- Consider designing the Broadway elevation(s) of the ITEC site such that there is a discernable visual break in the building mass that marks the main building entrance and the transition to the E Howell Street Passage. Provide active pedestrian areas between the street frontages and buildings.
- The Broadway façade **should** be highly transparent nature with Street Activating Uses and be a prominent feature of the building design. This **should** extend to the SE corner transition to the E Howell Street Passage.
- **Consider** using the building corner at the E Howell Street Passage, and the street crossing access to Cal Anderson Park as a transition point of building character, scale, and mass.
- Protected pedestrian walkways for a minimum of 50% of the frontage **will** be provided.
- **Aspirational Guideline** – Renovate the west-half of the Howell Street Passage (between the SAM and Edison buildings) to create a continuous accessible pathway for all users.



Transparent facade gives a view into activity within - activates the street and promotes college programs, Point Park University



Transparent skybridge offers visual connections in and out of building

Student Housing **Pike/Pine Neighborhood Design Guidelines**

CS2 – Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.

II.i – Site Characteristics: Massing and articulation should respond to the established Pike Pine development pattern of street facing façade widths and mid-block depth.

- The student housing entrance **should** be highly transparent nature and be a prominent feature of the building design.
- Building design, site and setbacks **should** visually integrate the Harvard frontages with the adjacent multifamily residential context abutting the properties.
- **Consider** designing the Student Housing building site such that there is a discernable main student/building entrance separate from the retail/commercial/parking garage.
- The E Pine Street façade **should** be of highly transparent nature with Street Activating Uses and be a prominent feature of the building design. Provide protected (covered) pedestrian walkways for a minimum of 50% of the E Pine Street frontage.
- Amenities and retail opportunities that are tied to the mission of the College **should** be incorporated along the E Pine Street Frontage. These spaces **should** step with the grade change on E Pine Street.
- Vehicle access into and out of the parking garage **will** be located on Boylston Avenue only and removed from Harvard Avenue.

- SCC **will** work with the City of Seattle jurisdictions to support the development of traffic calming, and pedestrian crossings consistent with a pedestrian friendly environment along Harvard and E Howell streets.
- The design of the façade **should** align with the character of adjacent historic “auto-row” buildings.

Student Union

- **Consider** designing the Broadway face of the Student Union site such that there is a discernable visual break in the building mass that marks the Student Union Plaza, and the pedestrian pass-through to Cal Anderson Park.
- The College **should** provide an enhanced, publicly accessible midblock pedestrian connection from Broadway to Cal Anderson Park
 - **Aspirational Guideline** – Provide an ADA-approved pathway via a ramp or elevator/lift.
- The Broadway façade **should** be highly transparent nature with Street Activating Uses and be a prominent feature of the building design.

Capitol Hill Neighborhood Design Guidelines

PL1 – Complement & contribute to the network of open spaces around the site & connections among them.

1.a – Parks: Design buildings facing a part of P-patch to enliven and enhance the safety of the open space. Orient entries, windows, balconies, decks, and other amenity spaces to face the park. Design buildings facing Cal Anderson Park with active street level uses to support and reinforce its role as the “front yard” and civic square for Capitol Hill

- The design of the Cal Anderson facing façade **should** enliven and enhance the safety of the adjacent space. Orient entries, windows, decks, and other amenity spaces to face the park.
- The design of the street-level façade on Nagle Place **should** include active street level uses to support and reinforce its role as an active participant in the park.
 - See “Street Level Activation and Uses”



Active interior spaces/functions like exercise rooms and lounges located at windows, and exterior balconies on the upper levels create can enliven adjacent outdoor space and make the outdoor spaces feel safer

Harvard Building, I and Harvard Building II

- **Consider** designing the Harvard and E Howell building corners such that there is a discernable visual break in the building mass that marks the main building entrances.
- The building entrances **should** be highly transparent nature and be a prominent feature of the building design.
- **Consider** designing the building corners at the pedestrian crosswalks to the E Howell Street Passage as a transition point of building character, scale, and mass.
- Building design, site and setbacks **should** visually integrate the E Howell and Harvard frontages with the adjacent multifamily residential context abutting the properties.
- SCC **will** work with the City of Seattle jurisdictions to support the development of traffic calming, and pedestrian crossings consistent with a pedestrian friendly environment along Harvard and E Howell.

District Energy Plant

- SCC **should** renovate the South Plaza to the greatest extent possible including, but not limited to; replacing brick pavers with paving that is more slip resistant; removing the sunken lawn area between the existing Broadway Performance Hall (planned Broadway Achievement Center) and main plaza by bringing the entire area to the same level.
- Open space improvements/amenities **should** provide weather protected areas so the plaza can be enjoyed in inclement weather.
- **Aspirational Guideline** – Provide ADA-approved ramp access to the plaza from Harvard Ave.
- **Aspirational Guideline** – Create educational opportunities about sustainable energy by providing publicly visible expressions of the plant's features and functions.

Open Space Design Guidelines

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

DC3 – Integrate open space design with the design of the building so that each complements the other.

Seattle, and Capitol Hill Neighborhood Design Guidelines

PL1 – Complement and contribute to the network of open spaces around the site and the connection among them.

Capitol Hill 3.c.2 – Pedestrian Amenities: Provide functional pedestrian amenities such as benches (that enrich and enhance pedestrian flows).

PL2 – Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

General - Campus Wide

- Student, faculty, and staff usability of open space **will** be prioritized over public usability.
- Preservation of public access and use is essential and **will** be maintained.
- Circulation between places on campus **should** be safe, convenient, direct, and visually attractive.
- The campus landscape/hardscape **should** unify the campus through complementary palettes of planting, street furniture, paving and other built elements.
- New and renovated open spaces **will** be designed to be inclusive of the diversity present in Capitol Hill and not intentionally exclude any people or groups.
- Open spaces **should** complement and contribute to the network of existing campus open space and the connections to the greater Capitol Hill neighborhood.
- Open spaces **will** use paving materials that are slip resistant and appropriate for the climate and desired use of the space.
- Open spaces **should** provide variety in terms of shade and direct sunlight.
- Bike storage **should** be provided and designed to not detract from the quality and functionality of open space or building entries.
- Connectivity. All open space development **should** utilize design approaches that provide pedestrian links between campus entries, campus building entries, major pedestrian streets, Cal Anderson Park, Sound Transit Link stations, Seattle Streetcar stations, and King County Metro bus stops.
- Minimize the impact of light and glare on surrounding buildings and spaces while keeping the needs of safety and security in mind. Open spaces **will** include supplementary pedestrian lighting strategies in addition to that required for public safety. (See Lighting guidelines for additional information)
- Open spaces **should** have multiple entry/exit points – avoid dead-ends or one-way-in/out spaces.
- Open spaces **should** include gateways, bollards, landscaping, or other site features that define the extents of the college grounds. See Campus Identity guidelines for additional information.
- Edges of open spaces **should** include impediments such as gateways, bollards, landscaping, or other physical site features to reduce high-speed travel via human- or engine-powered modes.
- Site furnishings for student and community use **will** be provided.
- Small level changes in open spaces **should** be avoided – larger, more unified open spaces are preferred.
- **Consider** providing infrastructures (power, water, lighting, built elements) that foster flexible and temporary uses. (Impromptu gatherings, special events, pop-up retail, etc.)
- **Consider** taking advantage of any grade changes to create transitions that can be used for seating or other amenities.

- Outdoor space **should** include covered areas and amenities to encourage use during inclement weather.
- When opportunities are available to improve underutilized open spaces, redevelopment **will** prioritize the needs of students, faculty, and staff. To assure the redeveloped spaces are transformed to high-quality, attractive, and accessible public space, the design guidelines included throughout this document **will** be utilized.
 - I.e., E Howell Street Passage, the sunken area at the South Plaza (redeveloped so it is no longer sunken).
- In open spaces, lighter-colored hard surfaces and intermittent landscaped areas **should** be used to reduce urban heat island effect.



Build seating off of existing site features (brick bulkheads)



Built-in site furnishings with simple forms; paving materials indicate clear circulation path



Stepped plaza transitions grade and offers different amenities



Covered outdoor space adjacent to open space provide opportunities for using space during inclement weather



An identifiable palette of plantings and site furnishings to reinforce the college district

Green Space Design Guidelines

Seattle and Capitol Hill Neighborhood Design Guidelines

DC4 – Use appropriate and high-quality elements and finishes for the building and its open spaces.

Capitol Hill 4.a – Beneficial Plants: Use plant species that are suitable for site conditions, climate, and design intent. Maximize use of native and/or naturally growing (non-invasive) plants that are self-sustaining, low maintenance, drought and pest resistant, and durable in urban conditions. Encourage the use of pollinator plants and those that provide wildlife and avian habitat appropriate to the region. Avoid invasive species that may jeopardize local ecosystems, or species that require the use of petrochemical fertilizer or pesticides.

Capitol Hill 4.b – Diversity: Plant diversity provides resistance to insects, diseases, and pests. As a general guide for larger sites, plant no more than 10% of any species, no more than 20% of any genus, and no more than 30% of any family.

- Green space **should** have multiple entry/exit points – no dead-end or one-way-in/out spaces.
- Underutilized green spaces **will** be redeveloped/reprogrammed when funding is available.
- When opportunities are available to improve underutilized green spaces, redevelopment **will** prioritize the needs of students, faculty, staff, and community at large. To assure the redeveloped spaces are transformed to high-quality, attractive, and accessible public space, the design guidelines included throughout this document **will** be utilized.
- Planting design and maintenance **will** support personal safety.
- Create a palette of plantings and trees to reinforce the college “district.”
- Plants and groundcover that is drought tolerant, climate adaptive, and promotes urban habitat **should** be used.
- All landscape **will** utilize low-maintenance plants and groundcover. Open lawn areas **should** be minimized.
- Use stormwater treatment strategies to greenify the campus and mitigate stormwater runoff.
- **Consider** public art that integrates rainwater capture.
- When existing trees (including Heritage and Exceptional trees) are affected by site work, they **will** be reviewed regarding their suitability in the space and how they frame/define adjacent spaces. City guidelines for preservation/replacement/mitigation **will** be followed.
- Campus Landscaping and right-of-way improvements **should** support urban wildlife by creating new habitats for insects and birds through design and planting for green roofs, walls, and planting beds. Maximize the use of native plantings.



Landscaping that includes pollinator and native plants



Existing Glen at South Plaza dead-ends at E Pine and Harvard - avoid this condition



Stormwater mitigation facilities (right) with green space for play (left)



Street trees with planting beds enhance the sidewalk streetscape



Fixed, raised planting areas protect vegetation, integrated seating for pedestrians



Modular site furnishings provide seating and planting beds

Pedestrian Circulation

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

PL2 – Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

Capitol Hill Neighborhood Design Guidelines

CS2.1.a – Broadway:

- Consider active pedestrian transition areas between the street level building façade and sidewalk for outdoor café seating and walk-up windows.
- Enhance visual connections and pedestrian flows to and from the Capitol Hill light rail station as well as the Seattle Central College campus.

Central Campus Crossing

The primary entry point to SCC is located at a major entrance to the Broadway Edison complex. This entrance accesses the primary academic and student service functions of campus. Improvements are recommended to the central campus crossing located at the Broadway pedestrian crossing between the main entrance to the Broadway Edison Complex and the Mitchell Activity Center/Bookstore (the site of the Planned Student Union project). This is an important crossing because it links the main academic building (Broadway Edison Complex) with student activity services at the Mitchell Activity Center and Bookstore/Student Leadership Building; is a major link to Cal Anderson Park, its play fields, and courts; and the commercial services on the east side of Broadway. Opportunities **should** be sought to create an identifiable “Central Campus Crossing” that clearly links pedestrian access between academic space, student services and activities, commercial services, and Cal Anderson Park.



Portland State Campus Center intersection with MAX Light Rail line.



Pedestrian Street Crossings

Highly utilized street crossings are currently located at signalized intersections or well-marked un-signalized intersections. These crossing points link the main campus with pedestrian oriented commercial uses on Broadway and to the Pike/Pine neighborhood. There is also a need to improve the pedestrian crossings along Harvard Avenue. Structured crossing improvements coupled with traffic-calming measures **will** reinforce pedestrian/vehicle safety. Improvements **should** be sought to better define the pedestrian paths adjacent to the vehicular access.

SCC **will** work with SDOT and other City of Seattle jurisdictions to support the development of traffic calming, and pedestrian crossings consistent with a pedestrian friendly environment at all crossings. **Consider** pavement treatments, landscaping, lighting fixtures, and other elements that indicate the spaces are shared among pedestrians, cyclists, and motor vehicles. Particularly areas of emphasis are:

- Intersection at Harvard and E Pine
- Intersection at Harvard and E Howell
- Intersection at E Howell and Broadway
- Mid-block crossing of Broadway between the MAC/Bookstore and the main BE Complex entrance.
- Mid-block crossing of Nagle Place between the planned Student Union and Cal Anderson Park



Chicanes can help slow and calm vehicle traffic to create safer streets for pedestrians



Raised crosswalks improve pedestrian safety and accessibility.



Bollards located at busy sidewalks, intersections, and building entrances provide additional protection for pedestrians



Crosswalks with contrasting material enhance visibility

Streetscape Improvements

Enhancements to the pedestrian circulation network **will** be made to better integrate the campus into the community fabric, and to create a more pedestrian oriented scale.

As building projects are developed along a public right-of-way, the following streetscape improvements **will** also occur when appropriate and feasible:

- Signage along campus edges **should** support wayfinding and contribute to the character of the street.
- The selection of street furnishings **will** contribute to the uniformity of the street character; these may include lighting, benches, garbage and recycling receptacles, bicycle racks or other bicycle parking, and information kiosks.
- Where transit services (Seattle Streetcar and Metro Bus) run adjacent to SCC properties, the college **will** strive to integrate transit stops into the fabric of the streetscape and provide street features to encourage transit ridership such as awnings for protection from weather and areas for public seating.

Sidewalk Improvements

SCC **will** work with SDOT and other City of Seattle jurisdictions on the planning, design, and construction of sidewalk improvements. Special sidewalk and landscape treatments **will** help delineate pedestrian spaces and elevate the quality of the pedestrian environment; this may be accomplished through:

- Landscape improvements including planting beds, rain gardens, and trees.
- Pavement improvements including special treatment of crosswalks or other special pedestrian areas through the use of distinguishing paving materials, stamped or colored concrete, or permeable pavement.
- College buildings with frontages on Broadway and E Pine Street **will** provide overhead weather protection above sidewalks for at least 50% of the street frontage. Buildings on other frontages **will** provide overhead weather protection at entries and **should** provide overhead weather protection along other areas of the frontage.
- **Consider** providing bollards at building entrances, edges of open spaces, and adjacent to curb cuts for additional pedestrian safety and protection from vehicles.

Universal Accessibility

- Accessible routes **will** be provided along all public edges and pathways.
- Paving materials that minimize the risk of injury in wet/freezing conditions **will** be used.
- See Universal Design Guidelines for more information.

Inclusive Neighborhood

- **Consider** design features that visibly represent and celebrate the diversity of the Capitol Hill and Pike Pine neighborhoods so that the college environs contribute to a welcoming, supportive, safe, and inclusive public realm.

Street Level Activation and Uses

This section articulates a vision for how SCC – along with the city, commercial building owners, and neighborhood involvement – can enhance the urban fabric of the campus that also provides benefits to the surrounding neighborhood. Broadway serves as an important retail corridor and pedestrian destination for the city. The Broadway corridor provides many opportunities to connect the College to the surrounding neighborhood and to create a district comprised of both College and non-college uses. This section details several strategies and design guidelines that SCC can pursue to enhance the vibrancy of its neighborhood. The improvements described would be added adjacent to new development or in conjunction with major renovations of existing buildings as funding for projects occurs and is feasible.

Improvements to campus boundaries and open spaces are critically important to supporting strong physical connections between the SCC campus and the surrounding neighborhoods. All improvements in or impacting public right-of-way **will** be developed consistent with the Seattle Department of Transportation (SDOT) requirements. Some of the improvements include crosswalk enhancements at E Howell and E Olive Streets; streetscape improvements along Broadway; traffic calming along Harvard Avenue; enhancements to existing and new open spaces; and the creation of new campus gateways.



College/public shared streetscape
New York University, New York

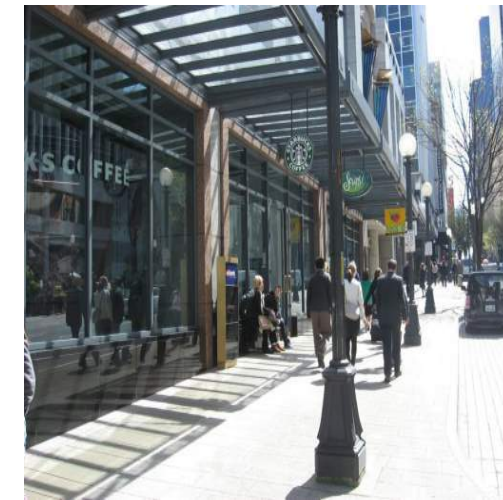


College/public shared streetscape
Rochester Institute of Technology

In general, the plan seeks to increase the permeability of campus, activate building frontage and streetscapes, and improve safety. Enhancements to the main central campus crossing on Broadway between the main entrance to the BEC complex and the Bookstore/MAC are proposed. Major pedestrian gateways **will** be created including the entrances along Broadway near E Pine and E Denny streets. Perimeter landscaping and street trees **will** be provided along the street frontages of new developments and substantial renovations as described in the Development Standards chapter. Design guidelines for campus improvements are outlined below.



Use a combination of stairs & ramps to provide universal access on campus



Transparent/ translucent sidewalk canopies offer protection from rain while allowing sunlight to shine through.



Facade setback at ground level creates extra space for pedestrians



Seating with different orientations and sizes near main entries



Fitness Centers have many users moving through the space throughout the morning, day, and night.

Community Service and Retail Uses

SCC recognizes the important contribution of retail and commercial spaces to the vibrancy of the Broadway and Pike/Pine corridors. Coffee shops, restaurants, cafes, retail stores, and other services generate pedestrian activity, enhance the street experience, and provide walkable destinations for residents and visitors. College development projects **should** include provisions for retail type functions at street level based on project location and context. In addition, new developments also create opportunities to provide rooftop terraces which can help elevate the energy of the corridor and provide 'eyes on the street' that enhance public safety.

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

PL3 – Encourage human interaction and activity at the street level with clear connection to building entries and edges

- Create opportunities for retail/commercial uses (where appropriate).
- Street furniture for College and Community use **will** be provided. Include in areas to promote activity, and in locations that offer respite from the bustle of busy streets.
- **Consider** providing functional/interactive art in open spaces.
- **Consider** activating blank facades with art or installations like murals or banners.
- Canopies or cantilevered structures **should** be provided at walkways along frontages with high levels of pedestrian traffic to provide weather protection.
- **Consider** enhancements to the pedestrian environment thought inclusion art, societal, and other placemaking features.

Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

DC1 – Optimize the arrangement of uses and activities on site.

- Structures with street frontage facing E Pike, E Pine, or Broadway **should** orient active street-level uses on these streets. The uses **should** be transparent with visibility into and out of, the structures. Uses **should** include highly activated functions that bring energy and interest to the street. The College **will** explore the following uses for street level spaces:
 - Campus Retail (bookstore, coffee shop, bakery, bistro)*
 - Food services*
 - Student lounges*
 - Gathering spaces*
 - Meeting spaces (student, college, community)*
 - Academic Program Exhibition (makerspaces, digital sandboxes, art gallery, etc.)*
 - Fitness Centers*
 - Public Safety Offices*
 - Performing Arts Venues*
 - Community Service Centers*
- **Aspirational Guideline** – *When appropriate, provide College outreach functions, community services, or opportunities for small storefront businesses.*



PLU Bookstore is part of urban shopping district and is available for public use.



Bikestation, Downtown Seattle



Public plaza with services at Rochester IT.

Lighting

Appropriate lighting levels **will** be a primary means of making a campus feel safe and inviting and facilitating its use beyond daylight hours. It **will** be used to elevate and enhance the quality and character of space by providing attractive architectural or artistic design form during the daytime, and a variety of ambient levels during the evening. The campus lighting strategy **will** be multi-level to create a hierarchy of lighting for different spaces and uses including:

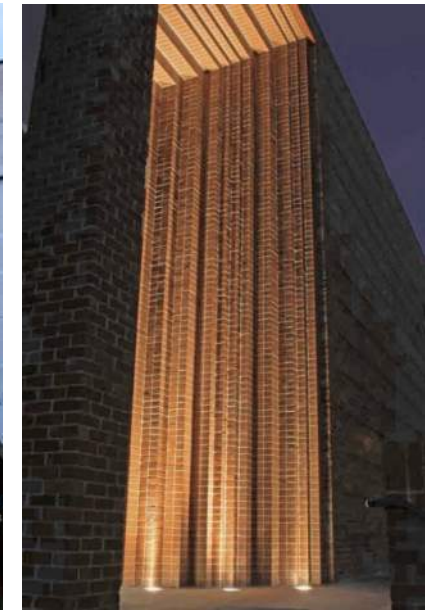
Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines

DC4 – Use appropriate and high-quality elements and finishes for the building and its open spaces.

Capitol Hill 2.d – Lighting: Use directional down-lighting and other dark-sky friendly lighting strategies to enhance the perception of safety and minimize light pollution. Avoid outdoor lighting with high blue light content or other attributes that could adversely affect wildlife behavior and reproduction. Use low-wattage, warm tone lighting wherever possible and diffuse exterior light to make it more consistent with the context.

- Campus street frontages, internal pathways and open spaces **should** be well-lit to create a sense of safety and security.
- Pedestrian-scale lighting improvements **should** be provided along façades, streets, and sidewalks to promote nighttime activities and safety. This applies to new and existing developments.
- Lighting design **will** minimize light pollution. Dark sky lighting standards **should** be used to be in keeping with achieving a sustainable design approach.
- Energy-efficient lights **will** be installed throughout the Campus to minimize energy usage.
- Lighting design of open spaces **will** be carefully chosen to complement the use and character of the space and to enhance the unique elements and landscapes within.
- Pedestrian scale lighting **will** be used within open spaces and walkways.
- The choice and style of light fixtures **should** contribute to building campus identity and creating a quality environment. The fixtures **should** complement the architecture and landscape and read as part of an overall design palette of the Campus environs.
- Lights selected for illuminating paths, open spaces, and providing general visibility **will** have warm tones. Lights with cool tones/high blue-light content **will** be avoided.
- Pedestrian street crossings **should** have additional lighting to increase visibility and safety.
- **Consider** the use of Threshold Illumination – additional lighting at main building entrances, plaza/ open space entrances, and pedestrian pathways.
- **Consider** the use of Accent Illumination – illumination of artwork, murals, and gathering spaces within larger plazas/open spaces.
- **Consider** the use of Artistic / Pop Illumination – lighting to create visual interest on building facades, sidewalks, and/or in plazas.
- Improved sidewalks, open spaces, and other exterior areas **will** adhere to appropriate site lighting levels (fc = foot-candles)

Campus perimeter (non-pedestrian areas)	0.2-0.5 fc
Pedestrian walkways and building entrance/exit	2.0-3.0 fc
Vehicle entrances	2.0 fc
Building perimeter (pedestrian walkways and open site areas)	1.0-2.0 fc
Building entrances	5.0-10.0 fc
Service yard areas	0.2 fc



Lighting in window wells, alcoves, and main building entrances



Building-mounted lighting brightens the sidewalk



Tall fixtures effectively illuminate a large area



Bollard fixtures can provide direct, low-level light



Integrated site lighting illuminate's paths

Campus Identity

As Broadway has become more developed in recent years, and with the transit access points of Sound Transit and the Seattle Streetcar, there is now an opportunity to create a district identity that strengthens the important relationship between the college and the neighborhood. The identity of this district can be determined by establishing SCC district gateways. This plan proposes the creation of campus gateways adjacent to the Sound Transit station to be located near Broadway and E Denny, the mid-block crossing on Broadway between the Broadway Edison complex and the Bookstore/MAC, and at the planned Student Housing project on the corner of Harvard Ave and E Pine St. These campus gateways **will** communicate the importance of Broadway as a vital pedestrian link between the Broadway Business District and the Pike/Pine Corridor as well as the role that SCC plays in the city. The campus gateways may be distinguished by special hardscape and landscape treatments, signage, lighting, pedestrian amenities, and art. Gateway design guidelines include:

- Gateways, column-motifs, bollards, landscaping, or other significant physical feature(s) **should** be used to reinforce campus identity and extents.
- The design of main campus entries **will** be clear and distinguishable from minor/student-only entries.
- Branded signage **will** be provided and **will** reinforce the College district as a unique space in Capitol Hill.
- Wayfinding signage **will** be on sidewalks, open spaces, campus edges, and transit stops to direct students and guests.
- **Consider** designing signage and wayfinding systems using symbols/icons instead of English words or phrases so non-native English-speaking students and members of the community can more effectively navigate campus.



Open gateways, campus-identifying art, and other features define the extents of the college grounds.



Branded wayfinding markers



Wayfinding/branding embedded into building or pedestrian surfaces

Sustainability *Seattle, Capitol Hill, and Pike/Pine Neighborhood Design Guidelines*

CS1 - Use natural systems and features of the site and its surroundings as a starting point for project design.

Encourage healthy and sustainable lifestyles.

- Short-term bike parking **will** be located near building entrances.
- See Transportation design guidelines below for more information.

Energy Use

- At a minimum, all new buildings **will** meet state/city **standards** for sustainability of public facilities (LEED Silver, Washington State Energy Code – Commercial, City of Seattle Energy Code, etc.) where **standards** conflict, the more stringent **standard** will apply.
- **Aspirational Standard:** *When dedicated funding is available, endeavor to meet higher standards of sustainability such as:*
 - *Washington State Executive Order 20-01 – State Efficiency and Environmental Performance. (Zero Energy - Capable, Zero Energy)*
 - *Living Building Challenge*
 - *Core Green Building Certification*
 - *2022 Washington State Clean Buildings Bill (Clean Buildings Performance Standard)*
 - *Passive House Certification (Passive House Institute or Passive House Institute US)*
- New buildings **will** incorporate building-integrated renewable energy generation.
- **Consider** providing publicly visible expressions of sustainable energy use and conservation measures.

Water

Capitol Hill Neighborhood Design Guidelines

CS1.5 – Water Features:

5.a – Consider sustainable design opportunities such as shared water systems for rainwater harvesting, greywater reuse, and blackwater processing/reuse. Reduce flows into the municipal stormwater system through stormwater management, green roofs and walls, and swales. Consider other functional solutions for sustainable water reuse and/or drainage that work well with the neighborhood’s soil condition and topography.

5.b – Design landscapes that reduce potable water use for irrigation such as via the following strategies:

- *Reuse captured stormwater, greywater, HVAC blowdown or condensate for irrigation.*
- *Specify plants, soils, and other features to be self-sustaining with natural precipitation only.*
- *Design planting zones so that plantings no longer require irrigation once established.*

Wherever feasible, SCC **will** pursue sustainable strategies in the rights-of-way adjacent to college properties. Some examples include rain gardens, pervious pavement, and increased tree canopy. Right-of-way improvements shall be consistent with the City of Seattle’s - Right-of-Way Improvements Manual, which strives “to balance the access and mobility needs of all users of the street right-of-way: pedestrians, non-motorized vehicles, automobiles, transit, and freight.”

- **Consider** providing publicly visible expressions of water conservation measures.
- Projects **will** reduce stormwater flows to the municipal systems. **Consider** strategies like GSI (Green Stormwater Infrastructure).
- **Aspirational Guideline** – *Provide rainwater harvesting, greywater reuse, blackwater processing/reuse, centralized shared water cisterns. Provide for potential expansion with adjacent projects/improvements.*
- **Aspirational Guideline** – *Reduce flows into the municipal water system through stormwater management of building green roofs and walls.*



Examples of green streets utilizing rain gardens to manage stormwater.

Lighting

- Lighting design **should** provide adequate illumination while minimizing light pollution. Dark Sky lighting guidelines **should** be used to be in keeping with achieving a sustainable design approach.
- Controlled Daylighting (windows, skylights, sunshades, window shading, light shelves, etc.) **should** be used to optimize natural light and reduce energy needs and consumption.

Transportation **Seattle/Capitol Hill Neighborhood Design Guidelines**

PL4 – Incorporate design features that facilitate active forms of transportation such as walking, bicycling and use of transit.

- The College **will** actively work to exceed the requirements of the Transportation Management Plan.
- The College **will** provide highly visible bike parking facilities near main campus entrances and supplemental bike parking near student entrances. Some bike parking **should** include canopies to protect bikes from rain.
- Short-term bike parking **will** be available for use by the public.
- Short-term bike parking facilities **will** use durable, high quality infrastructure and strive to remain compact while providing reasonable maneuvering space.
- Signage **will** be provided around building entrances and major intersections/corridors (like the South Plaza and E Howell St Passage) indicating where short-term and long-term bike parking is located.
- In the new parking facilities included in the Student Housing and ITEC projects, the College **will** provide secure bicycle storage (individual lockers and/or group lockers/rooms) for students, faculty, and staff. Charging infrastructure for e-bikes **will** be provided in these spaces.
 - **Aspirational Guideline** – Provide secure bicycle storage and e-bike charging infrastructure for public use.
- The College **should** advocate for initiatives that support safe streets for pedestrians, like the Stay Healthy Streets / Blocks program.
- Stair access ramps or runnels **should** be provided at exterior stairways so people can roll their bikes up and down the stairs.
- The College **should** work with City of Seattle jurisdictions to implement designated areas for parking bicycle-share and scooter-share modes.
- SCC **will** work with the SDOT and other City of Seattle jurisdictions to support the development of protected bike lanes.
- SCC **will** work with SDOT and other City of Seattle jurisdictions to support the development of safe intersections and reduce bicycle and pedestrian collisions.
- **Consider** reserving vehicle parking spaces for contractors and vendors to mitigate contractor and vendor vehicles getting parked in the E Howell Street Passage.



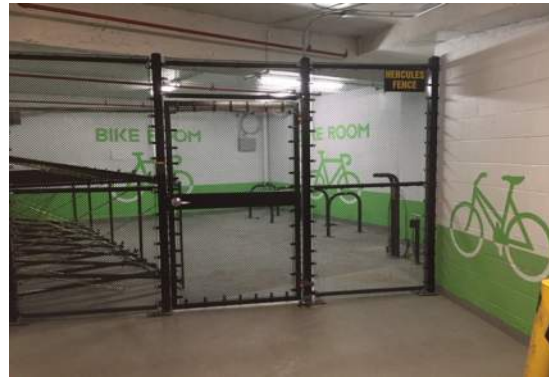
Public transit and ride-share information kiosks



Designated parking areas for bike and scooter share modes



Bike lockers for public use



Secure bike storage in parking garages

Campus Safety and Security Guidelines

Campuses carry high expectations regarding the safety of their diverse user population. A failure to provide the expected degree of safety (risk level) and comfort (fear level) will jeopardize the institution's image as a safe haven for learning. Consequently, enhancing security **should** be both a goal and byproduct of any campus development.

Safety is a concern of any planning exercise for public use. Both the layout and clarity of the campus play a physical role in enhancing the well-being of diverse groups of people including people of color, with disabilities, the elderly, foreign students, and students where English is a new language. The college is a center for diversity. It is a collection of many people from many places.





Implementation of Safety and Security Design Strategies

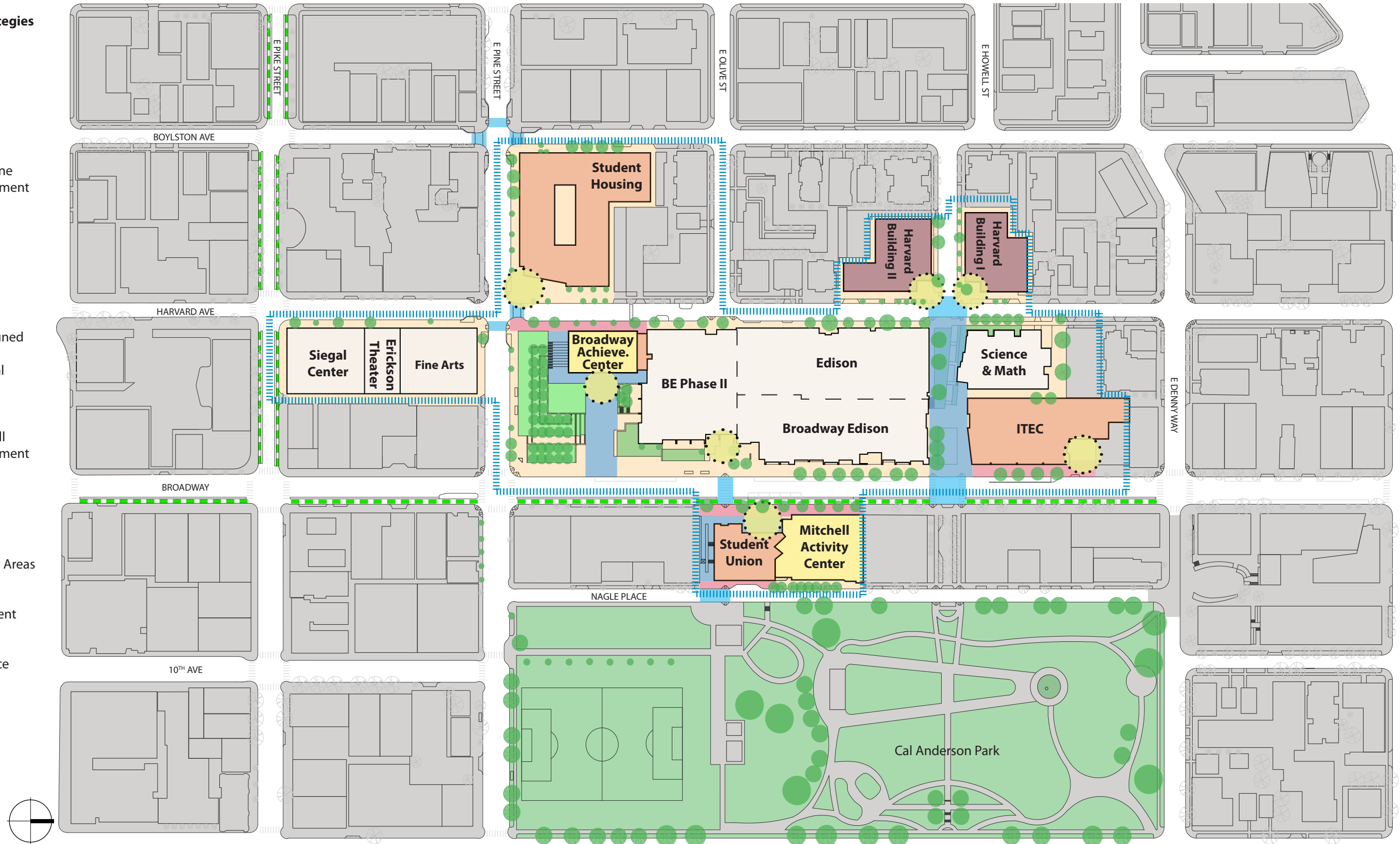
All Planned and Potential projects **should** utilize the Safety and Security design strategies to the greatest extent reasonable. However, surveys of existing campus, discussions with campus staff, and comments from the community have noted specific areas of concern. Planned and Potential projects will address many of these areas of concern by applying strategies as indicated on the following diagram. **See Figure 4-4.1 – Safety and Security.** Where safety and security improvements occur in the ROW (street frontage and street crossings), the College **will** coordinate planning, design, and construction of improvements with Seattle Department of Transportation (SDOT).

FIGURE 4-4.1 – SAFETY AND SECURITY

Safety and Security Design Strategies Implementation Locations

- ITEC Building**
 - Campus/Building entry
 - Streetscape at Broadway
 - Howell passageway
- Student Housing**
 - Building Entry
 - Streetscape at Harvard and Pine
 - Pedestrian crosswalk development at Pine and Harvard.
- Broadway Achievement Center**
 - Building entry
 - Streetscape at Harvard
 - South Plaza improvements
- Student Union**
 - Building entry
 - Streetscapes at Broadway
 - Streetscape at Nagle (City defined Greenway)
 - Pedestrian passageways to Cal Anderson (North and South)
- Harvard Buildings I and II**
 - Building Entry
 - Streetscape at Harvard, Howell
 - Pedestrian crosswalk development at Howell and Harvard.

- LEGEND**
-  Campus/Building Entry Areas
 -  Streetscape Development
 -  Passageway/Open space improvements
 -  Street Crossings



Federal Requirements

Title IX

Seattle Central's operations regarding safety and security are based upon compliance with all aspects of Title IX, which requires that preventative policies be in place and training is presented on a recurring basis and within the scope of the law to prevent sexual harassment and violence on campus. Title IX also prescribes the way the College conducts internal investigations, subsequent actions taken by the college to ensure incidents are resolved, and measures put in place to prevent any further occurrences between the involved parties.

Clery Act

Seattle Central College maintains compliance with the Clery Act, which requires the College to report on security policies and to collect, maintain, and report crime statistics that are included in the annual security report.

Design Strategies

Natural Surveillance

The incorporation of natural surveillance on the SCC campus can impact the safety of the campus and Capitol Hill Community. Campus development must promote design features that maximize visibility of people, pedestrian walkways and building entrances: doors and windows that look out on to streets and parking areas; pedestrian-friendly sidewalks and streets; front porches; and adequate nighttime lighting. Primary to successful natural surveillance at SCC is building transparency so that a building's internal activities can overlook public areas, giving people the ability to see where they are going as well as to inform others that people can see them – to See and Be Seen.

Successful natural surveillance design strategies to be applied to all campus development **will** include:

- Visibility - Open visibility into and out of open stairways, building emergency exits, service areas, etc.
- Transparency - High levels of building transparency at the ground level of all buildings, particularly when they abut public walkways, stairwells, building entries and exits, and service areas.
- Activity - Provide open activity areas (seating, gathering, and cultural spaces) immediately adjacent to building entrances/exits.

The goal of Natural Surveillance is to reduce the opportunity for unwanted interactions.

Territoriality

The use of territory definition is a key element in signaling to visitors that they are entering the environs of Seattle Central College and that it is a safe and secure environment.

Defining campus space from public space is a delicate balance. Distinctive territorial indicators can be accomplished in numerous ways. Territoriality design strategies to be applied to campus development **will** include:

- Landscaping – Use distinctive and unique plantings that can be applied across the extents of campus)
- Paving – Replacement and/or extension of the existing distinctive red pavers
- Signage – Building signage, district boundary markers, security.
- Lighting – Use distinctive and unique lighting solutions.
- Site Furnishings - Provide distinctive and unique seating, planter boxes, fencing, etc.)

The purpose of territorial definition is not to stop unwanted behavior but to deter it. Definition of the campus environs conveys the message to students and staff that this area is their home. This sense of ownership then supports a shared proactive approach in concert with the college public safety department to maintain a safe and secure environment.

Maintenance

Properly maintained buildings and grounds are an expression of care and concern not just to college students and staff, but also to the larger community. Deterioration indicates less control by the college and indicates a greater tolerance of disorder. One of the greatest challenges for Seattle Central College is preventing and cleaning of vandalism. The more quickly vandalism is removed, the less likely it is to

be repeated. The college has instituted several strategies across campus which will be extended to all new project development:

- Sacrificial films provided on all ground level glazing. These protective films on glass surfaces create an affordable means to protect glass from etching and painting.
- Anti-graffiti coatings applied to masonry/concrete/stone surfaces. These coatings make the removal of paint easy and preserve the intended finishes.
- Maintenance contracts are in place with outside vendors to provide rapid repairs of vandalism and other damage. Specifically, Seattle Central College has existing contracts for glass replacement and graffiti removal.

Clear Pedestrian Arrival, Drop-off, and Transitions to Transit

- Develop vehicular drop-off areas with clear connections to major paths and building entrances.
- Drop offs **should** be well-lighted with clear signage to find major destinations – see Lighting Design Guidelines.

Pedestrian Pathways

- Include clear paths of travel from all parking/transportation areas to building entrances.
- Provide clear routes amongst all major activities.
- Locate facilities with nighttime activities along major pathways.
- Connect campus pathways to city trails, sidewalks, and transportation routes.

Signage

- Mark parking entrances from main roadways.
- Unify campus with a campus-wide, consistent approach to signage.
- Signage **should** reinforce path hierarchy.
- Develop signs for a diverse population. Make signs more visual/universal than language based.
- Indicate locations of bicycle parking from streets and open space. Provide signs for different types of bicycle parking (short-term racks, individual lockers, group lockers/rooms, etc.)

Lighting, Day, and Night Use

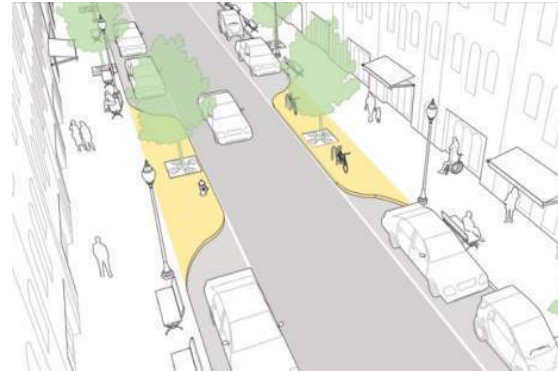
- Develop lighting for paths with connections to overall path hierarchy.
- Unify campus with consistent lighting types and locations.
- Light campus with poles and bollards rather than by lights on buildings.
- Provide emphasized lighting at building entries.
- Emphasize vehicular drop-off areas with higher light levels.
- Provide lighted paths from parking to building entrances for nighttime use.

Aspirational Guideline Traffic Calming on Harvard Avenue between E Pike and E Denny

The following pictures represent a concept for improving the amount of green space and the pedestrian character of college property along Harvard Avenue. Potential street narrowing and traffic calming along Harvard Avenue between E Pike Street and E Denny Streets, (at some point in the future) would help to enhance the pedestrian realm. With the opening of the Seattle Streetcar on Broadway in 2014, the additional bike lanes, and the vehicle lane designation changes have resulted in more vehicular traffic on Harvard Avenue. Due to the number of students and community members that cross or traverse Harvard, there is concern over the increase in traffic and safety. Efforts to calm vehicle traffic along this important edge of campus by street narrowing would result in additional green space by extending the curb line into the existing street alignment. The street narrowing will provide for two lanes of traffic and one lane of on-street parking (no change from existing conditions).



Traffic-calming chicane design from SDOT Streets illustrated.



Traffic-calming pinch point design from SDOT Streets illustrated.



Existing condition on Harvard Avenue near the intersection with E Pine Street

SECTION 5 – OTHER GUIDELINES

Additional standards may be proposed by an institution or required by the Director to DCLU per the Major Institution Overlay District (23.69.030.C.4).

Sustainability Guidelines

Seattle Central College is committed to creating High Performance Educational Facilities that **will** ensure the optimal health and productivity of students and faculty. It also supports and **will** comply with all State of Washington LEED compliance mandates.

Whether termed “Sustainable,” “High Performance,” “Green,” or “Environmentally Friendly,” people have varied notions about what sustainable building means. Some think it means saving energy while others think it means protecting the environment. While these are important aspects of sustainability, they are not sufficient to describe it, because sustainability has a human dimension as well.

Sustainable building can provide improvements in lifestyle, comfort, satisfaction, and health along with protecting ecosystems and saving energy and resources. It integrates project designing, planning, and engineering to work *with*, not against, nature. Sustainable building practices incorporate nature’s “free” services (wind, sun, thermal properties, greenhouse principles, light, etc.) to create a high-quality indoor environment while circumventing as much damage to the ambient environment as possible.

Buildings are a primary source of pollution that leads to urban air quality problems, climate change, habitat destruction, and overfilled landfills. The challenge is to design and construct buildings prudently, so that they use a minimum of nonrenewable energy, produce minimum pollution, and use as little extracted material resources as possible, while at the same time increasing the comfort, health, and safety of the people who live and work in them.

Traditional building practices often overlook the interrelationships between a building, its components, its surroundings, and its occupants. “Typical” buildings consume more of our resources than necessary, negatively impacting the environment, and generating a large amount of waste.

Conversely, sustainable building practices offer an opportunity to create environmentally sound and resource-efficient buildings by using an integrated approach to design. Sustainable buildings promote resource conservation, including energy efficiency, renewable energy, and water conservation features; consider environmental impacts and waste minimization; create a healthy and comfortable environment; reduce operation and maintenance costs; and address issues such as historical preservation, access to public transportation and other community infrastructure systems. The entire lifecycle of the building and its components is considered, as well as economic and environmental impact and performance.

“High Performance Educational Facility” refers to the physical facility. Good teachers and motivated students can overcome inadequate facilities and perform at a high level almost anywhere, but a well-designed facility can truly enhance performance and make education a more enjoyable and rewarding experience. A high-performance educational facility is healthy; thermally, visually, and acoustically comfortable; energy, material, and water efficient; safe and secure; easy to maintain and operate; commissioned; has an environmentally responsive site; is a building that teaches; a community resource; is stimulating architecture; and is adaptable to changing needs.

Building Siting

Siting is one of the most important issues to grapple with when planning for new construction. The siting of any facility will impact every aspect of the campus, from the direct environmental impact to energy consumption, and on to indoor environmental quality. Siting involves both the decision of where to put the building and how to orient it on the site. Several issues need to be addressed when siting any new construction. These include, but are not limited to vehicle, pedestrian, and transit access, landscaping impacts, stormwater management and orientation of the building for passive heating, natural ventilation, and daylighting.

Indoor Environmental Quality (IEQ)

Indoor Environmental Quality is an integral aspect of a high-performance educational facility. Good IEQ can reduce student and faculty absences, increase student performance, reduce illnesses related to indoor toxins, and improve teacher retention rates. IEQ includes indoor air quality (IAQ), acoustics, daylighting and lighting quality, and thermal comfort. These factors **will** help reduce distractions, improve comfort levels, and keep students, faculty, and staff healthy.

All aspects of IEQ react and interact with each other as well as with other aspects of high-performance educational facilities. Siting issues will affect daylighting potentials and acoustics. Building envelope design will affect thermal comfort, daylighting, and indoor air quality. Materials choices will affect all aspects of IEQ. The construction process and the operations and maintenance will also affect IAQ. To optimize good IEQ, it is important to consider it throughout the design and construction process.

Energy Resources

“Sustainable” facilities **should** be models of energy efficiency. They can support sustainable energy efforts by using an integrated design process that takes into consideration everything from building siting and orientation to the building shape and the landscaping around it, as well as to the lighting, heating, cooling, and ventilation sources.

Integrated design strategies can result in long and short-term savings. For example, reduced heat-generation from an energy efficient lighting system and effective natural ventilation can reduce cooling demands, and thus the size and cost of the air conditioning units. All members of the design team **should** meet early in the planning process and continue to coordinate integrated design concepts throughout the project to reduce energy costs. The result of integrated design is reduced overall energy consumption, thereby saving construction costs through the downsizing of the systems and on-going cost of operation through reduced utility bills.

Water Resources The most economical, efficient, and environmentally appropriate approach to deal with water demand is to reduce water consumption and to use water resources more wisely. High performance educational facilities can contribute to this effort by using water-efficient landscape techniques and by using water-efficient fixtures and controls in indoor and outdoor plumbing systems.

Materials It is important to consider material efficiency in the design, construction, and renovation of buildings. Material efficiency refers to durable, reused, salvaged, refurbished, recycled content, and recyclable materials manufactured using environmentally friendly practices. The result is buildings that are environmentally responsible models to both their students and their community. Material efficiency can often save money by reducing the need to buy new materials and by reducing the amount of waste taken to the landfill. Buildings can reduce the number of materials needed by reusing onsite materials, eliminating waste created in the construction and demolition process, and choosing materials that are safe, healthy, aesthetically pleasing, environmentally preferable, and contain low embodied energy.

Community Matters

Sustainable design encompasses more than the physical building. It is imperative for SCC to consider the impact of new/renovation construction on the surrounding community.

The site on which a building is constructed impacts the surrounding community in several ways: pedestrian and automobile traffic, visual and physical effects of parking lots, quantity, and quality of open space as an amenity to a neighborhood, and community services the building may offer.

Aspects such as the exterior design of the college, amenities that it may provide, and environmental design features can be a source of pride to the community. The SCC campus is a center for teaching and learning that also adds a functional value within the community by providing access to facilities as well as services such as childcare, dental and health clinics, continuing education, conference facilities, performing arts venues, outdoor spaces for community events (farmers market, etc.).

Community-Wide Sustainability Efforts

SCC’s adjacent community neighbors have and are actively pursuing a variety of community-based sustainable efforts. As a primary landowner in the Capitol Hill neighborhood, SCC is committed to working with and supporting this community-based work, which includes the efforts by Community Roots (formerly Capitol Hill Housing) to create an Ecodistrict:

Community Roots is exploring the creation of an Ecodistrict on Capitol Hill at the properties over the Link light rail station, focusing on shared, sustainable, environmentally responsible, and well-designed new development. SCC **should** become an active shareholder and assist in the research of possible solutions for campus buildings and systems, in concert with the work proposed on the light rail TOD sites. This may include:

- District-wide energy solutions
- Shared water resources
- Regional stormwater detention and treatment systems
- Optimized waste, recycling, food production and composting
- Building orientation and development which maximizes passive heating and cooling opportunities

Commissioning Without proper commissioning, a building’s many sustainable design elements can be compromised. The Commissioning Process is a quality-oriented process for achieving, verifying, and documenting that the performance of facilities, systems, and assemblies meet defined objectives and criteria. The commissioning process begins at project inception (during the pre-design phase) and continues for the life of the facility, through the occupancy and operational phases. By implementing a commissioning plan, SCC can be sure that all systems function at optimum levels.

Faculty and Student Performance

Washington State Community and Technical Colleges are facing quite a list of challenges, which include tight budgets, ever-increasing student enrollment, growing needs for renovation and building, and most importantly, a higher expectation of faculty and student performance among these compelling circumstances. While facilities built to be sustainable cannot solve every issue facing a campus, they can certainly have a favorable impact on the College’s budget, help protect the environment, and encourage better performance of faculty and students as a result of providing a better learning environment. High performance educational facilities integrate today’s best technologies with architectural design strategies to achieve a better learning environment. Well-designed buildings include properties such as appropriate lighting (integration of daylighting and artificial lighting technologies), reduced noise levels (acoustic materials and low noise mechanical systems), healthy air quality, temperature, and humidity levels (indoor air quality or IAQ), and thermal comfort (HVAC systems, and low-emission materials). This reduces distractions and creates environments where students and faculty can see and communicate with one another clearly and comfortably.

Universal Design Guidelines

Universal Design, as defined by The Center for Universal Design at North Carolina State University, is “the design of products and environments to be usable by all people, to the greatest extent possible, without the need for adaptation or specialized design.” Universal design principals are typically more stringent than code-mandated accessibility requirements (i.e., the American with Disabilities Act). Seattle Central College promotes the use of these design principles in all new building development, renovation, or remodel.

Universal Design asks from the outset how to make the design work beautifully and seamlessly for as many people as possible. It seeks to consider the breadth of human diversity across the lifespan to create design solutions that work for all users. The following seven principles describe the basic philosophy of Universal Design:

- Equitable Use** The design is useful to people with diverse abilities. Guidelines include:
- Provide the same means of use for all users: identical whenever possible; equivalent when not.
 - Avoid segregating or stigmatizing any users.
 - Provisions for privacy, security, and safety **should** be equally available to all users.
 - Make the design appealing to all users.
- Flexibility in Use** The design accommodates a wide range of individual preferences and abilities. Guidelines include:
- Provide choice in methods of use.
 - Accommodate right- or left-handed access and use.
 - Facilitate the user’s accuracy and precision.
 - Provide adaptability to the user’s pace.
- Simple and Intuitive**
Use of the design is easy to understand, regardless of the user’s experience, knowledge, language skills, or current concentration level. Guidelines include:
- Eliminate unnecessary complexity.
 - Be consistent with user expectations and intuition.
 - Accommodate a wide range of literacy and language skills.
 - Arrange information consistent with its importance.
- Perceptible Information**
The design communicates necessary information effectively to the user, regardless of ambient conditions or the user’s sensory abilities. Guidelines include:
- Use different modes (pictorial, verbal, tactile) for redundant presentation of essential information.
 - Provide adequate contrast between essential information and its surroundings.
 - Maximize “legibility” of essential information.
 - Differentiate elements in ways that can be described (i.e., make it easy to give instructions or directions).
 - Provide compatibility with a variety of techniques or devices used by people with sensory limitations.
- Tolerance for Error**
The design minimizes hazards and the adverse consequences of accidental or unintended actions. Guidelines include:
- Arrange elements to minimize hazards and errors: most used elements, most accessible; hazardous elements eliminated, isolated, or shielded.
 - Provide warnings of hazards and errors.
 - Provide fail safe features.

Low Physical Effort

- The design can be used efficiently and comfortably and with a minimum of fatigue. Guidelines include:
- Allow user to maintain a neutral body position.
 - Use reasonable operating forces.
 - Minimize repetitive actions.
 - Minimize sustained physical effort.

Size and Space for Approach and Use

- Appropriate size and space are provided for approach, reach, manipulation, and use regardless of user’s body size, posture, or mobility. Guidelines include:
- Provide a clear line of sight to important elements for any seated or standing user.
 - Make reach to all components comfortable for any seated or standing user. Accommodate variations in hand and grip size.
 - Provide adequate space for the use of assistive devices or personal assistance.

Universal Design Recommendations

It is recommended that future developments seek to include successful implementation of these recommendations:

Circulation Issues

- Clearly define and highlight accessible routes throughout campus.
- Utilize color and textured walking surfaces to ease use by the visually impaired.
- Connect accessible routes to accessible building entries.
- To the greatest extent possible, do not separate accessible routes from those used by others.

Site Issues

- Provide personal and van parking stalls in excess of code minimum requirements. Locate these stalls adjacent to building entrances.
- Site benches **should** be provided with arms to assist in transferring from wheelchairs.
- Provide an accessible route to and from all city blocks that contain SCC facilities, including curb cuts, cross walks, and pathways.

Building Use Issues

- To the greatest extent possible, do not design elements that are exclusive to any portion of the population.
- Fully integrate accessible features into all public gathering spaces.
- Provide clear lines of sight to important elements for any seated or standing user particularly in classroom spaces.
- Make reach to all components comfortable for any seated or standing user. Accommodate variations in hand and grip size.
- Provide adequate space for the use of assistive devices or personal assistance.
- Use accessible bathroom stalls which are larger than code minimum. Include space for baby changing tables.
- Replace all vending and self-help equipment with those that are compliant with reach and operational limitations.
- Make all classroom, kitchen, and break room sinks accessible.
- In auditoriums and larger classroom spaces, provide lighting at instructional head walls for sign language interpreters.
- Update the location of toilet accessories, HVAC controls, light switches, elevator buttons, etc. to comply with recent code revisions.
- Make shades, curtains, windows, and mechanical doors operable by a closed fist.
- Acoustical performance will comply with WAC (Washington Administrative Code) requirements.
- Doors to operate with minimum required force. Where feasible, eliminate doors altogether. Where doors are needed, provide automatic door operators to the greatest extent reasonable.
- Provide piping protection below all sinks.
- Provide instructor consoles which are fully usable by those with disabilities, and which do not block the view of those seated in wheelchairs.
- Provide elevators to roof areas of all new buildings.

SECTION 6 – BICYCLE FACILITIES

Introduction

In an effort to increase the use of sustainable transportation options, and to reduce students and staff driving to campus, SCC seeks significant improvements to the manner in which it supports those who come to campus via bicycle.

Existing Conditions

The existing campus has a total of 134 bicycle parking spots. 36 of these are located in the supervised Parking Garage. 10 are in the form of secured bike lockers and the remaining are standard bike racks. The remaining 98 are standard bike racks located in publicly accessible areas around campus and in plaza areas. There are 89 additional parking spaces for bicycles located in public rights-of-way within or immediately adjacent to the proposed SCC MIMP Boundary

See Figure 3-1.6 -Existing Transportation and Bicycle Parking for specific locations and quantity.

Proposed Capacity

SCC, as a State of Washington state agency, is required as part of any new capital project to comply with state-mandated sustainability guidelines. Among these guidelines are standards established by the US Green Building Council's – Leadership in Energy and Environmental Design (LEED) program. As such, the standards established by this program for large campus environments provides an appropriate guideline for both quantity and quality of a bicycle parking program.

This master plan is based on development up to 7,500 student FTE (Full Time Equivalent) enrollment. 500 of those 7,500 students are assumed to be residents of the planned Student Housing project. SCC's typical student to faculty/staff ratio of approximately 20:1 would anticipate faculty and staff FTE of 375. Based on this total of 7,875 total occupants as an assumed maximum capacity on campus at any one time. The LEED 4.1 standard for Bicycle Facilities for Large-Occupancy Projects, would require:

Please Note: LEED considers students to be "visitors" due to the transient nature of their use. Faculty, Staff, and Students who live on campus are considered "regular building occupants". Therefore, for the purposes of total campus planning:

Visitors	= 7,000
<u>Regular Building occupants</u>	<u>= 875</u>
Total campus occupancy =	7,875

LEED Version 4.1 Bicycle Facilities Standard

- Provide short-term bike storage for at least 2.5% of all peak visitors.
- Provide long-term bike storage for at least 15% of all regular building occupants in residential facilities in addition to short-term spaces.
- Provide long-term bike storage for at least 5% of all regular building occupants in addition to short-term spaces.
- Provide 1 shower facility for first 100 regular building occupants: 1 per 150 up to 999 regular occupants.

Types of Bicycle Parking

There are two types of bicycle parking spaces the College proposes – Short-term parking and Long-term parking.



Short-term parking is characterized as a typical bike rack, in a publicly accessible location, where security is provided wholly by the bike owner.



Long-term parking is characterized by bike parking in a monitored, secured facility (Bike Rooms) or user-controlled/accessed bicycle lockers.

Total Campus Need (per LEED v4.1, Location and Transportation – Bicycle Facilities as of January 2023)

Short-term storage for visitors = 7,000 x 2.5%	= 175 spaces
<u>Short-term storage for regular building occupants = 875 x 2.5%</u>	<u>= 22 spaces</u>
Total Short-Term space need across campus = 197 spaces	

Long-term storage for regular building occupants in residential facilities = 500 x 15%	= 75 spaces
<u>Long-term storage for regular building occupants = 375 x 5%</u>	<u>= 20 spaces</u>
Total Long-Term space need across campus = 95 spaces	


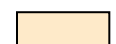
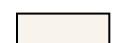
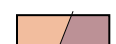
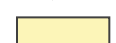







Shower Facilities for regular building occupants (excluding residential) = 1 for first 100 + 1 for each 150 after	
Total shower/changing facility need across campus = 3 total shower/changing facilities.	

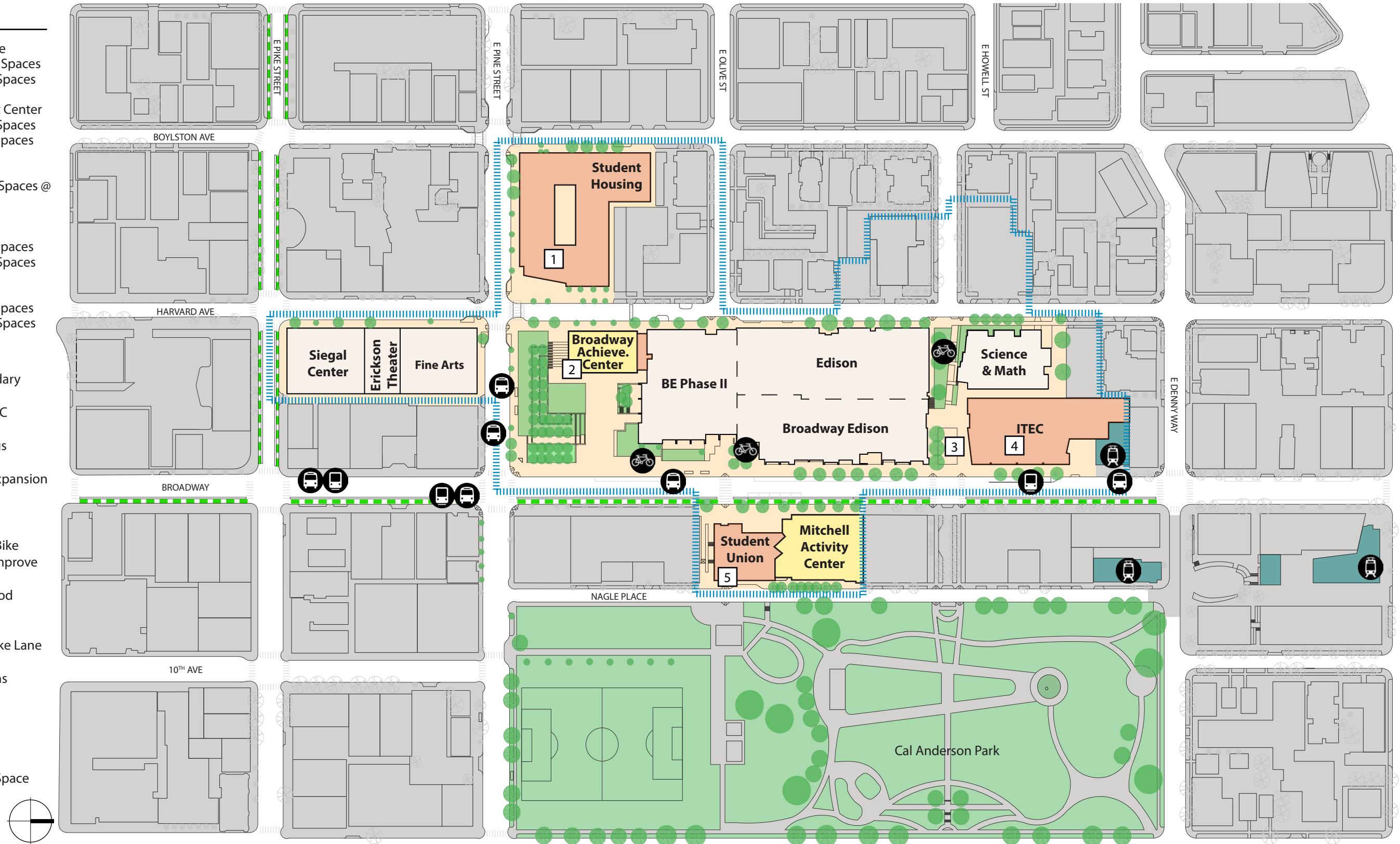
Planned Bicycle Parking Facility and Infrastructure Improvements

SCC will address bicycle parking and shower facility needs on a project-by-project basis. The quantities of parking facilities and showers below represent the minimum development the College is committing to. *See Figure 4-6.1 – Proposed Bicycle Parking and Facilities.*

FIGURE 4-6.1 – PROPOSED BICYCLE PARKING AND FACILITIES

LEGEND

- 1 Student Housing Garage
≥375 Long-Term Bike Spaces
≥15 Short-Term Bike Spaces
 - 2 Broadway Achievement Center
≥20 Short-Term Bike Spaces
≥20 Long-Term Bike Spaces
 - 3 Howell Street Passage
New ≥12 Short-Term Spaces @
Broadway
 - 4 ITEC Garage
≥25 Long-Term Bike Spaces
≥25 Short-Term Bike Spaces
 - 5 Student Union/MAC
≥36 Long-Term Bike Spaces
≥18 Short-Term Bike Spaces
-
-  Proposed MIO Boundary
 -  Parcels Owned by SCC
 -  Existing SCC Buildings
 -  New Construction/Expansion
 -  Renovation
 -  Existing Short-Term Bike Parking to Remain/Improve
 -  Existing Neighborhood Buildings
 -  Existing Protected Bike Lane
 -  Sound Transit Stations
 -  Bus Station
 -  Streetcar Station
 -  Park & Public Green Space



Bicycle Parking Facility Improvements

Existing facilities to remain:
 East side of BE Phase II – SE corner = 42 short-term bike parking
 Main Entry to BE complex – Broadway = 16 short-term bike parking
 E Howell Street Passage = 34 short-term bike parking

Student Housing:
 Sidewalks near main entrance = 15 short-term bike parking
 Secured Bike Room inside building = 375 long-term bike parking
 = 2 shower/changing rooms for non-residents

ITEC Building:
 Sidewalks near main entrance = 25 short-term bike parking
 Secured Bike Room inside building + public bike lockers = 25 long-term bike parking
 = 3 shower/changing rooms

E Howell Street Passage:
 Sidewalks near Broadway (in addition to existing spaces) = 12 short-term bike parking

Broadway Achievement Center (BAC):
 Sidewalks near main entrance = 20 short-term bike parking
 Secured Bike Room inside building and/or exterior bike lockers = 20 long-term bike parking
 = 1 shower/changing rooms

Student Union/MAC:
 Sidewalks near main entrance = 18 short-term bike parking
 Secured Bike Room inside building = 36 long-term bike parking
 = 6 shower/changing rooms (existing)

Cumulatively, across campus, the above commitments total:

- 182 short-term bike parking spaces in bike racks located in publicly accessible areas. (15 less than the LEED v4.1 standard).
- 456 long-term parking spaces in secured bike room or publicly accessible lockers. (361 more than the LEED v4.1 standard)
- 12 shower/changing rooms. (9 more than the LEED v4.1 standard)

Bicycle Parking Infrastructure Improvements

SCC will invest in infrastructure improvements to make its bicycle parking facilities more accessible and functional to College users and the community. See the Transportation Design Guidelines for information on bicycle infrastructure improvements.

Aspirational Improvements

SCC acknowledges that certain campus-wide aspirational goals would benefit by the addition of bike facilities and will include them with the following aspirational project(s).

- South Plaza Improvements
- E Howell Street Passage Improvements (West half)

Accommodations for Shared Bicycles and Scooters

SCC understands the importance of micro-mobility devices, their ability to reduce short vehicle trips and make cities more accessible and navigable, and that by sharing these devices we can make trips easier.

There are multiple companies providing shared e-bikes and e-scooters for public use in Seattle. As of June 2023, all these mobility devices are dockless, meaning they can be parked anywhere. Users are encouraged to park them in the curb zone of the sidewalk, at a bike rack, or in a painted bike/scooter parking area. Currently, SCC's campus features one of these painted parking areas in the right-of-way on Broadway next to the mid-block crossing between the Mitchell Activity Center and Broadway Edison Complex.

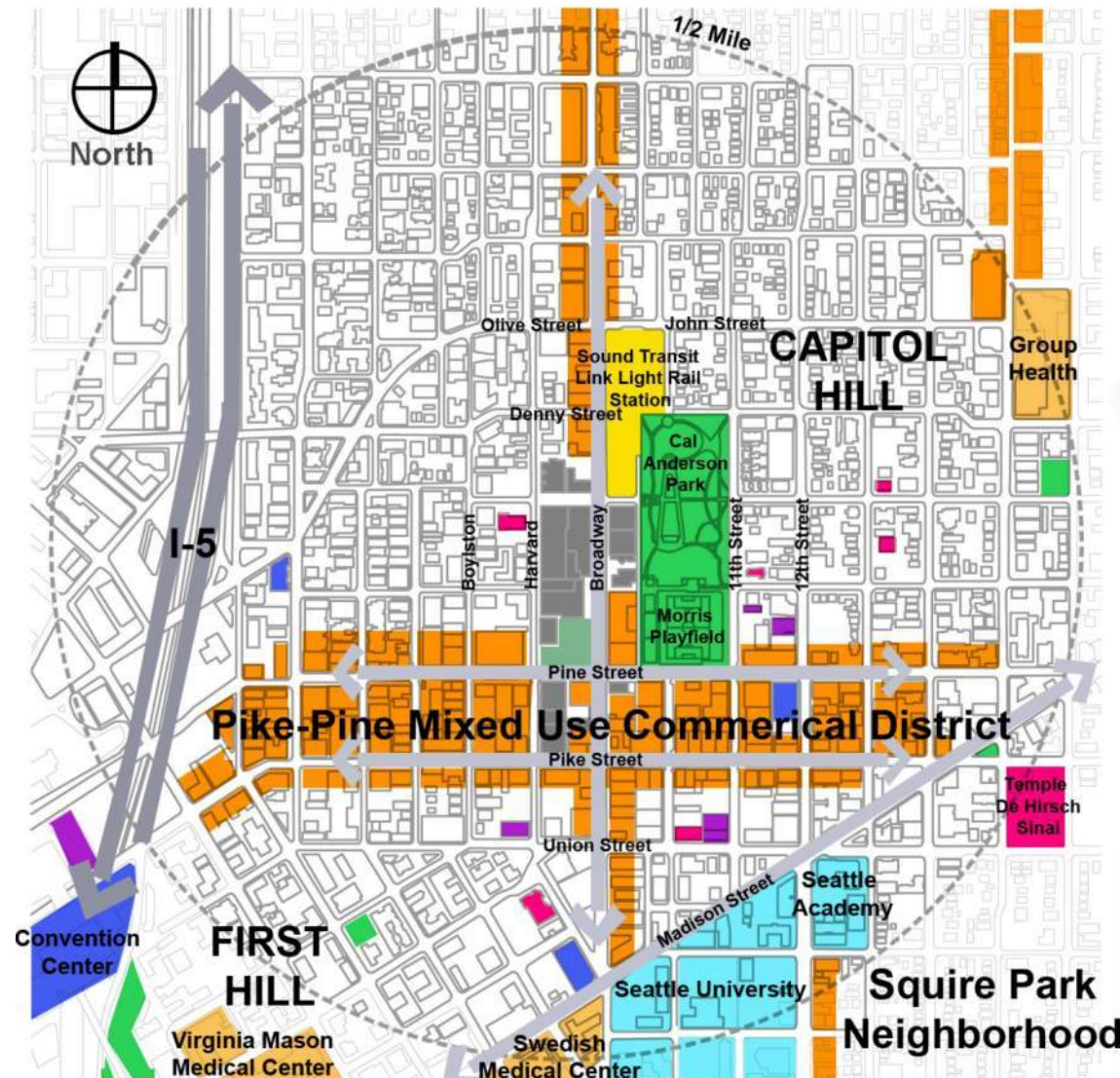
The College will endeavor to support shared-mobility devices by providing intentional parking space for them on campus and making students and staff aware of available [discounted fares](#). SCC is open to creating additional painted parking zones in consultation with Seattle Department of Transportation (SDOT).

CHAPTER FIVE – CAMPUS AND COMMUNITY CONTEXT

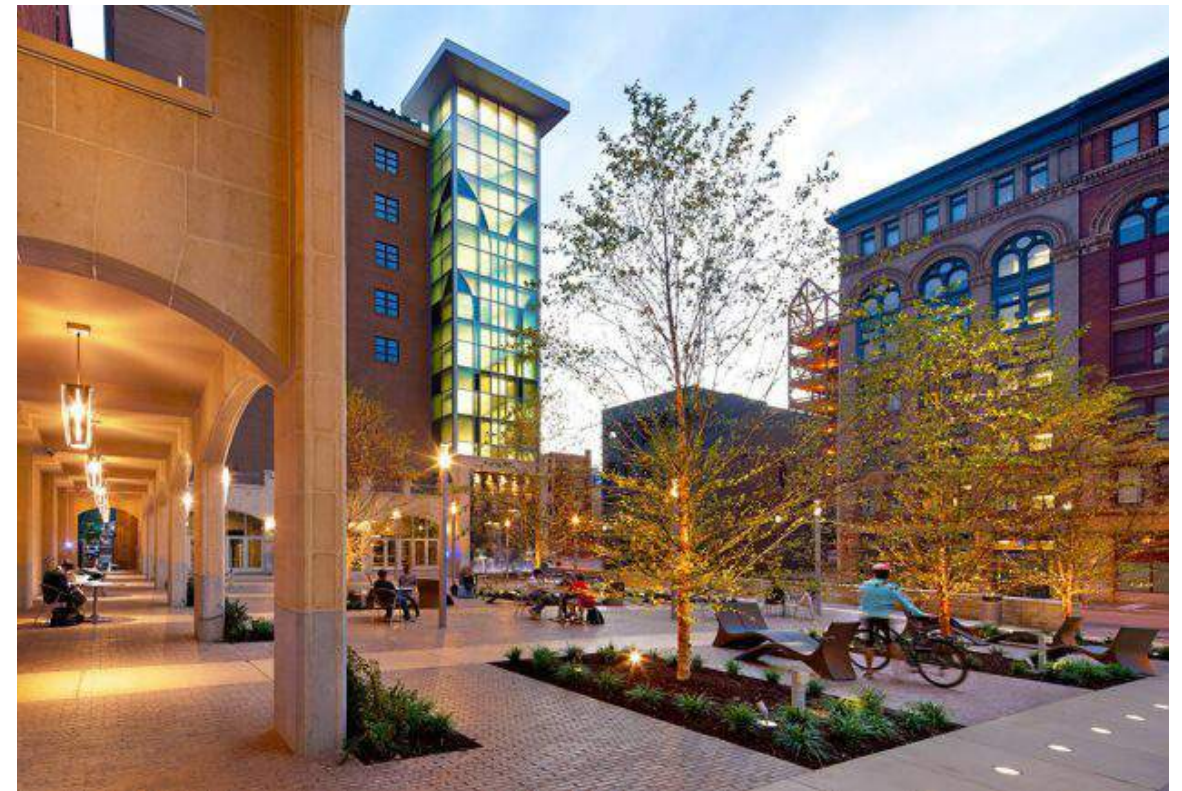
Introduction

Seattle Central College recognizes the importance of sustaining a vibrant campus environment with a strong physical connection to the neighborhood. This section provides guidance for future development that seeks to maintain vibrancy and integrate the campus into the community context.

The future vision for the college campus is that it be permeable and well-integrated into the urban fabric. Students and members of the surrounding community will blend in this urban cultural and business climate. Students will live on campus as well as in the surrounding neighborhoods. SCC’s students will interact with neighbors and the community through economic use, internships, and professional connections, as well as participation in and creation of cultural events. Leaders from business, government and non-profit sectors will be key partners with SCC and in turn will speak, teach, and mentor on campus. Increasingly, campus functions and services will be readily accessible to and used by residents of the surrounding neighborhood. An asset to the college and the community will be a re-vitalization of the college streetscape along Broadway from E Denny Way to E Pine Street. Through these and other activities, connections between SCC’s campus community, its surrounding neighborhood, and the community at-large will be strengthened.



Community connections within 1/2 mile



Point Park University is located in the midst of the urban commercial core of Pittsburgh, PA. Point Park recently, and is, undergoing a major revitalization of several downtown blocks into what they call an Academic Village which blends academic spaces, services, and cultural spaces with neighborhood amenities such as the Point Park Square shown above.



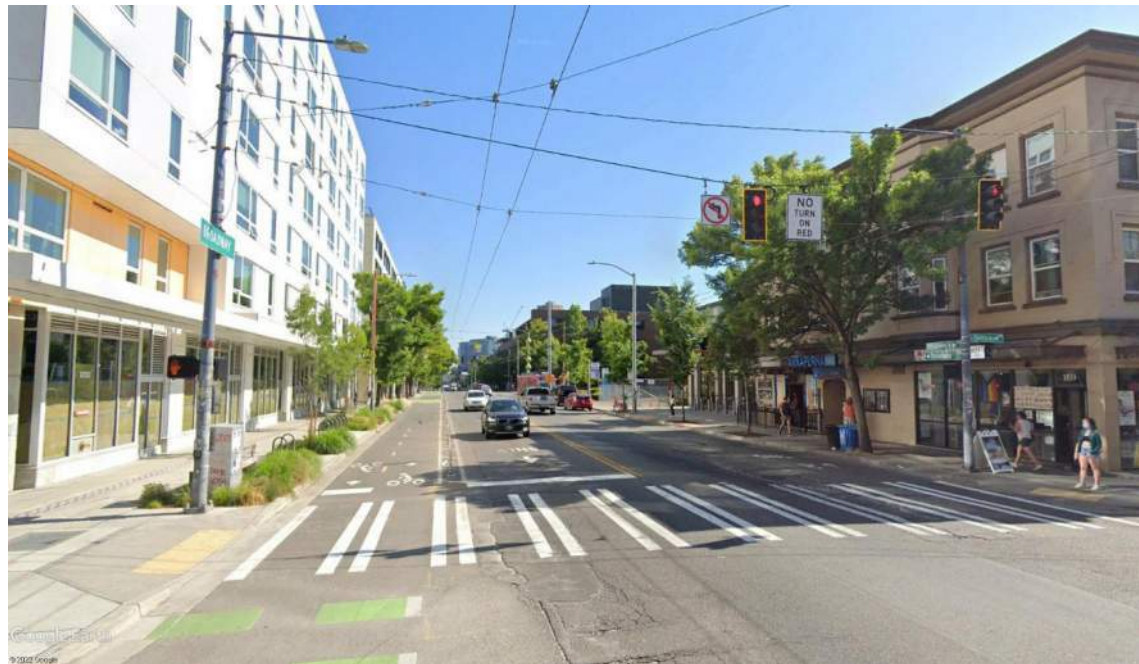
New York University, New York, is located over several non-contiguous urban blocks in the midst of Manhattan. Campus identity is clearly known due to the branding of the “district.” In addition, college students and the community at large blend together in using both university and neighborhood resources.

Community Connectivity

In recent years, the community that surrounds the SCC campus has seen substantive development. Most notably is the opening of the Capitol Hill Sound Transit Station and the development of the Sound Transit and other parcels at the north end of the SCC campus. This development activity has been openly embraced and shaped by Capitol Hill and Pike/Pine community participation. SCC seeks to become an active participant in the community fabric of the vibrant Capitol Hill neighborhoods.



Before



After

Broadway at E Denny Way, before and after images showing the new streetcar stop and Sound Transit Stop (right side of image)

In all future development, campus edges will be designed to be permeable and integrated into the urban fabric. Recent community design guidelines such as the Urban Design Framework (for the ST parcel development) and the Capitol Hill EcoDistrict (by Capitol Hill Housing) have articulated the community desires for the neighborhood. Members of the SCC staff have participated in the creation of both these plans. SCC embraces the plans and many of the relevant design guidelines are incorporated throughout this master plan document.

Any future college development will embrace these guidelines as a reflection of its strong commitment to have its physical presence match its community neighborhood. SCC students, faculty, and staff will engage with members of the surrounding community in this urban academic, cultural, and business climate. Seattle residents and visitors come to the urban campus to see performances, attend lectures, participate in continuing education offerings, and utilize the college's public services. Students will live both on campus as well as throughout the surrounding neighborhoods. Through these and other services, connections continue to grow between SCC's campus community, its surrounding neighborhood, and the community-at-large.
















Pedestrian Connections and Access to Surrounding Amenities and Services

In general, the plan seeks to increase the permeability of campus, activate building frontages and streetscapes, and improve safety for students and the community. Improvements to campus boundaries and open spaces are critically important to supporting strong physical connections between the SCC campus and the surrounding neighborhoods. **See Figure 5-1.1 – Community Connectivity – Planned** and **Figure 5-1.2 – Community Connectivity Aspirational** for depictions of anticipated improvements including:

- Improvements to the open space between the BE complex and the SAM/proposed ITEC buildings. The intent will be to improve the pedestrian linkages between the residential area west of campus, with access to the Broadway Commercial District, Link light rail station, and Cal Anderson Park
- Improvements to street frontages and crosswalks associated with Student Housing (E Pike Street, Boylston Ave, and Harvard Ave). **See Figure 5-1.3 – Potential Intersection Improvements for Student Housing**
- Crosswalk enhancements at E Howell Street.
- Pedestrian streetscape improvements along Broadway.
- Pedestrian streetscape improvements along Harvard.
- Crosswalk improvements on Nagle Place between the planned Student Union and Cal Anderson Park.
- Small opens spaces at the entrances to all new building projects.
- Creation of new campus gateways adjacent to the Sound Transit station south of E Denny Way
- Enhancements to the main central campus crossing between the BE Complex and the planned Student Union.
- Perimeter landscaping and street trees will be provided along the street frontages of new developments and substantial renovations.
- Continue working with neighbors and jurisdictional agencies to maintain clean, safe, and accessible alleyways adjacent to College buildings and parcels.

FIGURE 5-1.1 – COMMUNITY CONNECTIVITY – PLANNED

LEGEND

-  Proposed MIO Boundary
-  Streetscape Improvement
-  Open Space Improvments
-  District Gateway Enhancements
-  Existing Pedestrian Crossing
-  Existing Protected Bike Lane
-  Pedestrian Enhancement
-  Sound Transit Station Entrance
-  Streetcar Stop
-  Metro Bus Stops
-  Existing SCC Buildings
-  New Construction/Expansion
-  Renovation
-  Park & Public Green Space
-  Existing Neighborhood Buildings

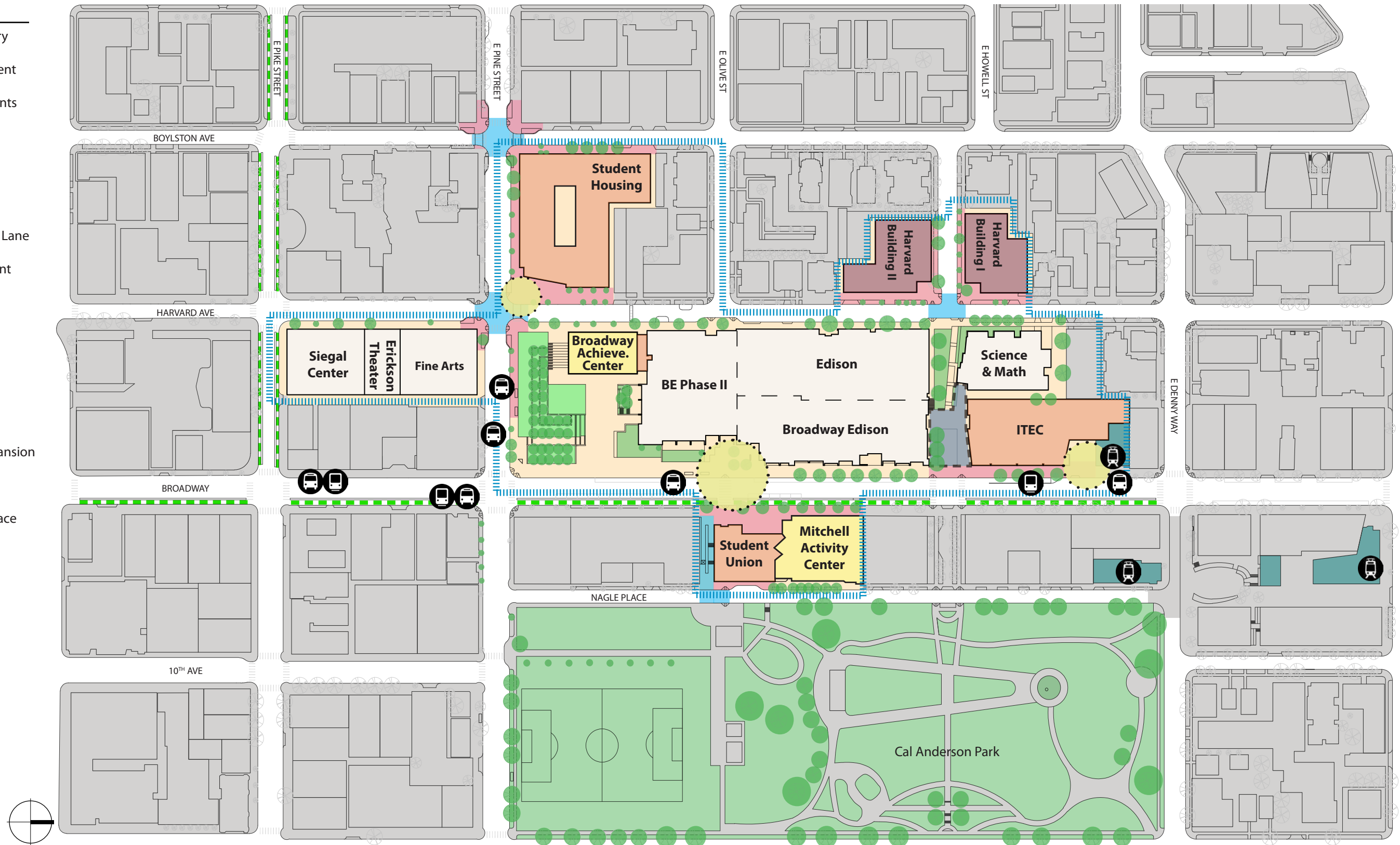

















FIGURE 5-1.2 – COMMUNITY CONNECTIVITY – ASPIRATIONAL

LEGEND

-  Proposed MIO Boundary
-  Streetscape Improvement
-  Open Space Improvements
-  District Gateway Enhancements
-  Existing Pedestrian Crossing
-  Existing Protected Bike Lane
-  Pedestrian Enhancement
-  Sound Transit Station Entrance
-  Streetcar Stop
-  Metro Bus Stops
-  Existing SCC Buildings
-  New Construction/Expansion
-  Renovation
-  Park & Public Green Space
-  Existing Neighborhood Buildings

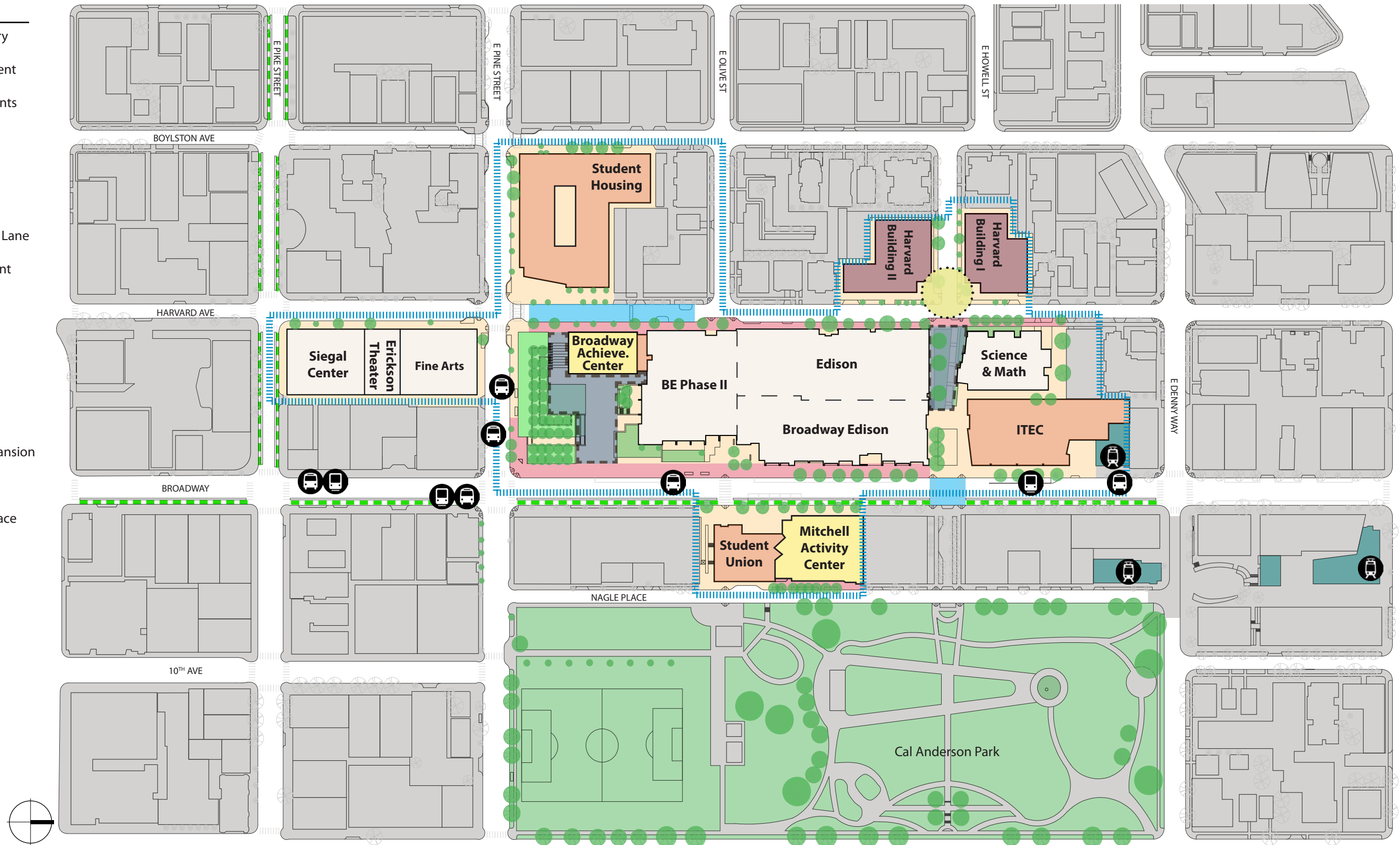


FIGURE 5-1.4 – POTENTIAL INTERSECTION IMPROVEMENTS FOR STUDENT HOUSING

Boylston Ave & E Pine St:

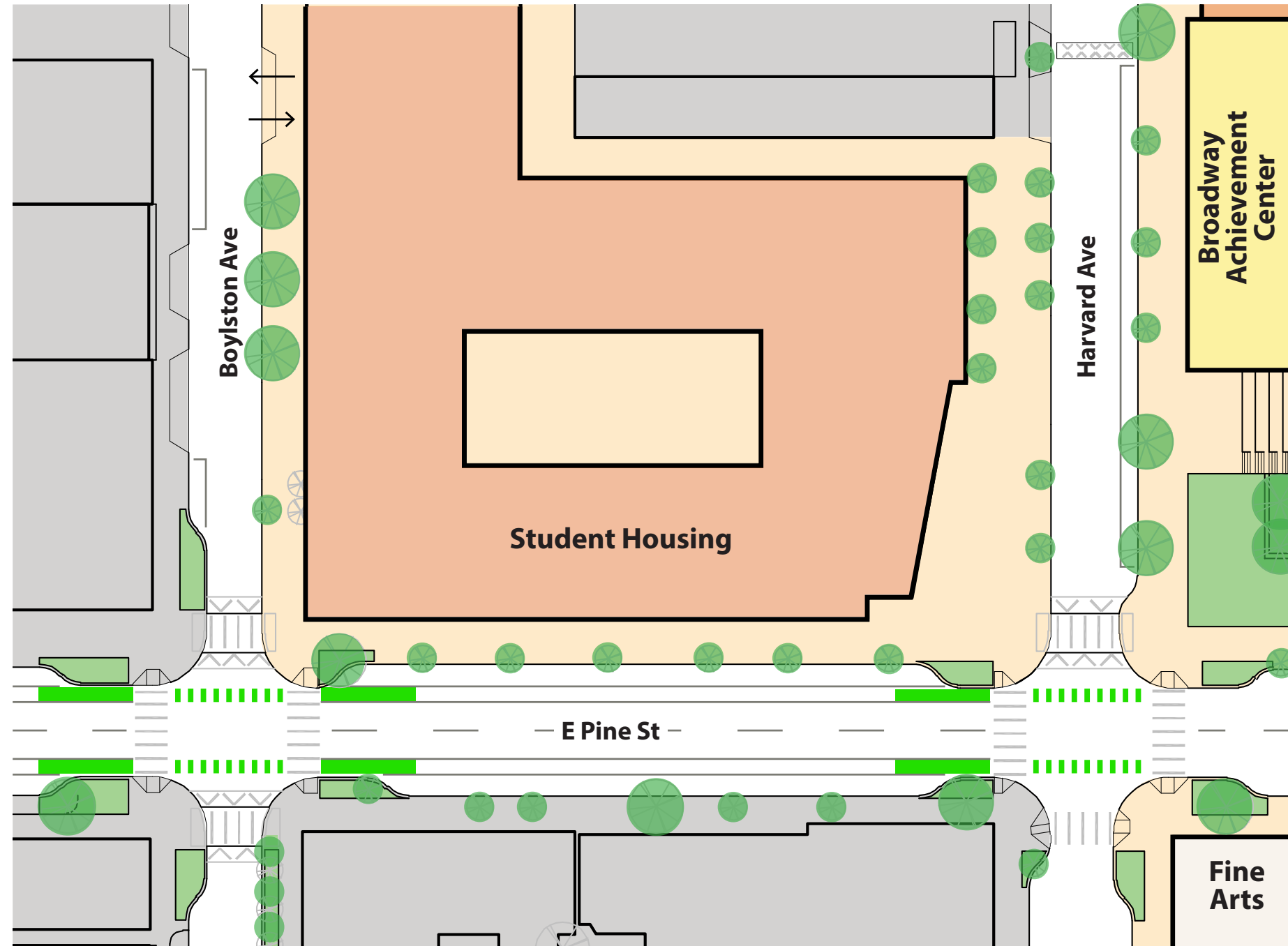
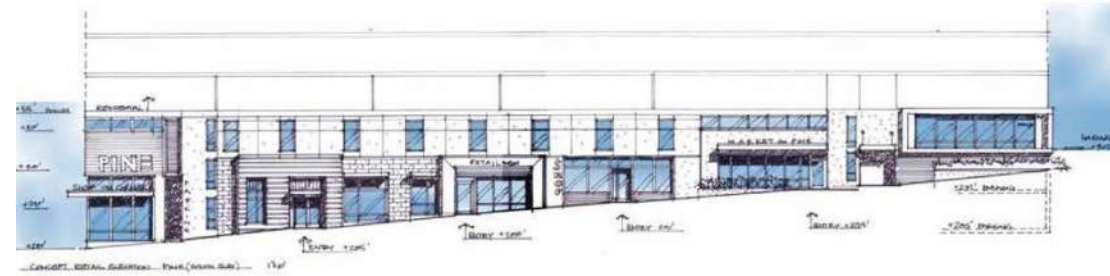
- Raised Crosswalks for pedestrians crossing Boylston Ave.
- Curb Bulbs at northwest and southwest corners.

Harvard Ave & E Pine St:







- Raised Crosswalk at north Harvard crossing.
- Curb Bulbs at northeast and southeast corners.

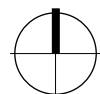
Other Improvements:

- No Student Housing Garage access from Harvard Ave.
- Student Housing Garage entrance on Boylston to be moved north - away from E Pine Intersection.
- Add traffic calming measures (speed cushions, chicanes, neck downs, etc.) on Harvard Ave north of Student Housing to slow vehicles approaching E Pine St.



LEGEND

-  SCC Existing Building
-  New Construction/Expansion
-  Renovation
-  Neighborhood Building
-  Park & Public Green Space
-  Existing Bike Lane



Major Institutions within 2,500 feet

Seattle Central College is within 2,500 feet of three other Major Institutions. Kaiser Permanente to the east, Seattle University and Swedish Medical Center to the south. Virginia Mason Medical Center is within easy walking distance.

Students from SCC's Health Education Programs (which are not located on the main campus but are instead located approximately 1.7 miles south at the newly renovated Pacific Tower) often perform internships at the medical institutions. SCC's other academic programs often feed transfer students to Seattle University.

projects are developed. The following are recommended general guidelines to be followed in new development throughout the campus.

Arts and Gathering Spaces

The Capitol Hill neighborhood is a robust center for the Arts with many venues that support community use. **See Figure 5-1.4 – Campus and Community Arts and Gathering Spaces.** Seattle Central College is an active partner in the neighborhood as its campus includes three theater spaces that are used, to varying degrees, for both instructional purposes and as venues for community use. The college has a long history of entering into both long-term leases and short-term rental agreements with community arts organizations to leverage use of its campus venues as a community resource. SCC's available venues include:

Broadway Performance Hall (BPH)

The BPH hosts presentations by faculty, staff and invited guests as well as student-run performances and events. In addition, arts organizations, community groups, traveling performers and others have events at this venue. The fixed seat space holds 295. In addition to the performance hall, the building is used by the college's Music department.

Fine Arts Building (which includes the Egyptian Theater)

Within the Fine Arts building is the 450 seat Egyptian Theater. The theater is not utilized by the College for instruction. The space is leased out, currently to the Seattle International Film Festival. The remainder of the Fine Arts Building houses academic instruction in the fine arts.

Erickson Theater

This intimate "black box" performance space and a resource for Seattle Central College's drama students and the Seattle arts community. In addition to performances, the venue is great for panel discussions, lectures, meetings, and other events. The space holds a maximum of 151. The black-box theater is also used by the college Drama program.

Preserving Arts and Gathering Spaces for Community Use.

Seattle Central receives operational funding based upon the number of students enrolled in various programs. It is important to note, that the funding streams for SCC for both operational and Capitol dollars are only provided to support enrollment activities. No funding is provided for supporting community or other enterprise use.

Enrollment in the performing arts programs at SCC is limited, thus the funding available to support these three venues is only a fraction of what it takes to operate and maintain. SCC has long collaborated with community and arts organizations to lease and rent these facilities. Despite this, the added revenue still does not support their operation and maintenances uses. The college has long sought and must develop more robust partnerships that can appropriately support preservation of these spaces for continued community use.

Proposed Changes to Arts and Gathering Spaces

Fine Arts Building

The college currently has no plans for any physical changes to the Egyptian Theater. It will remain in its current configurations and be leased to an outside partner. The remainder of the building will continue to serve the College.

Erickson Theater

The college currently has no plans for any physical changes to the Erickson Theater. It's expected to remain in their current theater configuration and be lease/rented to outside partners when not serving instructional needs.

Broadway Performance Hall

As a Planned Project in this master plan, the building is planned as a full interior renovation of the building. This 41,174 square foot building has less than 7,000 square feet of space used for instructional purposes. The building has had almost no improvements since its opening in 1978 after re-construction and needs a comprehensive renovation. The college submitted for Capitol funding to renovate the building interior to better serve the needs of the college in instruction and student support. Cognizant of the role these building plays as a community resource, the planned program for the renovation includes a 4,500 square foot auditorium. The space is expected to be a flexible multi-use space that can be used for a variety of college and community gathering of up to 250 people.

FIGURE 5-1.4 CAMPUS AND COMMUNITY ARTS AND GATHERING SPACES

LEGEND

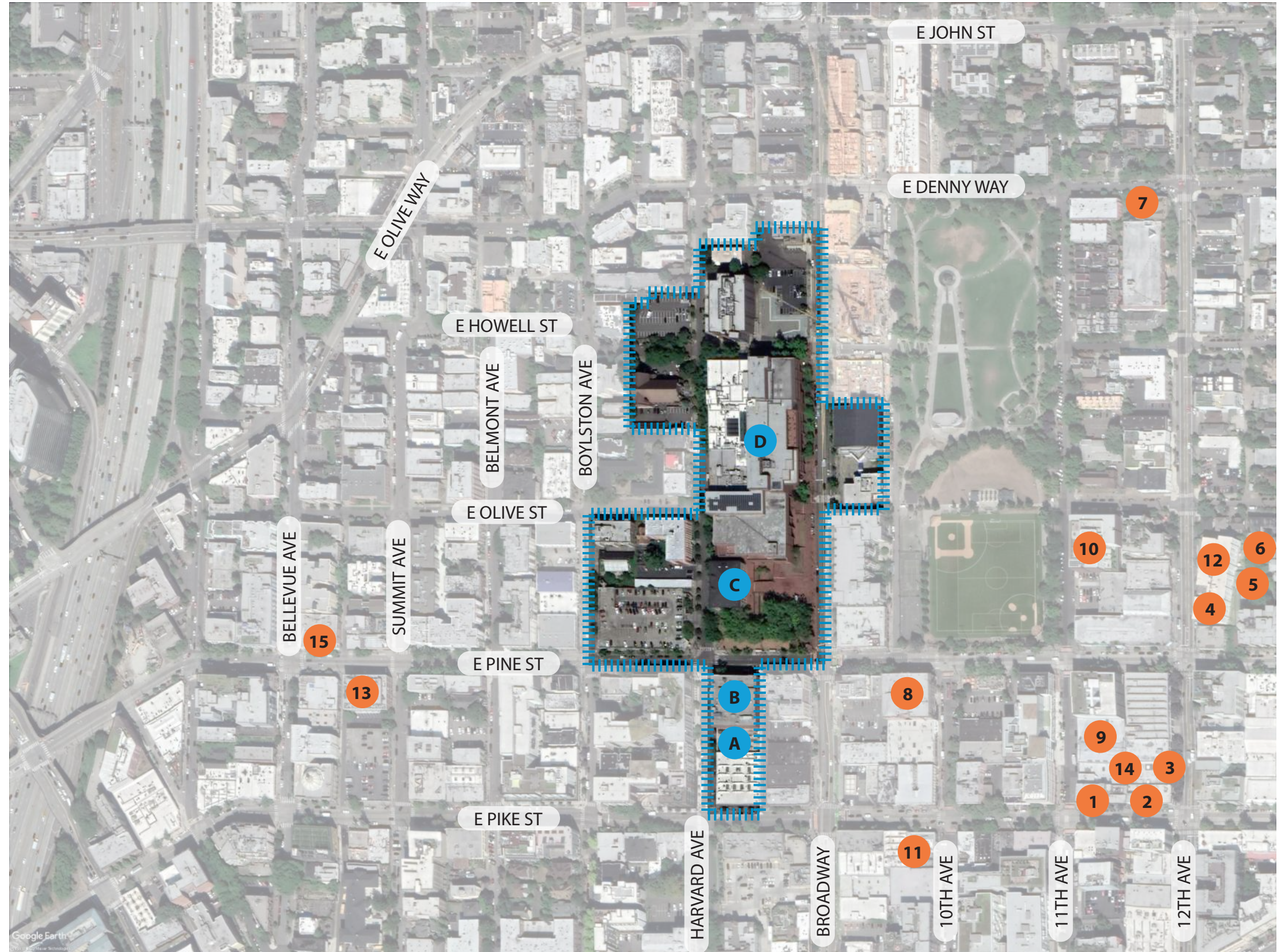
Proposed MIO Boundary

Seattle Central College Arts and Gathering Spaces

- A Erickson Theater
- B Egyptian Theater
- C Broadway Performance Hall
- D M. Rosetta Hunter Art Gallery

Existing Community Performing Arts Resources

- 1 Annex Theatre / Cassandra Blackmore Studio / Studio Current
- 2 ReAct Theatre
- 3 Northwest Film Forum / Indigenous Showcase
- 4 12th Avenue Arts
- 5 Washington Ensemble Theater
- 6 Three Dollar Bill Cinema
- 7 Artist Trust
- 8 Century Ballroom
- 9 Crybaby Studios
- 10 Hugo House
- 11 Neumos
- 12 Strawberry Theatre Workshop
- 13 True Love Tattoo & Art Gallery
- 14 Vermillion Art Gallery & Bar
- 15 Seattle's LGBTQ+ Center



Master Plan Consistency with Neighborhood Plans

Seattle Central College sits at the nexus to two neighborhoods; Capitol Hill with the Broadway district to the north, and the Pike/Pine corridor to the south. Planning included in the Capitol Hill Neighborhood plan, dated December of 1998 has been, for the most part fully realized, or is no longer valid.

Both neighborhoods have an extensive history of community planning. Most notable were the efforts undertaken around the Capitol Hill Light Rail Station planning from 2010. This resulted in the Capitol Hill Light Rail Station Design Guidelines which was published in 2013. Much of the planning contained in those guidelines has now been completed. Current neighborhood planning (as recognized by the City of Seattle and/or the Department of Neighborhoods is limited to the following:

- Capitol Hill Neighborhood Design Guidelines (version 2 was issued in 2019).
- Pike Pine Neighborhood Design Guidelines (version 3 was issues in 2017).

These plans were carefully reviewed by SCC and elements that are relevant to an institution of higher education have been incorporated wholly or adjusted during conversations with the SCC MIMP Development Advisory Committee (DAC).

This masterplan is consistent with the overall goals and visions of the neighborhoods to the extent reasonable for a state funded institution. Many neighborhood goals may be difficult to realize due to the SCC's funding (from State resources). In these cases, where appropriate, SCC has identified "Aspirational" commitments to seek funding for realization.

Master Plan Consistency with Comprehensive Plan

Seattle Central College has an important role in fulfilling aspects of the Seattle Comprehensive Plan. This can have a dramatic effect on the health and well-being of the neighborhood and Seattle as a whole. The Comprehensive Plan document referenced here is the 2022 Comprehensive Plan with amendments and updates.

Healthy Growth, Aging, and Lifestyles

The goal established is, "create a healthy environment where community members of all ages, stages of life, and life circumstances are able to aspire to and achieve a healthy life, are well nourished, and have access to affordable healthcare."

Seattle Central College, in partnership with Neighborcare Health and Delta Dental, operates the Dental Education Clinic. This clinic offers preventative and restorative dental services performed by licensed dentists with the support of students enrolled in various Dental programs at the Health Education Center. This partnership provides affordable dental care for the community and a unique learning opportunity and real-world training environment for students.

As a public institution that receives State funding, SCC projects that receive capital funds are implored/ required to meet minimum standards for sustainability. These standards include LEED Silver rating, the Washington State Energy Code – Commercial, and City of Seattle Energy Code. These standards require new and renovated buildings to be more energy efficient and use construction materials and methods that reduce the use of products containing harmful chemicals.

Lifelong Learning

The goal established is, "support an education system and opportunities for lifelong learning that strengthen literacy and employability for all Seattleites."

As one of the Seattle Colleges, Seattle Central College is a key pillar in the fulfillment of Seattle Promise. Seattle Promise is a college tuition and success program launched by Seattle Colleges, Seattle Public Schools, and the City of Seattle following passage of the Families, Education, Preschool, and Promise Levy in 2018. Seattle Promise includes free tuition at any of the Seattle Colleges, equity scholarships, and student support and advising starting in high school and continuing through a student's enrollment at a Seattle College campus. All graduating seniors attending Seattle public high schools are eligible for the Seattle Promise program regardless of grade point average (GPA), income, ability, or country of birth. Seattle Central College is proud to be a partner in providing education and growth opportunities for young Seattleites.

Through the Career and Technical Education (CTE) Dual Credit program, Seattle Central College (as well as all Seattle Colleges) is partnering with Seattle Public Schools to award community college workforce education credit by recognizing comparable learning in high school. This program is designed for high school students who plan a career that will require a certificate or an Associate of Applied Science degree from a community college. It strives to avoid duplicating curriculum content, sets high academic standards and prepares students to meet real-world career demands.

CHAPTER SIX – TRANSPORTATION MANAGEMENT PLAN

Introduction

This chapter identifies the key elements of the Transportation Management Plan (TMP) as defined in Seattle Municipal Code (SMC) 23.69.030 F. The minimum requirements of the TMP defined in this SMC section include:

- A description of existing and planned parking, loading and service facilities, and bicycle, pedestrian and traffic circulation systems within the institutional boundaries and the relationship of these facilities and systems to the external street system. This description shall include a description of the Major Institution's impact on traffic and parking in the surrounding area; and
- Specific institutional programs to reduce traffic impacts and to encourage the use of public transit, carpools and other alternatives to single-occupant vehicles. Any specific agreements with the City for the provision of alternative modes of transportation shall also be included.

This document also identifies TMP strategies that the Seattle Central College (SCC) is committing to along with a list of potential measures that could be implemented if the single occupancy vehicle (SOV) target is not met.

Campus Access/Circulation

Seattle Central College (SCC) is in the Capitol Hill neighborhood of Seattle. The Major Institution Master Plan (MIMP) anticipates a future campus population of 7,875.¹ This population would be accommodated on campus either with or without construction changes due to the MIMP. New construction is intended to accommodate future residential students, create a new Information Technology Education Center, and create two new buildings (Harvard I and II) west of Broadway to accommodate specific programs. Vehicular access to the existing campus parking lots occurs on Broadway, E Pine Street and Harvard Avenue. Planned changes to the campus parking will result in changes to access points.

Figure 3-1.4 – Existing Transportation and Vehicle Parking illustrates the existing campus transportation and **Figure 3-1.5 – Proposed Transportation and Vehicle Parking** illustrates the proposed campus transportation including transit and bicycle parking. The campus itself is not contiguous and spreads along several blocks. The main bulk of the campus is concentrated in the area between E Pine Street and E Denny Way, and between Harvard Avenue and Broadway. There are no bisecting public roadways in that area. The existing Fine Arts buildings are located south of the main campus, across E Pine Street. The Student Union, Bookstore, and Activity Center lie east of Broadway from the main campus blocks. The North Plaza parking lot is located west of Harvard Avenue. Vehicle, most transit, and pedestrian access to the SCC campus is provided via the surrounding street grid system. Regional access to the campus is provided via I-5 to the west of the campus, SR-520 to the north of the campus, and I-90 to the south of the campus. Broadway and E Pine Street, both minor arterials, serve as the primary routes to and from the campus.

All streets abutting the campus have sidewalks along both sides. Pedestrians flow between the different sections of the campus via a combination of signalized and two-way-stop-controlled intersections. Pedestrian flow rate analyses show that, currently, pedestrians experience free flow movement along each segment during the weekday peak hours. Pedestrians would have ample space to walk at preferred speeds without experiencing inconveniences due to lack of capacity. This pedestrian analysis shows that sufficient space would continue to be provided with the campus and area population growth.

The bicycle system surrounding the campus provides protected facilities that connect to the surrounding neighborhood land uses as well as to downtown Seattle. Broadway features protected bicycle lanes on the east side, south of E Denny. North of E Denny, sharrows are provided along Broadway. Pine Street has bicycle lanes on both the north and south sides of the street. Protected bicycle lanes exist along Pike Street, west of Broadway. This connects the campus to downtown Seattle.

Seattle Central College is well served by transit with service provided by King County Metro, SDOT, and Sound Transit. The campus is served by 8 bus routes. King County Metro routes 2, 8, 10, 11, 49, and

60 operate at 10-to-15-minute headways. Route 9 operates on 30-minute headways, connecting the campus to Rainier Beach. Route 43 runs between the University District to the north and downtown Seattle, operating adjacent to the campus at 20-to-40-minute headways. The nearest stops to campus are provided along E Pine Street, Broadway, and E John Street.

Sound Transit's Link Light Rail and the First Hill Streetcar also serve the campus. Sound Transit's Link Light Rail Capitol Hill station is located just a few blocks from campus near E John Street and Broadway. This operates 7 days a week at 7-to-8-minute headways. SDOT's First Hill Streetcar operates along Broadway near E Howell Street, directly adjacent to the campus. The Streetcar operates at 20-minute intervals.

The transit agencies have plans to increase service and frequency to campus. The 2021-2026 SDOT CIP, Sound Transit, and King County Metro Transit plans identify potential transit improvements that may impact the Campus by 2035. The proposed King County Metro RapidRide G Line (Madison Bus Rapid Transit (BRT)) is expected to be in service by 2024 along Madison Street, south of campus. The BRT line is fully funded and should open in 2024. The Culture Connector (formerly the Center City Connector) is a 1.27-mile segment of the Seattle Streetcar that will link the South Lake Union and First Hill Streetcar lines, creating a system that will connect over a dozen Seattle neighborhoods in Seattle's Center City. The City of Seattle recently restarted the planning process, and the line may open in 2026.

The expansion of Sound Transit's existing Link Light Rail is planned to include connections via the East Link (2025), Lynnwood Link (2024), Federal Way/Tacoma Link (2026/2035), and West Seattle Link (2032). Thus, more SCC faculty, staff, and students could access the campus from further afield via transit. Numerous King County Metro transit stops are located around the campus, as well as a Sound Transit Link Light Rail station and a stop for the First Hill Streetcar. An analysis of transit waiting areas was conducted, and the local transit stops would operate at LOS A or B with the increased student population. LOS A and B related to a comfortable waiting experience for riders, who have plenty of room to move about and not feel crowded. Transit vehicle ridership was measured around campus to understand if there is enough capacity (i.e., sitting and standing room) to accommodate the MIMP. Transit capacity will be utilized at approximately 80 percent or lower with the future growth anticipated for the campus and non-campus population, with estimated increases in utilization of 15 percent or less relative to existing conditions. There would continue to be available capacity to accommodate additional riders during the weekday peak periods.

Campus Parking

A new Information Technology Education Center (ITEC) would be constructed in place of the existing North Plaza parking lot. The existing driveway on Broadway would be closed. Another existing parking garage sits to the north of E Pine Street and west of Harvard Avenue, across from the bulk of the campus. This garage is intended to be replaced by student housing and new structured parking. With construction of the student housing building and parking garage, the existing driveway on Harvard Avenue will be closed, and parking access will be provided by a new access on Boylston Avenue. With the MIMP, a small parking lot located on the northwest corner of Broadway and E Pine Street will also be closed, as will the existing driveway on Broadway. New parking constructed will prioritize electric vehicles, carpooling, and other sustainable modes such as bike and scooter parking. The existing campus parking supply of 608 parking spaces would be reduced to 494. While the campus MIMP anticipates square footage to increase by about 65,000 square-feet, the parking supply rate would decrease by 19 percent, from 608 to 494 spaces. This reduction provides support for the reduced drive-alone goals of the City of Seattle.

Campus Traffic

With the full buildout of the MIMP by 2035, the campus is forecast to generate 2,800 daily vehicle trips with 228 vehicles during the weekday AM peak hour and 250 vehicles during the weekday PM peak hour. These forecasts assume a 15 percent SOV based on increased light rail use with the substantial expansion of Link Light Rail and implementation of this TMP. The current campus SOV is 19 percent.

¹ This includes 7,500 FTE students and 375 FTE staff/faculty.
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Loading and Service Facilities

Loading activities associated with service, deliveries and garbage are centralized for the existing campus operations at the Edison Building near the intersection of Harvard Avenue and E Olive Street. There are four off-street loading berths at the Edison Building. In addition, there are commercial load zones along Harvard Avenue. Short-term visitor/deliveries parking is also accommodated within the on-campus parking supply. No changes are proposed to the loading and delivery facilities with the MIMP. Centralized campus operations will continue to be provided at the Edison Building.

Transportation Management Plan Goal

A 15 percent SOV goal has been identified consistent with SMC 23.54.016 C requirements for the MIMP. The SOV goal applies to employees (staff/faculty) and students combined. For employees, the SOV goal is based on the Commute Trip Reduction (CTR) survey and is measured based on SOV trips during the commuter period, divided by the total number of CRT affected employees². For students, the SOV goal is inclusive of both commuter and resident students and is based on SOV trips occurring during the weekday PM peak hour (4-6 p.m.), divided by the total number of full time equivalent (FTE) students. The TMP SOV goal focuses on the weekday PM peak hour as that is the period with the highest congestion levels for the adjacent streets. Reducing SOV trips during this time period, will reduce congestion on the local streets surrounding the site or more regional routes such as I-5. While the SOV rates are calculated based on the definition described above, the programs that have been described in this TMP are available to all employees and students associated with the campus.

Many of the person-trips related to the campus are non-SOV. Most campus users travel by transit, bicycling, or walking. Based on the most recent 2019 surveys (pre-COVID), the SOV for SCC is 34 percent for employees and 17 percent for students. The total current campus SOV is just 19 percent, considering both the employee and student population together. The current SCC campus SOV is less than the existing MIMP goal as well as the CTR target. Given the substantial expansion of available service of the Link Light Rail to the campus by 2035, a shift in the mode split to increased light rail use is anticipated.

The institution has been actively administering its current TMP program. A 2019 CTR and student survey showed an SOV rate of approximately 19 percent for employees and students combined. The goal of 15 percent SOV represents a reduction in 4 percent from the current 19 percent SOV rate.

There are nine components of the TMP, each one contributing towards the success of the overall TMP program:

1. Transit
2. Shared-Use Transportation
3. Parking Management
4. Bicycle
5. Pedestrian
6. Marketing and Education
7. Telecommuting/Distance Learning
8. Institutional Policies
9. Monitor, Evaluate, Report



Changes to the TMP will be made as needed to achieve the TMP goal. Updates are made based on the monitoring and reporting requirements outlined in the SMC. Under each of the nine TMP components is a list of committed and potential strategies. The lists include those strategies that SCC is committed to implementing as part of the TMP. Potential strategies are identified that could be implemented in the future should additional strategies be needed to meet the SOV goal. Strategies may be implemented one at a time or in combination with other strategies. SCC may choose among these strategies or others that may be discovered during the life of the TMP, if such measures appear likely to better further the objectives of limiting vehicle trips and encouraging non-drive alone modes. The strategy lists do not reflect any prioritization. Prioritization will be determined on a year-to-year assessment of the CTR and student survey results.

1. Transit

The transit element of the TMP identifies strategies to increase utilization of transit by SCC students and employees. An integrated transit network allows users the flexibility to travel in a variety of modes and provides choices to reduce SOV trips. The TMP identifies transit strategies that are currently in practice or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Provide a 50% subsidy for transit passes for employees and students and allow employees and students participating in vanpool or carpool to also purchase an ORCA card at a 50% subsidy.
- Provide a free, unlimited, enhanced ORCA card for resident students.
- Institute an ORCA lending library for trips taken during the day, for those who do not use the ORCA subsidy.
- Non-drive alone employees are eligible for the car share benefit. SCC pays for the cost of membership and the use of the car share.
- Guaranteed Ride Home (GRH) will be offered to all employees who use non-drive alone modes and need a ride in case of emergency, illness, or unexpected schedule changes.
- Maintain clear and safe walk routes between buildings and the on-site transit stop.
- Promotions discussed below in the Marketing and Education TMP element.

² Currently defined as employees that begin work at this worksite between 6 and 9 a.m. (inclusive) on two or more weekdays for at least 12 continuous months, who is not an independent contractor, who is scheduled to be employed on a continuous basis for fifty-two weeks for an average of at least thirty-five hours per week and do not need a personal vehicle to complete their work. This definition is subject to change in response to any changes in the Washington State CTR definitions/policies.

Note the ORCA can also be used for the ferry.

Potential Transit Strategies:

- Increase the transit pass subsidy for employees and/or students.

2. Shared-Use Transportation

Shared use transportation includes a range of methods for providing flexible travel options through the sharing of transportation resources, including cars and bikes. Currently, carpools and vanpools to and from the campus are facilitated through King County Transportation – Rideshare Online. Regional ride match service allows employees and students to receive a list of potential commuters who live nearby. It is up to the individual to organize a carpool or vanpool.

The TMP identifies shared-use transportation strategies that are currently in practice or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met. The strategies that the institution is committed to implementing include:

- Offer free parking for vanpool and carpool vehicles with of three or per vehicle.
- Provide reserved preferential parking for all vanpool and carpool vehicles.
- Provide half-price parking for carpool vehicles with two persons per vehicle.
- Guaranteed Ride Home (GRH) will be offered to all employees who use non-drive alone modes and need a ride in case of emergency, illness, or unexpected schedule changes.
- Dedicated car-share parking spaces will be provided on campus and be open to the public.
- Host third-party rental bicycles, scooters or other shared micro-mobility services on campus.
- Work with SDOT to establish specific parking areas for shared bicycles and micro-mobility devices.
- Non-drive alone employees are eligible for the car share benefit. Seattle Central College pays for the cost of membership and the use of the car share.

Potential Shared-Use Strategies:

- Encourage use of new technologies to increase ease of forming carpools and vanpools on a flexible need basis. Future opportunities may exist to leverage technology to assist in ride matching.
- Partner with transit agencies to focus increased carpool/vanpool efforts on users and geographic areas currently not well served by transit.
- Consider the use of mobility options such as transportation network companies, car-share, taxis, and other shared-use service providers. If employee/student interest exists, SCC will coordinate with Ride Share Companies and provide designated spaces.
- Evaluate the feasibility of subsidizing micro-mobility trips, especially if such programs are merged with the ORCA system.

3. Parking Management

The parking proposed under the MIMP represents a balance of the needs of the institution to serve students, faculty/staff and visitors, minimize parking impacts in the surrounding neighborhood, and at the same time set the supply at a level that can discourage student and employee single occupancy vehicle (SOV) usage when coupled with the individual TMP strategies.

The TMP identifies parking management strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

- Determine guidelines for restricting resident students from bringing vehicles to campus.
- Reduce resident parking by listing remote vehicle storage suppliers, limiting residence permits, and providing residents with unlimited enhanced transit passes.
- Evaluate raising parking rates at a price that discourages driving alone and explore ways to provide parking passes to employees/staff at equitable rates.
- Provide a limited number of parking vouchers to employees without a parking permit.
- Monitor parking demand and review parking supply as part of the incremental development that would occur under this MIMP.

Potential Parking Management Strategies:

- Evaluate eliminating parking permits in favor of daily or hourly rates in the future as new parking facilities are built or parking is remodeled. Implementation will include providing technology to support this method of payment (which currently does not exist) and coordinating with the employee union on contract agreements allowing for daily and hourly parking rates.
- Reducing the total on-site parking supply proposed by the MIMP.

4. Bicycle

Seattle Central College has historically supported bicycle commuting through infrastructure and programming. SCC will continue to invest in the capacity and security of campus bicycle parking. To encourage bicycling and accommodate the growth in bicycle usage to campus, SCC is adopting a bicycle plan. As guidelines, standards established by the US Green Building Council's – Leadership in Energy and Environmental Design (LEED) program were consulted. The standards for large campus environments provide an appropriate guideline for both quantity and quality of a bicycle parking program. The bicycle plan will meet or exceed LEED Version 4.1 standards for bicycle facilities, including both short and long-term bike storage and shower facilities. Seattle Central College also provides for e-bike usage at SCC. Seattle Central College intends to provide bicycle storage and amenities that exceed City of Seattle requirements as described in the bicycle master plan.

The TMP identifies bicycle strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Adopt and implement Seattle Central College Bicycle Master Plan, which identifies bicycle parking supply that exceeds City requirements.
- Provide outdoor bicycle maintenance station(s) stocked with repair tools.
- Offer bicycle theft and safety program to provide information on bicycle registration with the open source www.bikeindex.org.
- Include indoor secure bicycle parking with new construction and expanded capital projects.
- Include secure bicycle parking in new campus parking structures
- Provide covered outdoor bicycle parking and outdoor secure bicycle parking throughout campus.
- Support a culture of indoor bicycle parking for those who do not want to park bicycles outdoors.
- Provide additional bike lockers and/or secure bike cages that accommodate traditional bikes and larger e-bikes.
- Provide charging stations for e-bikes in bike storage areas or other convenient locations.
- Support third-party rental bicycles and scooters on campus and work with SDOT to establish specific parking areas for shared bicycles and micro-mobility devices.
- Coordinate parking areas for micro-mobility devices such that locations and availability are more reliable for potential users.
- Provide showers and changing rooms in new or expanded buildings meeting or exceeding City requirements.
- Develop benefits such as discounts at local bicycle shop, periodic drawings for prizes, and individual recognition for those who use bicycles or walk to campus.
- Offer five free day parking passes per quarter to bicyclists.
- Review utilization of short and long-term bicycle parking as part of the biennial survey and reporting process and assess the results to identify needs to increase bicycle parking supply.

Potential Bicycle Usage Strategies:

- Evaluate the feasibility of subsidizing micro-mobility trips, especially if such programs are merged with the ORCA system.
- Programs including bicyclist safety training and bicycle maintenance offered throughout the year in various media formats.
- Encourage local transit agencies to identify strategies for accommodating increasing bicycle travel demand on transit.
- Monitor the existing bike parking supply throughout the campus and supplement the supply or locations as needed to encourage bicycle use.
- Consider bike share programs.
- Investigate opportunities for non-drive alone incentives within the confines of state employee regulations.

5. Pedestrian

Everyone is a pedestrian at some point in their trip. At SCC, students and staff will walk from parking, from the transit stops, between buildings, and to and from bicycle parking.

The MIMP would improve on-campus connections and provide required frontage improvements where new buildings are constructed. Facilities will be designed to minimize vehicular/pedestrian conflicts and encourage non-vehicle commuting.

The existing campus provides extensive pedestrian amenities throughout campus and to adjacent roadways, transit, and sidewalks. A new midblock crossing will be added as part of the MIMP on Nagle Place between the Student Union building and Cal Anderson Park.

The TMP identifies pedestrian strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Protect and improve upon the pedestrian experience within the Seattle Central College campus. Make all transportation choices, policies, and improvements supportive of the pedestrian environment and experience.
- Maintain the on-campus pedestrian network, including addressing ADA accessibility
- Provide ADA accessible routes throughout the site and during any on-site construction periods.
- Provide safe pedestrian environments by giving attention to lighting, visibility/safety along walkways, etc.
- Offer five free day parking passes per quarter.
- Evaluate possible infrastructure improvements for implementation in each new project.
- Work with SDOT to ensure marked crosswalks are clearly visible for roadways around the campus.

Potential Pedestrian Strategies:

- Investigate opportunities for non-drive alone incentives within the confines of state employee regulations.

6. Marketing and Education

Marketing and education are essential to build understanding and support of the TMP's goals and objectives. The transportation coordinator (TC) role will be maintained to ensure that all aspects of the TMP are promoted and implemented. The TC will ensure that commuter information resources are provided consistently to employees and consistently market to and educate site employees on alternatives to driving alone.

The TMP identifies marketing and education strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Appoint Transportation Coordinator (TC) and ensure TC role is permanently staffed.
- TC will participate in Transportation Management Association (TMA) programming, attending at least 1 training per year.
- Focus efforts on new employees, new students, people who are moving homes, and people whose transportation options have changed.
- Provide information to staff regarding biking, walking, transit, carpooling, and telecommuting options. Provide information to regarding biking, walking, transit, and carpooling.
- Encourage use of non-auto modes or SOV travel.
- A commuter information center (CIC), including ridesharing, transit, bicycle, and pedestrian information, will be in a convenient location for students and employees.
- Transit kiosk(s) will be provided on campus (including within Student Housing) and include live/online trip planning web access at each kiosk as well as information on transit arrival times for nearby bus and light rail stations.
- TMP information including transit service and subsidy, parking rates and rideshare discounts, ride match assistance, guaranteed ride home and other elements will be made available on the SCC website for employees and students.
- Visitors will be provided information on commuting to campus on the SCC website including transit, biking, parking and other non-drive alone modes.
- Organize special promotions supported by transit agencies and other agencies supporting commuter services such as King County Metro, Sound Transit, Commute Seattle, City of Seattle and private operators.
- Coordinate special promotional events to correspond with special events sponsored by King County Metro and other entities such as Oil Smart promotional campaigns

Potential Marketing and Education Strategies:

- Promote national modal days (e.g., Bike to Work Day, Take Transit to Work Day, etc.).

7. Telecommuting/Distance Learning

Telecommuting/hybrid or distance learning is not practical for all staff and students. Provisions for telecommuting or hybrid work will be based on the functional requirements of the job and the needs of the campus administration. Distance learning will be based on course needs/requirements.

The TMP identifies telecommuting/distance learning strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Establish policies and promote telecommuting, hybrid, flex-time, compressed workweeks, and other techniques that reduce peak period travel.
- Explore opportunities to continue to provide educational instruction through "distance learning".

No potential strategies have been identified. If additional strategies are necessary those would be defined as part of the ongoing TMP reporting process.

8. Institutional Policies

Seattle Central College can modify and implement institutional policies that promote different modes of travel and/or reduce vehicle trips on the transportation network. While the other TMP elements provide transportation choices, institutional policies are aimed at reducing the SOV rates and controlling forecasted growth of SOV vehicle trips.

The TMP identifies institutional policy strategies that are currently in practice, or the institution is committing to and potential future strategies that could be utilized if the SOV goals are not being met.

The strategies that the institution is committed to implementing include:

- Determine guidelines for restricting resident students from bringing vehicles to campus.
- Evaluate raising parking rates at a price that discourages driving alone and explore ways to provide parking passes to employees/staff at equitable rates.
- Continue to promote compressed work weeks during the summer.
- Allow individual departments to determine compressed work weeks or flex-time schedules throughout the academic year.

Potential Institutional Policy Strategies:

- Manage employee/class schedules, to the extent feasible, to limit commuting activity during the weekday peak commute periods.

9. Monitor, Evaluate, Report

Seattle Central College has an extensive program of monitoring, evaluating, and reporting transportation conditions. SCC will continue to monitor and report on its progress toward meeting the revised TMP goal of limiting SOV travel in compliance with CTR and MIMP Annual Report requirements. This is anticipated to include observations of vehicular and bicycle parking demand and utilization to be conducted in junction with the CTR and student surveys. Additionally, questions are included in the CTR and student surveys to help assess commuter needs and barriers to employees and students utilizing non-drive alone transportation modes to assist in identifying opportunities to improve the TMP and select appropriate potential strategies to implement.

The institution is committed to implementing:

- Conduct periodic surveys of TMP effectiveness, as established by the City at least once every two years for both students and employees. At a minimum use the WSDOT/CTR survey standards for the core survey questions and required response rates.
- Submit regular reports about TMP elements as required by the City at least once every two years, in non-survey years.

Potential Monitor, Evaluate, and Report Strategies:

- Establish a working group with internal and external stakeholders to support the TMP goal.

APPENDIX A DEFINITIONS

Acronyms and Abbreviations

The following definitions apply to terminology used throughout this Major Institution Master Plan document. If a term is not defined herein, the definition shall be per the Definitions section of the land use code found in the SMC 23.84A. (Seattle Municipal Code (SMC), title 23, chapter 84A. See:

https://library.municode.com/wa/seattle/codes/municipal_code?nodeId=TIT23LAUSCO_SUBTITLE_IVAD_DIV2GETE

BEC	Broadway Edison Complex
CAC	Community Advisory Committee
DAC	Development Advisory Committee (formerly known as CAC)
EO	Executive Order (State of Washington)
EIS	Environmental Impact Statement
FAR	Floor Area Ratio
ICP	Internal Concept Plan
LEED	Leadership in Energy and Environmental Design
MIMP	Major Institution Master Plan
MIO	Major Institution Overlay
ROW	Right-Of-Way
SAM	Science and Math building
SBCTC	State Board of Community and Technical Colleges
SCC	Seattle Central College
SMC	Seattle Municipal Code
SOV	Single-Occupancy Vehicle
TMP	Transportation Management Plan
VMT	Vehicle Miles Traveled

Definitions

- Alley** "Alley" means a public right-of-way not designed for general travel and primarily used as a means of vehicular and pedestrian access to the rear of abutting properties. An alley may or may not be named.
- Arterial** "Street, arterial" means every street, or portion thereof, designated as an arterial in SMC Exhibit 23.53.015 A.
- Designated Open Space** Open space within the MIO District that is significant and serves as a focal point for users of the Major Institution, per SMC 23.69.030.E.4.b.
- Environmental Impact Statement (EIS)** An "Environmental Impact Statement" is required by the State Environmental Policy Act. As used in this title, the term refers to a draft, final or supplemental EIS.
- Floor Area Ratio (FAR)** Means a ratio expressing the relationship between the amount of gross floor area or chargeable floor area permitted in one or more structures and the area of the lot on which the structure is, or structures are, located, as depicted in Exhibit [23.84A.012](#) A with exceptions stated in this MIMP.
- Gross Floor Area** "Gross floor area" means the number of square feet of total floor area bounded by the inside surface of the exterior wall of the structure as measured at the floor line. Gross floor areas for future projects identified in this MIMP are approximations and are usually rounded to the nearest 1,000 square feet.
- Internal Concept Plan (ICP)** The "Internal Concept Plan" is the first step of the formal MIMP process, as specified in SMC 23.69.032.C.
- LEED** Leadership in Energy and Environmental Design; refers to the "Green Building Rating System" developed and maintained by the United States Green Building Council. The USGBC describes LEED as a "third-party certification program and the nationally accepted benchmark for the design, construction, and operation of high-performance green buildings."

- Landmark Structure** "Landmark structure" means a structure designated as a landmark, pursuant to the Landmark Preservation Ordinance, Chapter 25.12.
- Lot Coverage** "Lot coverage" means that portion of a lot occupied by the principal structure and its accessory structures, expressed as a percentage of the total lot area, refer to SMC Exhibit 23.84.024 B.
- Major Institution** "Major Institution" means an institution providing medical or educational services to the community. A Major Institution, by nature of its function and size, dominates and has the potential to change the character of the surrounding area and/or create significant negative impacts on the area. To qualify as a Major Institution, an institution must have a minimum site size of sixty thousand (60,000) square feet of which fifty thousand (50,000) square feet must be contiguous and have a minimum gross floor area of three hundred thousand (300,000) square feet. The institution may be in a single building or a group of buildings which includes facilities to conduct classes or related activities needed for the operation of the institution.
- Major Institution – Educational** Educational Major Institution means an accredited post-secondary level educational institution, operated by a public agency or non-profit organization, granting associate, baccalaureate and/or graduate degrees. The institution may also carry out research and other activities related to its educational programs.
- Major Institution Master Plan** The intent of the "Major Institution Master Plan" shall be to balance the needs of the Major Institutions to develop facilities for the provision of health care or educational services with the need to minimize the impact of Major Institution development on surrounding neighborhoods.
- Modulation** Means a stepping back or projecting forward of sections of the facade of a structure within specified intervals of structure width and depth, as a means of breaking up the apparent bulk of the continuous exterior walls (Exhibit A for [23.84A.025](#))
- Neighborhood Plan** "Neighborhood plan" means a plan adopted by the Council which has been developed to guide neighborhood growth and development and deal with other neighborhood related issues such as housing, institutions, transportation, economic development, and other community development activities.
- Open Space** "Open space" means land and/or water area with its surface predominately open to the sky or predominantly undeveloped, which is set aside to serve the purposes of providing park and recreation opportunities, conserving valuable natural resources, and structuring urban development and form. See also Designated Open Space.
- Overlay District** "Overlay districts" are established to conserve and enhance the City of Seattle's unique natural marine and mountain setting and its environmental and topographic features; to preserve areas of historical note or architectural merit; to accomplish City policy objectives for specific areas; to assist in the redevelopment and rehabilitation of declining areas of the City; to balance the needs of Major Institution development with the need to preserve adjacent neighborhoods; and to promote the general welfare by safeguarding such areas for the future use and enjoyment of all people.
- Application of Regulations** Property located within an overlay district as identified on the Official Land Use Maps, Chapter SMC 23.32, is subject both to its zone classification regulations and to additional requirements imposed for the overlay district. In any case where the provisions of the overlay district conflict with the provisions of the underlying zone, the overlay district provisions shall apply.

Pedestrian Designated Zone

A pedestrian designation (a "P" suffix to the standard zoning designation) indicates that such areas are intended to create a pedestrian-oriented environment. Pedestrian designated development regulations apply to projects located within a pedestrian designated zone where they front onto a designated principal pedestrian street, as identified in SMC 23.47A.005.E.2. The location of uses in pedestrian-designated zones are described in SMC 23.47A.005.E.1. Other street-level development standards for pedestrian designated zones are found at SMC 23.47A.008.C.

Planned Projects

"Planned Projects" are those that the College has definite plans to construct as funding (public or private) becomes available. In general, these projects are anticipated to be developed in the next 10 years.

Potential Projects

"Potential Projects" are less definite than "Planned" but could be constructed in the as needs arise and funding becomes available.

Setback

Means the minimum required distance between a structure or portion thereof and a lot line of the lot on which it is located, or another line described in a particular section of this title.

Zoning Designations

The following General zoning designations are noted throughout this master plan document. These classifications are established by the Seattle Municipal Code - SMC 23.30.010.A. Detailed explanations of the designations are indicated in SMC 23.34.010 through 23.34.086 which can be found at:

https://library.municode.com/wa/seattle/codes/municipal_code?nodeId=TIT23LAUSCO_SUBTITLE_IIILAUSRE_CH23.34AMOFLAUSMARE_SUBCHAPTER_IIRECR_23.34.010DESF5000SF7200SF9600ZO

NR1	Residential Neighborhood 1
LR1	Residential Multifamily Low-rise 1
LR2	Residential Multifamily Low-rise 2
LR3	Residential Multifamily Low rise 3
MR	Residential Multifamily Midrise
HR	Residential Multifamily Highrise
RC	Residential-Commercial
C2-65	Commercial 2 - 65'
NC1-30	Neighborhood Commercial 1 - 30'
NC2-40	Neighborhood Commercial 2 - 40'
NC3-40	Neighborhood Commercial 3 - 40'
NC3-65	Neighborhood Commercial 3 - 65'
NC3-90	Neighborhood Commercial 3 - 90'
NC3-160	Neighborhood Commercial 3 - 160'
P suffix	Pedestrian Designated Zone (as overlay)

RESPONSE TO CAC COMMENTS ON CONCEPT MASTER PLAN

The following comments were received from the Seattle Central Community Advisory Committee on March 2, 2021. Please see below for responses provided by Seattle Central College

Historic, Arts, and Cultural Spaces

- Comment 1 The college is located within the Capitol Hill Arts District. The District is home to diverse groups of arts and cultural organizations making it one of the densest arts communities in the State of Washington. The neighborhood is experiencing rapid change and gentrification. Existing arts organizations are under real threat of being displaced by rising rents and redevelopment.
- Response 1 Agreed. No further response provided for this statement.
- Comment 2 The committee understands the college's limitations with regards to funding new projects, and recognizes it is unrealistic that they would receive funding to perform significant renovation outside regular maintenance not outlined in this Major Institution Master Plan (MIMP).
- If the opportunity to redevelop the Fine Arts Building, Erickson Theater, and/or Broadway Performance Hall were to arise, the committee recommends the college support the preservation of these historic and cultural assets.
- Response 2 A full renovation of the Broadway Performance Hall (BPH) is a Planned Project. SCC will comply with all DAHP and the City of Seattle's Landmarks Nomination processes.
- The master plan does not include any Planned or Proposed projects for the Fine Arts or Erickson Theater building. SCC will comply with all DAHP and City of Seattle Landmarks Nomination processes for the Fine Arts and Erickson Theater buildings.
- SCC acknowledges and supports the aspirational goal of maintaining community access to BPH for cultural arts and will commit to efforts to maintain it. However, the need to maximize the use of the BPH to support College mission and goals must be the College's priority.
- Comment 3 The college has historically provided use of its performing arts spaces to student and the public. Prior to pursuing transfer of ownership/operation of these performing arts spaces, the committee recommends that the college actively pursue both private and public partnership opportunities that will enrich both the college and Capitol Hill community.
- To offset maintenance and operations costs and increase student enrollment, should the need to sell the these performing arts spaces arise, the committee strongly recommends the college find a buyer who will support arts and culture uses in the community after following the required disposition process.
- Look to similar partnership models for guidance:
- Historic Seattle: operates Washington Hall
 - Cornish College of the Arts Raisbeck Performance Hall
 - [City of Seattle Structure for Stability - Recommendations For Developing Affordable Community-Based Cultural Space April 2019](#)
- Response 3 SCC understands these venues are a community asset. It also acknowledges and supports the aspirational goal of maintaining public access to these facilities. SCC has, for several years, actively worked with community arts organizations as it seeks to maximize the use of, access to, and to offset the costs of maintenance and operations.
- SCC remains committed to its past and on-going efforts, However, the need to maximize the use of these venues to support College mission and goals must be the priority. SCC acknowledges and supports the aspirational goal of maintaining community access to its cultural assets for arts and will commit to efforts to maintain it.

- Comment 4 When a Master Use Permit (MUP) application impacting a structure or place that is 50 years or older is referred to the City's Historic Preservation Officer (CHPO), the committee recommends that the college commit to affirmatively supporting the landmark process and advocate on behalf of the historic places and structures that will be impacted.
- Response 4 SCC will comply with all DAHP and the City of Seattle's Landmarks Nomination processes. As part of those processes, SCC will engage independent historic preservation consultants to prepare and make all recommendations regarding any nominations. SCC supports the aspirational goal to preserve historic structures. However, the need to maximize the use of all facilities to support College's mission and goals must be the priority.
- Comment 5 The committee recommends the "Cultural Spaces (Resources) in Vicinity Map" should be corrected to show a more accurate accounting of cultural and performing arts spaces in the vicinity using the list compiled by the Office of Arts & Culture found [here](#) and updated to reflect groups that are no longer in operation on Capitol Hill.
- Response 5 The Cultural Spaces (Resources) in Vicinity Map has been updated.

MIO Boundary/Alternatives and Decentralized Options/Planned Projects/Potential Projects

- Comment 6 The committee supports the College's need to plan for future expansion in the Capitol Hill neighborhood but is reluctant that the Lenawee building is the best place to do this.
- The committee believes the Lenawee building is an asset for the neighborhood because of the housing it provides, as well as its architectural interest, regardless of any historic relevance. The college's limited funding for development and maintenance of their campus is an obstacle as this building may require significant funding to convert to another use or preserve long-term when those funds could be more efficiently used elsewhere.
- If the college does proceed with including the Lenawee in the MIO, the committee would ask that the college present in the Draft MIMP mitigation measures to offset the loss of housing and architectural interest if the building were to be demolished.
- Response 6 If the Lenawee building is acquired by SCC, it will comply with all DAHP and the City of Seattle's Landmarks Nomination processes. Further, SCC will commit to considering the highest and best use of the Lenawee building for college needs including its re-use for housing or other appropriate administrative need. If the SCC's needs at the time of any proposed development do not support preserving and re-use of the building, SCC reserves its rights to develop the site for the highest and best college use.
- Mitigation for the loss of architectural interest will be accommodated by any determinations made by DAHP or the City of Seattle Landmarks process. Any mitigation for the loss of housing will be addressed by requirements of the Seattle Municipal Code - Chapter 23.69
- SCC's recent parcel transactions, have substantively increased availability of housing in the Capitol Hill neighborhood. This includes:
- SCC exchanged four parcels (totaling 29,760 square feet) adjacent to the Broadway Pine intersection, for Sound Transit's Site D (10,383). As part of the agreement, the exchanges parcels will be developed by Community Housing organizations and are expected to result in:
 - Estimated 125 housing units for LGBTQ-Affirming Affordable Senior Housing (development by Community Roots Housing and Rise Together) – [find more information here.](#)
 - Estimated 100 Affordable housing units, including 70 housing units for homeless youth (development by Community Roots Housing and YouthCare) – [find more information here.](#)
 - SCC's Planned Project for Student Housing will add over 500 beds for SCC students. By providing affordable student housing for SCC student, the availability for affordable housing in the neighborhood will increase.

Comment 7 If the college would like to include properties west of Harvard Ave, the committee recommends the college to consider the three parcels south of the Presbyterian Church for inclusion in the Major Institution Overlay (MIO) boundary as they currently represent great redevelopment potential and are unlikely to be designated as landmarks.

Response 7 The College will consider acquisition of any parcels that may become available adjacent to, or in close proximity to the proposed MIMP boundary. These specific parcels are of relatively small dimension and are not generally conducive to college needs of larger developments. As there are currently no college needs that are aligned with these parcels' development characteristics, the College is not including them in the proposed MIMP Boundary at this time. If the parcels become available, and the College is interested in acquiring them, the SAC (Standing Advisory Committee) will be engaged.

Comment 8 The committee is open to further discussion with the college about the addition of a building at the corner of Broadway and Pine. This would decrease the size of the South Plaza/Green but bring constructive energy and activity to the space, which is a goal for this committee.

Response 8 The College appreciates the CAC opening this comment for consideration. At this point in time, the College does not envision a space need or funding that would be appropriate for this location. A Sound Transit easement below this area also creates complexities to development that need study before and planned or potential development on this site.

Campus Security Guidelines

Comment 9 New building construction shall be designed to meet a unifying standard for campus infrastructure to tie separate college spaces together. Where feasible, existing infrastructure should be altered to match the same standards and requirements. This will ensure people are aware of the boundaries of the campus and feel welcome in its public spaces. These modifications shall address the following considerations:

- a. Provide lighting improvements along building facades, streets, and sidewalks to promote nighttime activities and safety.
- b. Unify wayfinding that clearly articulates locations, access points, and routes through campus.
- c. Tie signage and graphics within the campus together to create a unified campus.
- d. Install plantings, hardscape, and building materials that encourage safety while promoting natural, organic forms that the community can respect and protect.
- e. Provide transparency opportunities per Recommendation #10.

Response 9 SCC is in agreement with these comments. Please see the proposed Design Guidelines on [Campus Safety and Security, Campus Identity, Open Space, Green Space, and Lighting](#) for more information. These are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP.

Comment 10 The South Pine Plaza is the first physical impression that a visitor to the college experiences and should be a celebrated gateway and identifying feature of the college that conveys an open and welcoming environment. The space currently does not reflect inclusive values and is not well integrated with the surrounding built environment.

The committee recommends that the college redesign the South Pine Plaza as an extension of the Broadway Performance Hall renovation to maximize safety while simultaneously recognizing the long and storied role the plaza has played in supporting peaceful protest and exercise of free speech and preserving it for continued civic use. The following design outcomes should be pursued to make this space feel welcome, inviting, and safe for students, staff, faculty, and the public:

- a. Increase visibility and decrease available hiding spaces at night.
- b. Improve access and visibility to transportation at the adjacent bus stops as well as the light rail and streetcar stations.
- c. Improve ADA accessibility.
- d. Retain the plaza as a public gathering space and as a green space/respice from the busy urban life and street noises adjacent to it.
- e. Accommodate multiple levels of scale and use ranging from individual contemplation to markets to socially designated civic gathering space. This design should ensure that pedestrians always have unobstructed access around the South Pine Plaza and into the college campus regardless of what scale the plaza and glade are at that moment being used for.

f. Eliminate the exposed subterranean portions and associated fencing of the plaza. The reclaimed plaza space should support varying levels of scale and use, integrate well with the surrounding buildings, and use a mix of plants and hardscape for the maximum benefit of the community.

Response 10 SCC is in general agreement with the comments provided. However, the planned Broadway Achievement Center project (former Broadway Performance Hall) is a State-Funded Renovation. Its available funding does not include funds for exterior site improvements. SCC cannot commit to any redesign of the South Plaza as part of this project. SCC will consider the above aspirational comments to the extent achievable as additional funding sources become available.

Should the College undertake any institutional development in the South plaza area, SCC will incorporate the recommendations as achievable within the extents of the project development and funding.

Comment 11 Evaluate the considerations of Crime Prevention Through Environmental Design (CPTED) to ensure requirements selected are relevant for non-discriminatory and equitable safety around the campus. CPTED principles can have inequitable and discriminatory impacts because of implicit biases of individuals only considering the perception of personal safety within a context of systemic racism. Environmental design tactics can promulgate existing prejudices and result in BIPOC, and lower income people being reported to police more often than white people before they have committed any offenses. Any CPTED measures implemented shall minimize threats for all people from public, staff, and authority (administration, police, etc.) and not exclude activities such as using the plaza for personal rest and enjoyment, entering the building safely as a public person, or congregating with people of the same ethnicity or socio-economic status. When applying CPTED principles to future projects, the CAC recommends the following objectives be met:

- a. Strive for a culture of connection and belonging with safety as the outcome.
- b. Create solutions for more interior active spaces along street fronts to encourage "eyes on the street." This approach may include interior renovation of existing buildings to remove private offices from street facades.
- c. Create safe spaces for all people by allowing safe resting areas with appropriate seating, lighting, garbage and recycling stations, and other common amenities
- d. Educate all occupants on the policies and communities that are welcome in the plaza and park areas surrounding the school and how to approach security without immediate involvement of police.

Response 11 SCC is in agreement with the comments provided. Please see the proposed Design Guidelines on [Campus Safety and Security](#) for information on how the College proposes to address. These are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP.

Comment 12 The diverse community of and around Seattle Central College shall be actively welcomed on the campus to participate in community-oriented activities and public functions. The college shall recognize its interconnectivity with the surrounding community and actively engage with the broader Capitol Hill neighborhood when planning for and providing a campus environment that is safe for all. When proposing projects in the MIMP, the college shall emphasize the surrounding community and provide safety for all groups. The buildings and alleys surrounding the campus are all intertwined to the safety and community of everyone. The college can support a safe community through the following considerations:

- a. Foster a campus environment that is welcoming, comfortable, and safe for students, staff, and the broader community; and
- b. Provide porosity of campus buildings through glazing, materiality, and scale that create welcoming spaces for all.

Response 12 SCC recognizes the important role the campus plays in Capitol Hill and will continue striving to be a good neighbor and steward in the community. The primary concern of the College is the education, health, and safety of students, faculty, and staff. When planning for any future projects, the needs to the public will be taken into consideration with the needs of the College. Providing intentional outdoor spaces for use by the College and community at-large will be a goal of any new or renovation projects where applicable.

Please see the proposed Design Guidelines on included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP. Included throughout these guidelines are numerous commitments that will promote engagement with the varied Capitol Hill communities and create and open and inclusive environment.

Parking and Transportation Provisions

Comment 13 The college's current transportation management plan (TMP) study does not adequately assess the complex intersection of transportation modes surrounding the campus and is devoid of any studies or assumptions made on the future impact of the COVID-19 pandemic on transportation to and from campus. The scope of the TMP should be expanded to provide more detailed information on transportation modes to inform the college's decision on how to shift single occupancy vehicle (SOV) commutes to alternative forms of transportation. This information should also be used to determine whether or not the college should maintain their total existing parking capacity. The college shall provide information in the areas requested below and take the following actions:

- a. Make assumptions for future demand and mode splits on possible post-COVID scenarios of higher education for students and college employees by making decisions based on data and predictions. The TMP currently does not attempt to plan for a post-COVID world or how the pandemic could alter future mode demand. The college shall rectify this by preparing for a wide range of outcomes and develop the planning capacity to quickly adapt to any possible increase or decrease in transportation and parking demand.
- b. Partner with the greater Seattle College system and Sound Transit to expand parking options for students and college employees who live outside of walking distance to campus, providing an opportunity for them to utilize mass transit. Specifically:
 - i. Support and advance efforts to create provisions for shared parking within the Seattle college system, especially for students and employees living in and around Northgate.
 - ii. Identify Sound Transit parking garages and King County Metro park and rides that connect with rail, bus and other transit options that are or could be frequently used to travel to campus and attempt to make provisions for college parking at those locations.
- c. Study current trends in rideshare pick-up and drop-off locations on campus and work with major rideshare companies to establish designated loading zones that mitigate disruptions to the campus and surrounding streets.
- d. Conduct a study to determine the potential benefits and costs of constructing a direct, subterranean connection between the Capitol Hill Link light rail station and the campus, and then present these findings to the SAC.
- e. Provide supportive data for projected participation in carpool, carshare, vanpool, rideshare, bikeshare (bikes, scooters, and comparable modes) and electric vehicle participation.
- f. Provide information on the impact of new MIMP projects, especially the student housing structure and garage, on traffic and on the supply and demand of parking spaces to serve the college and community's needs.
- g. Encourage and incentivize greater transit ridership. Provide specific information on transit subsidies and how those programs can be expanded both in coverage and participation rate. We applaud the college for providing subsidized ORCA passes to faculty, staff, and students—and urge the college to work with the City and the County to find a way to provide those passes free of charge to these members of the College community.
- h. Provide information and analysis on how future transit expansions in the Link Light Rail and Seattle Streetcar systems will affect transportation and parking demand on campus.

Response 13 The purpose of the Transportation Management Plan (TMP) is to require and track the reduction of Single Occupant Vehicle (SOV) trips to campus. A TMP does not include any analysis or assessment. It is the purpose of the EIS to appropriately assess the multitude of transportation modes available to the SCC campus and to provide data to the City to be utilized in the creation of the new TMP.

SCC is fully committed (and required) to achieve the goals establish by the new TMP. It is the City's right to withhold future development of campus until such time as TMP goals are achieved. Further, the TMP is reviewed and assessed with each project development at the time they are permitted.

- a. The impacts of the Covid-19 pandemic on higher education operations are simply not known at this point in time. While there are certainly changes to the way education can be delivered, the ultimate efficacy of remote/online instruction varies wildly across different instructional programs and teaching pedagogies. At this point in time, any assumed changes to the needs of parking or transportation would be based on incomplete data and anecdotal evidence.

What certainly appears to true, is that certain aspects of college operations and instruction can be effectively delivered without being on Campus. The results of this will undoubtedly be a reduction in SOV trips to campus.

- b. SCC already partners with the other Seattle Colleges as part of a shared parking program. All SCC faculty, staff, and students have access to parking at other campuses. North Seattle College, and its proximity to the Northgate Light Rail station provides easily accessible parking for those living north of campus. SCC has previously engaged with Sound Transit about preferred access at park and ride venues. Sound Transit has to date, not been willing to provide preferred access as they make all parking available free, and on a first-come, first-served basis. SCC has also discussed access at Metro Park and Rides, but previous discussions have not yielded joint benefits. SCC is committed to continuing discussion with both agencies to make access and use readily and easily available to all faculty, staff, and students.
- c. The major rideshare companies have already established pick-up and drop off zones at campus locations (currently at the intersection of Harvard and Howell). As part of any redevelopment of the Howell Street Passage, the College will commit to engaging with SDOT and the City to include frontage improvements that will include a dedicated rideshare zone.
- d. The cost and scope of an underground connection to the Capitol Hill Link Station will be studied at the time ITEC project design commences.
- e. The EIS will provide mode share analysis and the College will conduct annual monitoring and survey of use. The results will dictate further efforts to reduce the targeted SOV rates till the TMP is in compliance.
- f. The EIS will provide an analysis of the projected impacts on each proposed project has on parking, traffic, and transportation.
- g. The purpose of the TMP is to encourage and incentivize greater transit ridership. SCC offers discounted, pre-loaded ORCA cards to all students and encourages students to see if they're eligible for ORCA LIFT reduced fair rates. More information on student transit passes is [available here](#). For SCC employees, subsidized ORCA cards are available for purchase by permanent, fulltime, or benefits eligible employees. More information on employee transit passes is [available here](#). Currently, the College is able to offer discounted transit passes to student and employees because of the revenue generated from parking fees.
- h. This is covered by the EIS process.

Comment 14 The committee understands that while the college is an asset to the region, it is also a major SOV trip generator which contributes towards vehicular congestion and the release of carbon emissions. The college shall mitigate and reduce these detriments through encouraging and incentivizing students and employees to take non-SOV transportation modes to campus.

The committee commends the college on meeting their current TMP goals and expects that the college will continue to provide at a minimum, the same Commute Trip Reduction (CTR) strategies and incentives currently offered, such as the guaranteed ride home program, as a means for maintaining current mode shares and increasing confidence in transit usage.

Response 14	As part of the approved TMP the College will be required to take further efforts to reduce SOV trips and increase transit usage. SCC plans to continue offering a range of incentive programs including subsidized transit passes for faculty and students to promote use of public transit, the above-mentioned guaranteed ride home program, and others. The College can fund these programs in part because of revenue from parking garage usage. The College has not and will not incentivize SOV trips and will continue to explore options for encouraging use of mass transit and other personal transportation options.	Response 18	SCC is committed to being a good neighbor and fostering a safe environment within and around the campus. As required by law, the College will work with tenants within the parking garage when/if construction of the Student Housing project commences. Until such time, the College will review safety issues as they arise.
Comment 15	The committee understands that many people still rely on personal vehicles to get to campus due to a lack of affordable housing within the city core and a lack of adequate transit options to campus from more distant parts of the region. As such, the college shall retain a reasonable amount of parking spaces on campus. However, the college shall not provide more parking than what is found to be necessary per the ultimate results of the TMP.	Comment 19	The college shall encourage the use of alternative fuel vehicles for transportation to campus by installing charging stations in garages and/or retained surface parking lots. Where and when possible, charging stations shall be made available for use by the surrounding neighborhood.
Response 15	SCC will continue to encourage use of mass transit, ride share, and other mobility options. The College does not plan to expand current parking capacity as identified in draft EIS/TMP studies and diagrams.	Response 19	SCC plans to renovate the parking facilities in the existing Harvard Garage when the site becomes Student Housing. Parking facilities will also be added with the ITEC project. If funding and scope allows, the College will provide e-bike and electric vehicle charging stations. The College will prioritize student, faculty, and staff use. Use by the public and surrounding neighborhood will be considered where appropriate. In College parking facilities, secure bicycle storage will be provided.
Comment 16	The college's parking garages and surface lots are an asset to the surrounding community and should be made accessible for the public when possible. The following actions shall be taken by the college: <ul style="list-style-type: none"> a. Provide a market rate study for setting parking rates within the TMP. b. Offer parking for neighborhood residents, businesses, and visitors when parking assets are underutilized by college community (weekends, holidays, etc.). c. Dedicate parking stalls in the garage for the exclusive use of vanpool transit or other "last-mile" transit options. d. Participate in the e-park program by installing parking space sensors and signs that reflect the real-time amount of parking available within all garages and surface lots and compliment the system by posting real-time availability online and in integrated apps. 	Comment 20	Vehicular curb cuts create safety concerns for pedestrians and disrupt traffic flow. The college shall avoid creating new vehicular curb cuts on streets fronting the campus, with the exception of relocated curb cuts for the new student housing building. The committee approves of the college's current plan to locate the new ITEC garage on Harvard Avenue and merge it into a joint access entrance with the Math and Sciences parking garage. The college shall not locate the entrance anywhere else without the consultation of the SAC. If service vehicles need access to maintain building facades with lightweight vehicles, small curb cuts can be permitted for these uses only and should be complemented with bollards to prevent unplanned access.
Response 16	The Harvard Garage is available for the public to use at all hours; however college users typically take most available spaces by late morning (per the presentation by TranspoGroup dated October 19, 2020). <ul style="list-style-type: none"> a. Existing parking rates are established by an internal assessment by the college. The assessment seeks to balance affordability for faculty and staff, with availability for public use. Rates seek to maximize revenue from public use as it is a resource for funding student tuition need. While not a formal market rate study, the college believes it is balancing the needs effectively. b. The College currently offers this service to the public. c. The College would support adding stalls for vanpool transit if a need is determined. The College has provided this service in the past however it was seldom used. The priority should be for more effective options. d. The College's will consider this recommendation as part of the Planned Student Housing and ITEC project. 	Response 20	The College agrees with the above comment. The College is currently studying creating a curb cut for service vehicle access to campus at the intersection of Harvard Ave and E Howell St. The College agrees that providing a safe environment for pedestrians is of the utmost importance. No other curb cuts are planned at this time.
Comment 17	The college shall relocate the entrances to the new garage structure farther north on Boylston and/or Harvard, as far north as legally and practically feasible, to reduce congestion from vehicles queuing up on Pine Street.	Comment 21	The college shall make the following pedestrian street crossing improvements: <ul style="list-style-type: none"> a. Evaluate and propose crosswalk improvements for the mid-block Broadway crossing between the main campus and the MAC/Bookstore. b. Evaluate and propose crossing improvements on Harvard Avenue, between East Howell Street and East Olive Street, and on Nagle Place to and from Cal Anderson Park. c. Implement personal safety treatments that contribute towards pedestrian safety, such as LED flashing pedestrian signs, as permissible by local regulations.
Response 17	The College agrees that relocating the entrance is in the best interest of traffic and pedestrian safety. SCC will study all feasible options for redevelopment and restructuring the Harvard Garage for its future as a joint location with Student Housing. Preliminary plans show a single garage entrance/exit on Boylston Ave to consolidate vehicle traffic and create a safer pedestrian space on Harvard Ave.	Response 21	SCC will work the City of Seattle and future design consultants to provide street crossings that put pedestrian safety first. Projects will be taken on as funding and project scope allow (i.e., Student Union renovation scope overlaps with improvements on Nagle Place to/from Cal Anderson Park). Please see the proposed Design Guideline(s) section(s) on <u>Pedestrian Circulation</u> and the <u>Aspirational Guideline Traffic Calming on Harvard Avenue between Pike and Denny</u> (included under the section Campus Safety and Security) for more information. These sections are included in Chapter 4, Section 4. <ul style="list-style-type: none"> a. In the Seattle Streets Illustrated map, Broadway classifies as an Urban Village Neighborhood and Minor Arterial street-type, Pedestrian Zone, and Priority Investment location as part of the Pedestrian Master Plan. Based on this, the mid-block crossing could be eligible for a median/crossing island, curb bulb, and contrast surface treatments/ material in the crossing area. As the Streetcar runs on this street, this mid-block crossing is likely ineligible for a raised crossing. b. In the Streets Illustrated map, Harvard Avenue, E Howell St, and E Olive St classify as Urban Village Neighborhood Access street-type, and Priority Investment location as part of the Pedestrian Master Plan. Based on this, the crossings could be eligible for curb bulbs, contrasting surface treatments/ material in the crossing areas, raised crossings, and raised intersections. Nagle Place classifies as an Urban Village Neighborhood Access street-type, Pedestrian Zone, and Priority Investment and Missing Sidewalk location as part of the Pedestrian Master Plan. Based on this, the mid-block could be eligible for curb bulbs, contrasting surface treatments/ material in the crossing areas, raised crossings, and raised intersections.
Comment 18	The committee recommends the following for the existing parking garage: <ul style="list-style-type: none"> a. The committee supports state requirements for tenant coordination and relocation assistance with proposed redevelopment impacts. The committee recommends that the college mitigate construction impacts on the current two tenants if they continue to operate during construction. b. If the existing parking garage remains standing due to significant delays or termination of the new student housing project, the college should look for ways to improve safety and increase utilization of the parking structure by students, faculty, and the community by increasing perceptions of personal security. 		

	c. The College will work with SDOT to implement safety enhancements at crossings as development adjacent to said crossings occurs or as suggested by SDOT.		arborist, whose recommendations for tree maintenance or removal will be taken into consideration by the College and future design team.
Comment 22	The college should work with SDOT and the community to implement the following traffic calming measures:		The College will consult SDOT on appropriate sizing of curb cuts at acquired parcels at the time of development action.
	a. Provide pedestrian crossing and node improvements as approved by SDOT at East Howell Street and Harvard Avenue by:	Comment 25	The committee supports the maintenance of ADA-accessible street parking spaces along Harvard Avenue south of Howell Street.
	i. Raising the intersection to pedestrian crossing level, with SDOT's approval, to slow down traffic prior to entering the raised intersection.	Response 25	The College has no plans to remove the designated ADA-accessible street parking spaces on Harvard Ave south of Howell Street.
	ii. Providing new materials for the raised intersection to indicate the pedestrian-oriented zone, such as stamped pavement, concrete or pavement scoring, colorized concrete, or other materials.	Comment 26	The committee understands that no street vacations, which allow property owners to petition the City Council for private use of the public right-of-way, are proposed by the college, and the committee does not support any additional loss of public right-of-way for college purposes.
	b. Construct curb bump-outs on Harvard Avenue at East Olive Street and East Howell Street to channelize and slow traffic. Provide greenscaped areas within curb bump outs to accentuate a slowed pedestrian environment.	Response 26	The College is not proposing any street vacations as part of this MIMP. No additional comment.
	c. Evaluate the application of other traffic calming measures to slow or discourage through traffic along Nagle, Harvard and Boylston and make it as pedestrian friendly as possible This could include but not be limited to: rapid rectangular flashing beacons at existing pedestrian crossings, roadway width narrowing with or without landscape strip enhancements, pavement treatments, etc.	Comment 27	The committee recognizes the significant potential of redesigning the East Howell Street right-of-way into an active pedestrian-oriented corridor that connects the college campus to both Cal Anderson Park and the Capitol Hill Light Rail station. The college shall modify this space in the following ways:
	d. Use design cues at the sidewalk along Nagle to alert cars that they are approaching an unmarked mid-street crossing for people walking, biking, or rolling between Cal Anderson Park and the retained stairwell between the Hunter's Capital building the college's building.		a. Transform the pedestrian corridor between Broadway and Harvard into a lively active zone with areas of hardscape and greenscape, while allowing for increased accessibility.
	e. The college should study ways to pedestrianize Harvard between Pike and Howell Street.		b. Provide zones intended for public and student recreational use, while ensuring campus security and personal safety.
Response 22	In general, the College is in favor of streetscape alterations that improve pedestrian safety and create a more welcoming campus for everyone. Alteration of street layouts, curbs, and pedestrian crossings will be taken on as funds are available and/or when adjacent construction requires street alterations (e.g., construction of the Harvard I/II project(s) may trigger alternations to the E Howell St and Harvard Ave intersection). All work in the public right-of-way (ROW) will be done in consultation with SDOT. SCC will encourage design teams to consult the latest standards for safe street design from NACTO (National Association of City Transportation Officials; Urban Street Design Guide, Don't Give Up At the Intersection) and other organizations and published guidelines.		c. Utilize changes in slope to locate green stormwater infrastructure within landscaped areas going downslope to the west.
	Please see our response to Comment 21 above for more information.		d. Implement pedestrian connection improvements from Cal Anderson along Howell to Broadway, including but not limited to wayfinding and pedestrian amenities.
Comment 23	The college shall improve the streetscapes along all parcels that it acquires and bring them up to the same standards as the existing campus and as specified by the CAC. This includes streets, sidewalks, and pedestrian paths, and relates to circulation, wayfinding, lighting, pedestrian amenities, limiting curb cuts, and installing campus identity materials as specified in recommendation (#29).	Response 27	SCC looks forward to working with a future design team to transform the Howell St Passage (referred to as East Howell Street right-of-way in above Comment) into a vibrant pedestrian corridor. Due to the scale of this project, it would only be undertaken with the proposed ITEC building, a major capital project. The design and features of the space will be reviewed in consideration with college needs and community desires. Please see the proposed Design Guideline(s) section(s) on Open Space, Green Space, Street Level Activation and Uses, Pedestrian Circulation, Lighting, and Campus Identity for more information. Regarding facility access needs and opportunities at the Howell St Passage, please see Response 20. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
Response 23	SCC appreciates and agrees with the CAC's desire to provide high quality streetscapes in the Capitol Hill neighborhood. The College has limited funds to use for site/campus improvements, especially in areas not associated with a major capital project. The College will make improvements to sidewalks, streetscapes, and related infrastructure as funding allows.		In response to item d above, the College has authority to make changes on parcels it owns. Any wayfinding, pedestrian amenities or other improvements would be limited to the extent of E Howell St within SCC property. Additionally, E Howell St between Broadway and Nagle Pl has recently been redesigned to restrict traffic flow and provide more space for pedestrians.
	Any empty or underutilized parcels acquired by the College will be evaluated for how it can be of most use in the near-term (when a temporary use may be appropriate) and long-term (new construction/ renovation of existing buildings as shown in development diagrams).	Comment 28	The college shall partner with local transit agencies to improve access to transit in the following ways:
	Please see the proposed Design Guideline(s) section(s) on Open Space, Lighting, Campus Identity, and Pedestrian Circulation for more information. These guidelines are contained in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.		a. The college should partner with Sound Transit to introduce wayfinding within the Capitol Hill Light Rail station that guides visitors to the college via the southwest exit, avoiding two road crossings and offering protection from the weather.
Comment 24	The committee recommends that street tree canopies along newly acquired parcels be preserved with SDOT consultation and that existing curb cuts adjacent to new acquisitions be reduced to meet current SDOT driveway width requirements.	Response 28	The College agrees that improved wayfinding, station quality/maintenance, and stewardship can lead to increased transit ridership.
Response 24	Existing trees on parcels acquired by the College will be maintained in accordance with SDOT standards until the site development occurs. Upon development action, existing trees will be surveyed by an		a. The College agrees that providing wayfinding signage in the Light Rail station would be beneficial to visitors. SCC will engage Sound Transit to request signage be added to the Capitol Hill Station that directs Link Light-rail riders visiting Seattle Central College to the southwest entry (officially the West Entry) nearest to campus.

- b. The College will engage with King County Metro and discuss how bus stop infrastructure can be improved.
- Comment 29 The college shall strongly encourage and incentivize bicycling by providing the necessary amenities to support a thriving cycling culture among students, college employees and campus visitors. This shall be achieved by taking the following actions:
- Conduct an inventory of existing bike parking facilities within and around the MIMP boundary, including Sound Transit storage facilities, and make projections for future bike parking demand to inform the planning of new bike storage within the MIMP.
 - Use the data collected in the bike parking analysis to provide an appropriate amount of bike parking and lock-up facilities that are meaningfully distributed around campus in heavily trafficked access points and other places according to demand. The following locations on campus have been identified as bike parking priority areas: the new student housing structure and attached garage, the south plaza, the Howell Street pedestrian corridor, the connection between the ITEC and Capitol Hill Link light rail station, the combined Student Union and within parking garages. This is not a comprehensive list, and the college must make sure to not neglect other areas of the MIMP. Additionally, short-term covered bike parking should be located near every major entrance.
 - The bicycle parking should take the form of either lockers or restricted-access bicycle garages; unattended bike racks should be limited to meet day use demand. The parking should have clear signage and be well lit, be well advertised online, and be made available to the Capitol Hill neighborhood to the greatest extent possible. The college should provide security for bike parking on campus.
 - If current Sound Transit bike parking facilities are deemed inadequate in meeting demand, the college should partner with Sound Transit to provide an expanded bike lock-up facility in between the light rail station and the ITEC, or within the immediate vicinity, in support of encouraging multimodal transit usage. This should be done as an extension of the ITEC project.
 - The college shall explore methods for incentivizing students and employees to bicycle to campus by providing amenities that directly support bicyclists including but not limited to a bike repair workshop, a bike-oriented retail outlet, changing rooms with showers and charging stations for electric bikes.
 - The college should support city efforts to establish a safer and more connected bike network throughout the city and between campuses within the Seattle College system to facilitate greater access to the SCC campus.
 - The college shall encourage and incentivize the utilization of bikeshare modes to and from campus and shall not attempt or support efforts to prohibit bikeshare parking on campus.
 - The college shall mitigate the hazards posed by improperly parked bikeshare modes by engaging the community in design charettes to designate dock-less bikeshare and scooter parking zones.
 - The college should work with SDOT to make Streetcar tracks in the road more visible in order to increase safety for bicyclists along Broadway.
- Response 29 The College agrees that providing improved bicycle amenities and support infrastructure can increase ridership.
- The College will conduct a survey of existing bicycle parking on campus (within the MIO boundary/ on parcels owned by the College) and near Sound Transit Station Entrances.
 - The College will use data from the above survey, Parking and Transportation data collected by TranspoGroup on percentage of employees and students who bike to campus, and survey students directly to get feedback on where and what types of additional bicycle parking facilities are desired.
 - Currently, the College has 12 bike lockers in the Harvard Garage that are available on a first-come first-served basis for faculty and staff. Additional bicycle parking facilities will be provided as determined by demand, and as funding allows. If installed, restricted-access garages and/or secure cages will likely be accessed via badge/keycard and will therefore be prioritized for student, faculty and staff use. If installed, individual lockers will be prioritized for student, faculty and staff use, and the public may use them if the College deems it appropriate.
 - The College has no effective means or jurisdiction to determine if Sound Transit bike facilities are meeting current demands. A joint venture on an expanded lock-up facility between the West Entry and future ITEC project will be taken into consideration when the ITEC project is funded for design.
 - The College agrees providing amenities to support bicyclists could increase ridership. Currently, the Mitchell Activity Center (MAC) and Broadway Edison Complex offers showers and changing rooms

- for students, faculty, and staff. If demand for auxiliary shower and changing rooms is beyond the capacity of the existing facilities, the College may provide them where it sees fit after consultation with college users. The College will consider also partner with non-profit organizations (Bike Works Seattle, Cascade Bicycle Club, etc.) to lead classes and educate college users.
- The College is committed to working with the City of Seattle to support the development of protected bike lanes and a safe biking network.
 - The College is interested in providing incentives for students and employees using bike/scooter share. Incentives could include discounted rates, pre-loaded cards (similar to discounted ORCA cards offered by the college described in Response 13). It should be noted that previous attempts by the College to engage with the bicycling community to incentivize use, have not been fruitful.
 - The College will work with SDOT to create designated bike- and scooter-share parking zones within the boundaries of Campus as the College sees fit.
 - On the section of Broadway in front of SCC, the 2-way bicycle track runs on the eastside of the street and does not intersect with the Streetcar tracks. In a review of all College street frontages, there are no instances of bicycle tracks crossing Streetcar tracks.

Please see the proposed Design Guideline(s) section(s) on [Open Space, Green Space, Street Level Activation and Uses, Pedestrian Circulation, Lighting, and Campus Identity](#) for more information. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.

Internal Circulation and Open Space

- Comment 30 The college shall delineate the campus apart from the surrounding neighborhood so that students feel ownership of the space while sharing it with guests, building a sense of college community and identity. This shall be achieved through separating building materials, repeating aesthetic treatments in design of public spaces, implementing space demarcations such as the short stone wall and visual campus identity cues such as banners, landscaping, arches, gates, internal courtyards, etc.
- Response 30 SCC agrees that the extents of the campus should be distinguishable from the surrounding neighborhood. Design strategies have been proposed to address this accomplish this goal. Please see the proposed Design Guideline section(s) on [Open Space, Green Space, Campus Identity](#) for information on how the College proposes to create a unique and defined campus environment. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
- Comment 31 The college shall improve signage and wayfinding around campus.
- Response 31 As funding allows and comes available for different projects, the College will improve signage and wayfinding as a means of creating a more defined campus environment. Please see the proposed Design Guideline(s) section(s) on [Campus Identity](#) for more information. This section is included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
- Comment 32 The college shall ensure a comfortable pedestrian experience by providing the following pedestrian amenities: seating and rest areas, tables, recycling, compost, and trash receptacles, art installations, and other such amenities. The implementation of such amenities should take into consideration both use by college students, faculty, and staff, but also incentivize public/ community use.
- Response 32 The College is committed to providing quality public spaces and amenities. For all planned and proposed projects, the College will provide pedestrian amenities. The College will consult students and the community on pedestrian amenities to be added. As funding allows, the College will provide additional amenities in other areas of campus. Please see the proposed Design Guideline(s) section(s) on [Open Space, Green Space, Pedestrian Circulation, Street Level Activation and Uses, Lighting](#) for more information. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
- Comment 33 The college should coordinate with an outside provider to locate or place a publicly accessible bathroom in a non-secured location (no key card access) on or near the college's campus. The bathroom will be operated, maintained, and secured by a third-party entity so as not to encumber the college with these costs or responsibility. As the most prominent public institution in the Capitol Hill neighborhood, Seattle Central College is frequently expected to support significant community needs in addition to educating its students. We call on the City of Seattle to address this dual role of the college by actively partnering with the college with technical and financial support to provide public access to bathroom

Response 33	<p>facilities and access to resources for unhoused and/or mentally ill individuals. Further, effective management and appropriate funding of Cal Anderson Park by the City is necessary for the spaces in and around Seattle Central College to thrive.</p> <p>SCC has engaged with the City of Seattle, Sound Transit, and other entities seeking to provide a solution for public restrooms on/near the campus. To date, engagement has not been fruitful. The College remains committed to supporting a solution.</p>	Response 37	<p>The College appreciates the committee's feedback and support on skybridges/skywalks – a unique infrastructural element that can improve the life of students and faculty, especially regarding accessibility.</p> <ol style="list-style-type: none"> Broadway Achievement Center (BAC) BE Complex – In lieu of a skybridge, and based on discussions with the CAC, the College understands that the CAC supports a new building entrance and connection at the ground and upper floors. This will remove the north stairwell connection to Harvard Ave. Closing off this stairwell has been an ongoing safety concern. The College agrees with the CAC's recommendation. Across the Howell Street Extension – Based on discussions with the CAC, the College understands this skybridge to be a connection from the BE Complex to the planned ITEC building and/or SAM building. The College agrees to set any skybridge back from the street frontage. The College requests flexibility omitting the minimum 3rd floor location and is committed to working with the Standing Advisory Committee (SAC) to find an acceptable solution that creates activity, transparency and effective circulation for any crossing of the Howell St Passage. Science and Math Building (SAM) to ITEC – Based on discussions with the CAC, the College understands the CAC supports a connection that would connect all levels of SAM and ITEC.
Comment 34	<p>The college shall implement safety treatments that offer a sense of safety to pedestrians from the threat of cars. Various forms of barriers should be installed to separate sidewalks from the streets which can include bollards at curb cuts, trees, curb planters, street cafes and street parking. Vehicular services such as repair vehicles and waste removal should be kept separate from pedestrian activity to the greatest extent possible.</p>	Comment 38	<p>Balance accessibility improvements with thoughtful impediments to reduce high-speed travel through public spaces via bicycles, scooters, skateboards, roller-skates, etc. Provide reasonable accommodations, such as electric charging stations and lock-up facilities, for these alternative modes.</p>
Response 34	<p>SCC will work with SDOT and other City of Seattle jurisdictions to improve pedestrian safety in and around the College campus. Improvements with Planned projects include crossings at Harvard Ave and E Pine St (between the proposed Student Housing project and main Campus) and at E Olive St, at the mid-block crossing of Broadway (between the proposed Student Union and the main entrance to Broadway-Edison), crossing Nagle (between the Student Union and Cal Anderson Park). Improvements with Potential projects include the crossing at Harvard Ave and E Howell St/Howell St Passage. Please see the proposed Design Guidelines section(s) on Pedestrian Circulation and Campus Safety and Security for more information.</p>	Response 38	<p>The College is committed to creating safe and accessible open and green spaces for all users. As open and green spaces are updated, renovated, and rebuilt, pedestrian amenities and new features will be considered. Impediments such as gateways, bollards and landscaping can help define outdoor spaces and make people traveling on wheels slow down as they cross into the space. Fixed seating, benches, artwork, and plantings can help breakdown large open spaces as well. Please see the proposed Design Guideline(s) section(s) on Open Space, Green Space, Street Level Activation and Uses, Campus Safety and Security for more information. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.</p>
Comment 35	<p>Currently, waste and recycling collection is in the Edison Building, on Harvard Ave north of E Olive St. There are no plans to move nor is there currently space to relocate the waste and recycling area.</p>	<p>The College's commitment to electric charging stations for alternative transport modes and lock-up facilities are addressed in Response 19.</p>	
Comment 35	<p>The college should continue to invest and actively steward alleyway improvements behind their properties south of Pine (behind the Egyptian Theater) in conjunction with the Capitol Hill EcoDistrict and other adjacent property owners.</p>	Comment 39	<p>When construction or maintenance requires replacement of pedestrian brick pavers, the college shall replace the existing red brick with another material that is safer and has appropriate slip resistance for the climate which complies with current neighborhood design guidelines, as well as this MIMP for improved aesthetics that minimize the prior overuse of red brick.</p>
Response 35	<p>The College agrees to continue working with jurisdictional agencies and the community to maintain a clean, accessible, and safe alleyway adjacent to its buildings and parcels.</p>	Response 39	<p>The College appreciates the CAC's feedback and concern regarding the safety of public space on campus. As funding is available and conditions require, the College will work to improve the safety of new and existing open spaces and use appropriate paving materials. Please see the proposed Design Guideline section on Open Space for more information. This section is included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.</p>
Comment 36	<p>The college should consider open space improvements that would minimize the overbearing massing of the Broadway Edison Complex and establish a lively pedestrian connection between Harvard Avenue and Broadway as an extension of that project.</p>	Comment 40	<p>The committee recognizes the existing landscaping on the college campus is minimal and underutilized. Increasing the tree canopy provides environmental benefits and should be encouraged, however the committee recognizes it may prove challenging due to necessary campus circulation, transportation infrastructure, and utilities. Landscaping, ranging from trees, shrubs, plants, and grasses, should be incorporated into any new development or exterior renovation.</p>
Response 36	<p>SCC is open to any projects that improve the quality of outdoors spaces on campus. Improvements to/near the Broadway Edison Complex discussed with the CAC include murals/artwork on portions of the façade, adding lighting to activate/animate the façade and sidewalk, and adding seating and covered areas to the plazas to name a few items.</p>	Response 40	<p>The College agrees with the above recommendation for effective landscaping. SCC will engage with students, faculty, staff, and the SAC on the planning of new and renovated open and green spaces. As mentioned in Response 38, the College acknowledges that landscaping and planting can improve</p>
Comment 37	<p>Historically, capital funds for improvements to campus grounds are limited, unless it is associated with a major project. Regarding the open space around the Broadway Edison Complex, renovation of the South Plaza and adjacent areas may be within the scope of the Broadway Achievement Center and/or District Energy Plant projects. The Howell Street Passage on the north side of the Complex will be within the scope of the ITEC project, however funding will determine if the project boundary can extend to include the entirety of the Passage or only the portion in the immediate vicinity of the building.</p>	Comment 37	<p>The committee recommends the college take great caution when considering skybridges due to the social and economic detriments dealt to street life, and balance skybridges with the needs of students.</p> <p>A skybridge could be acceptable if it is light, transparent, engages with the Howell St Passage, provide views, and, in the instance of a skybridge across the Howell Street Extension Passage, is recessed from the street and is located a minimum of three floors above Broadway. If proper conditions are met, the CAC supports skywalks in the following locations with the conditions noted:</p> <ol style="list-style-type: none"> Broadway Performance Hall (BPH) and Library – Supported by the committee. Across the Howell Street Extension Passage – Supported by the committee under the condition that it is recessed from the street and located three floors or high above Broadway. Sciences and Math and Building and ITEC – Supported by the committee.

outdoor environments and create safer spaces that inhibit high-speed travel. Please see the proposed Design Guideline sections on [Open Space and Green Space](#) for more information. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.

- Comment 41 The current landscape character of the Seattle Central campus is primarily lawn, with a considerable tree canopy along Pine Street. As many of the projects in the master plan will take years to complete or even begin, the Committee recommends the college plant the underutilized existing lawn areas with habitat friendly plantings that reinforce the campus identity and function. Areas of particular focus are the sloped area along Pine St and Harvard Ave and the perimeter landscape to the Broadway Edison building. The CAC acknowledges the limitations the college faces in funding these improvements and encourages the college to pursue funding and stewardship opportunities in partnership with the community to fill this gap.
- Response 41 The College agrees with the above recommendations and will seek funding to make these improvements.
- Comment 42 The design goal for the proposed parking/housing structure should be for it to blend in to the other residential/commercial buildings in the neighborhood. In order to achieve this, the proposed structure should:
- Maintain an active pedestrian experience at the street level, including, but not limited to, commercial space and windows and features that support an active street frontage.
 - Minimize the appearance of exterior blank facades.
 - Use high quality building materials consistent with new buildings in the neighborhood.
 - Consider use of decorative grills or metal barriers between upper floors of garage and the residential floors.
- Response 42 The goal of any new construction or renovation project taken on by SCC is to provide a building that reflects the institutional character and function of the College, uses high-quality and long-lasting materials, and fits into the greater context of the neighborhood in its design and functionality as applicable.
- The College understands the CAC's desire to provide more commercial/retail space at street level, especially along the busy Pike-Pine Corridor. Currently, there are two café/restaurant tenants at street-level in the existing parking garage. The College will explore opportunities to provide more commercial/leasable space for street-level tenants that promote active use in the planned Student Housing project. Please see the proposed Design Guideline section on [Student Housing under Project Specific Guidelines](#) for more information. This section is included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
 - The College will comply with city and neighborhood design guidelines regarding the minimization of blank facades.
 - The College and the State encourage use of robust materials that have a minimum 50-year life span. This initial investment means decreases the likelihood of expensive envelope repairs in the beginning of the building's life.
 - The College agrees to provide screening of any open areas of the parking garage.
- Comment 43 The sidewalk fronting the BEC along Broadway has the potential to be an incubator space for vendors, student stalls and other community uses. The college shall engage the community and SAC to develop this space.
- Response 43 The College will seek to activate the spaces of the Broadway Edison Complex for active student and community-oriented services. Creation of any incubator space for "for profit" is not permitted on state-owned land unless the service "for profit" is to serve the College. Creating space (with capital or operational funds) for community uses is not available from the College typical funding sources. Any effort aligned with this comment would be "Aspirational".
- Comment 44 The college shall install pedestrian-level lighting and lighted pathway guides that promote wayfinding and security at night while simultaneously instilling a sense of campus identity and welcomeness. These can be implemented alongside other nighttime amenities to increase student comfort while taking evening classes.

- Response 44 The College agrees that installing additional pedestrian-level lighting can improve safety on campus and the quality of open and green spaces. Please see the proposed Design Guideline section on [Lighting](#) for more information. This section is included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.
- Comment 45 The college shall implement public realm improvements that greatly increase the quality of the public realm. Creating smaller, high quality public spaces is preferred over the retention of poorly performing but larger public open spaces.
- Response 45 The College will seek funding to make improvements to outdoor spaces whenever possible. Generally, small improvements may be possible between large projects. The best opportunity to improve or renovate outdoor spaces is in conjunction with a large project directly adjacent.
- Comment 46 The built environment of the campus does not reflect inclusive values and poses significant difficulties and elevation obstacles to the mobility impaired. The college shall make the following upgrades to ensure universal access to campus:
- Ascertain mobility obstacles by engaging with the community in design charrettes.
 - Rebuild the wheelchair ramp on the Howell Street right-of-way into something less austere and more aesthetically pleasing to reflect the college's commitment to universal access.
 - Design any new parking structures with mobility impairments in mind. Specifically, a new garage should have designated disability parking, clearly marked elevators on every floor, and be a comfortable experience for those using wheelchairs.
 - Consider building an exterior elevator or major ramp to negotiate the elevation difference between Broadway and Harvard, close to Pine Street and the new student housing structure.
 - Redesign the South Plaza with universal access in mind.
 - Make design improvements throughout the entirety of the MIMP boundary to expand comfort for those with mobility, sight and/or hearing impairments, such as braille textures, verbalized readers, automatic door openers and more.
 - Balance these accessibility improvements with thoughtful impediments to reduce high speed travel of these spaces via bicycles, scooters, skateboards, roller-skates, etc.
- Response 46 The College is required by law to provide a fully accessible campus. A Civil Rights audit is regularly conducted on full campus environs and delivers required improvements. All major capital projects are reviewed by a Washington State Accessibility Committee. In addition, SCC is committed to increasing accessibility and has adopted Universal Accessibility design guidelines to make the campus environs easily navigable by all users, no matter how they walk, roll, or move.
- As major projects occur, the College will engage with the SAC via a design charrette.
 - The College agrees and will seek to address this issue.
 - SCC agrees.
 - SCC agrees with this aspirational goal.
 - SCC agrees with this aspirational goal.
 - SCC agrees.
 - SCC agrees. See Response 38 regarding impediments to high-speed travel. Please see the proposed Design Guideline section on [Universal Design](#) for more information.

Neighborhood Integration + Design Guidelines

- Comment 47 Seattle Central College is partially located within the Pike Pine Conservation Overlay, which aims to preserve the auto-row character and history of the buildings through façade preservation incentives, adaptive reuse, and complimentary architectural details in new construction.
- Any further modifications to the buildings within the Overlay shall be subject to the requirements of the controls and incentives associated with the Overlay program. When additions or renovations are undertaken, look to the renovated buildings to be found along Pike-Pine as an example.
- Similarly, any new construction shall:
- Comply with the design standards for new construction within the Overlay.
 - Reflect the fine, granular nature of the acclaimed auto-row building fabric along Pike-Pine and the similarly detailed, pre-war buildings along Broadway; and
 - Honor the existing urban fabric, scale, and character along Harvard Avenue when integrating new

Response 47 structures and engender stewardship of the existing catalogue of historic buildings. SCC is committed to maintaining the buildings and historic assets on campus. All projects undertaken by the College, whether renovations, additions, or new construction, which fall within the Pike Pine Conservation Overlay District will follow the Pike/Pine Neighborhood Design Guidelines as required.

Comment 48 Proposed new buildings, additions, or building modifications located within the Capitol Hill Urban Village should seek to further design standards set forth in the 2019 Capitol Hill Neighborhood Design Guidelines which guide future development within the Capitol Hill Urban Center Village to maintain and further develop a healthy, diverse, and vibrant Capitol Hill Urban Village. When Capitol Hill Neighborhood Design Guidelines were developed, the community advisory board did not craft design guidelines specially for the college since that is under the purview of the MIMP and the design guidelines developed through that process. There was an expectation among the group that design of new projects outside the Capitol Hill Urban Center Village would seek to complement each other and that the college would look to these design guidelines when developing their own.

Response 48 The College understands the role it plays in the Capitol Hill neighborhood and that the success of the College is related to the success of the neighborhood and vis-versa. The College reviewed the 2019 Capitol Hill Neighborhood Design Guidelines and has included relevant items in the Design Guidelines and Development Standards (MIMP Chapter 4).

Comment 49 Special attention must be paid in redeveloping the Pine Street parking garage. There is concern that the existing structure will be left essentially intact, with housing simply placed upon it or that the new construction will replicate the long, blank facade along Pine Street. The student housing building should have ground-level activity along Pine that activates the streetscape and improves the pedestrian experience along this street.

Response 49 As stated in the project description of Student Housing in Chapter 3.2 – Campus Growth and Expansion Planned, preliminary plans show that the existing garage will not be left as-is with housing added on top. In the new development, the parking garage will be rebuilt and have capacity to hold about 50% fewer cars. Regarding streetscape development, SCC is committed to creating a vibrant street atmosphere with a goal of reconnecting the current “gap” that exists at the College along the E Pine St corridor. Please see the proposed Design Guideline section on Student Housing, and Street Level Activation and Uses for more information. These sections are included in Chapter 4, Section 4 of the Preliminary DRAFT MIMP document.

Height

Comment 50 The codes create a balance of allowable height to building footprint and it is the understanding of the committee that the College will follow the MIMP and the agreed to balance between the CAC and the University and not defer to the underlying zoning to gain additional square footage.

The understood goal of the proposed building massing and height is to provide opportunities for the necessary densification and growth of the College without adversely affecting the relationship to the neighboring buildings and community. The table below outlines what the underlying and MIO zoning allow, but the “College Proposed Height” is what the CAC recommends the College limit themselves to. Any future project that has a proposed height beyond the height of the project listed below (College proposed height) would be subject to a master plan minor amendment.

Project – stories	College Proposed Height	Allowable Height by Underlying Zone	Max MIO Height
Student Housing – 6	85’	75’/85’	105’
ITEC – 6	95’	55’/75’	105’
Student Union – 3	55’	75’	85’
Harvard 1/2 – 5	75’	85’	85’

Response 50 Per the guidelines of the city and this MIMP, the College cannot use both the City of Seattle Land Use Code and agreements in this MIMP to construct a building larger than what is allowed by either – the College must abide by one or the other. If the College elects to abide by the requirements of the Land Use Code, any previous agreements reached as part of this MIMP do not apply and the College must go

through a separate process for building approval. For design and construction of a SCC building, it is generally in the best interest of the College to abide by the rules and agreements in this MIMP.

The College is committed to working with and obtaining approval from the SAC for any deviations in overall height In lieu of a minor amendment.

The current MIMP establishes a maximum height of 75’ for buildings south of E Pine St. and 105’ for buildings north of E Pine St. For proposed building heights, please see Chapter 3 – Campus Growth and Expansion, subsections on individual projects. For proposed maximum building heights within the MIO boundary, please see Chapter 4 – Design Guidelines and Development Standards, subsection on Zoning. Proposed heights below:

Project – stories	College Proposed Height	Allowable Height by Underlying Zone	Max MIO Height
Student Housing – 6	90’	75’/85’	105’
ITEC – 6	95’	55’/75’	105’
Student Union – 3	60’	75’	85’
Harvard 1/2 – 5	80’	85’	85’

Please note, the above Proposed Heights have been adjusted to account for the Average grade plan definition included in City Zoning code. They differ slightly from what was presented to the CAC where we discussed height form main building entrances/street frontages.

Design Guidelines

Comment 51 The CAC does not support the guideline regarding “curved forms and harsh angles” as it is unclear how this can be executed successfully or interpreted during SAC review. The CAC recommends this design guideline be removed or reworded.

Response 51 This item has been removed from the Design Guidelines.

Comment 52 Review and revise precedent images for relevance and clarity. The images become the specific reference for the language and should reflect the intent of guidelines. Some images are lacking or missing (e.g., images for the side of the Student Center facing Cal Anderson Park, lighting installation of the tree felt cold, and green stormwater infrastructure images was depressing.)

Response 52 Precedent images will be revised and updated based on feedback received from the CAC in previous meetings and above requests.

Open Space

Comment 53 Revise the first bullet point to read “Student usability of open space shall be prioritized over public usability.

Response 53 The line-item currently uses the word “will” in the place of “shall” as shown above. To maintain clear and consistent language and terms throughout the design guidelines, the College will keep the word “will” as currently shown.

Comment 54 The concept of “redeveloping underutilized open space” should be more specific about the end goal. The CAC strongly encourages the college to seek to transform underutilized open space into high-quality public realm and this concept of transformation and quality should be reflected in the design guidelines.

Response 54 In general, the College will always work to provide high-quality, attractive, accessible public space to serve the needs of the College first and the needs of the public second. SCC doesn’t want to commit to a vision or use for a particular outdoor space prematurely or the needs of the College change, and then the College isn’t able to change course. The statement will be revised to reflect more intention.

Green Space

- Comment 55 The CAC is excited about the idea of an improved and enhanced public realm, focusing on a vision for increasing the number of plantings and greenery around campus by developing a consistent planting language that would strike a balance between being drought tolerant, climate adaptive, and providing of habitat value. If special maintenance were to be required, the college should provide training and education for maintenance staff including an established manual and guidelines.
- Response 55 The College agrees that providing a unified and quality planting palette will enhance the public spaces around campus and create a better sense of college identity. Staff will receive information and training on care requirements for any new plantings. The College supports using plantings that are drought tolerant, climate adaptive, and provide habitat value. To support the best possible outcome, low-maintenance plantings should be prioritized due to the College's limited staffing of grounds and landscape staff.
- Comment 56 The concept of "redeveloping underutilized open space" should be more specific about the end goal. The CAC strongly encourages the college to seek to transform underutilized open space into high-quality public realm and this concept of transformation and quality should be reflected in the design guidelines.
- Response 56 See Response to comment 54 above.

Street Level Activation and Uses

- Comment 57 Murals are not the only means for activating blank facades. The design guideline for facade activation should encourage public art "such as murals," which leaves open the possibility for other creative treatments.
- Response 57 The items listed under the design guidelines are intended to be suggestions and not limitations on what is or is not acceptable or appropriate (unless specifically identified as such). The item in question has been amended to be more open as suggested.

Sustainability

- Comment 58 The precedent image of the full stormwater infrastructure in action is dull and unattractive. Replace this image with more attractive stormwater infrastructure. One CAC member suggested looking to the Swale on Yale for suitable imagery.
- Response 58 The image in question has been replaced.
- Comment 59 The college needs to provide further assessment as to how and if a steam plant is appropriate to include within the proposed MIMP.
- Response 59 The College is proposing to construct as a potential project, a new District Energy Plant. Please see [Chapter 3 – Campus Growth and Expansion Potential](#), subsection on Potential Project Development for more information. This section is included in Chapter 3 of the Preliminary DRAFT MIMP document.

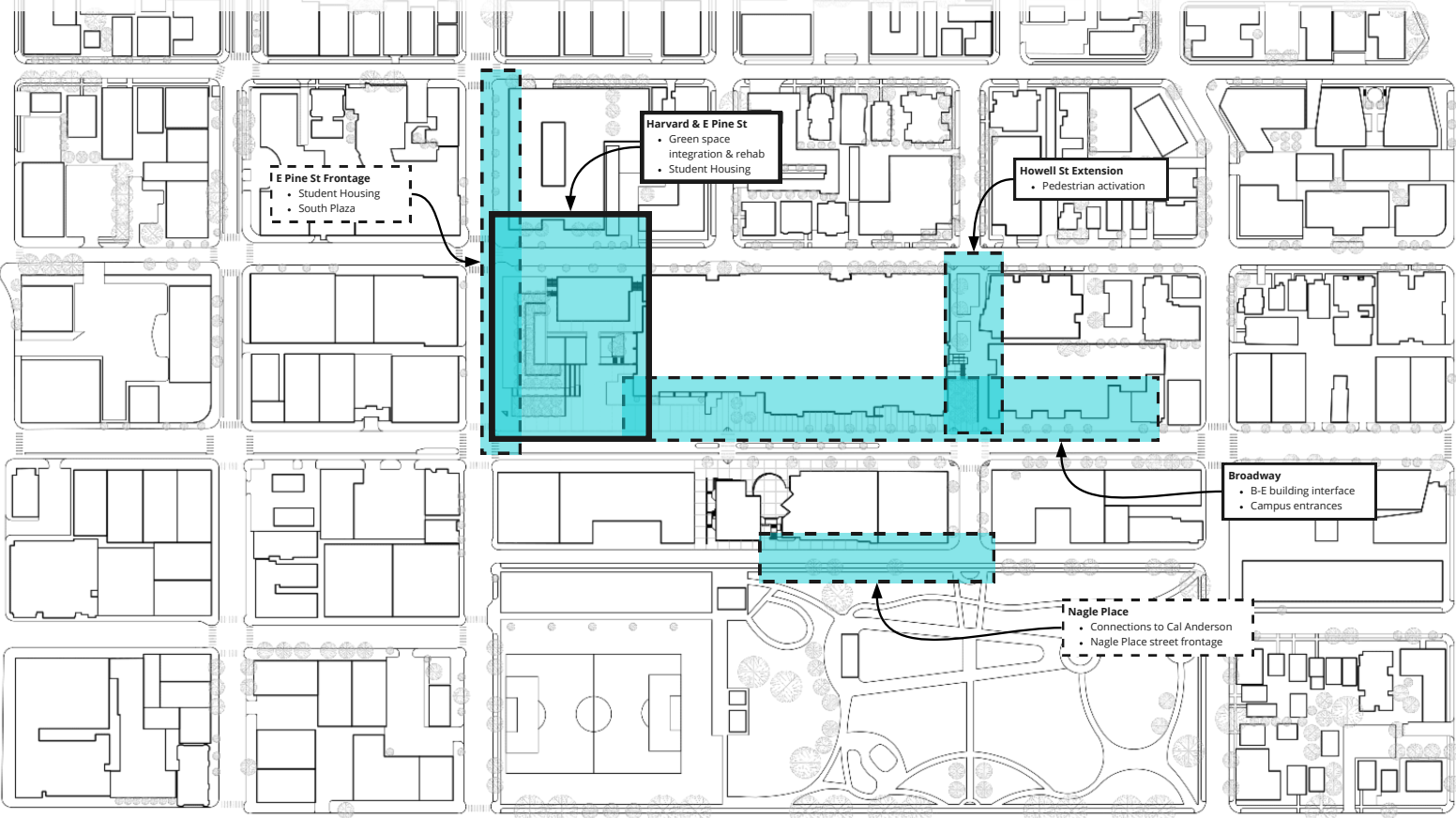
APPENDIX C CAC DESIGN CHARETTE SUMMARIES

The following pages include summaries of design charette held with the Citizens Advisory Council. They include.

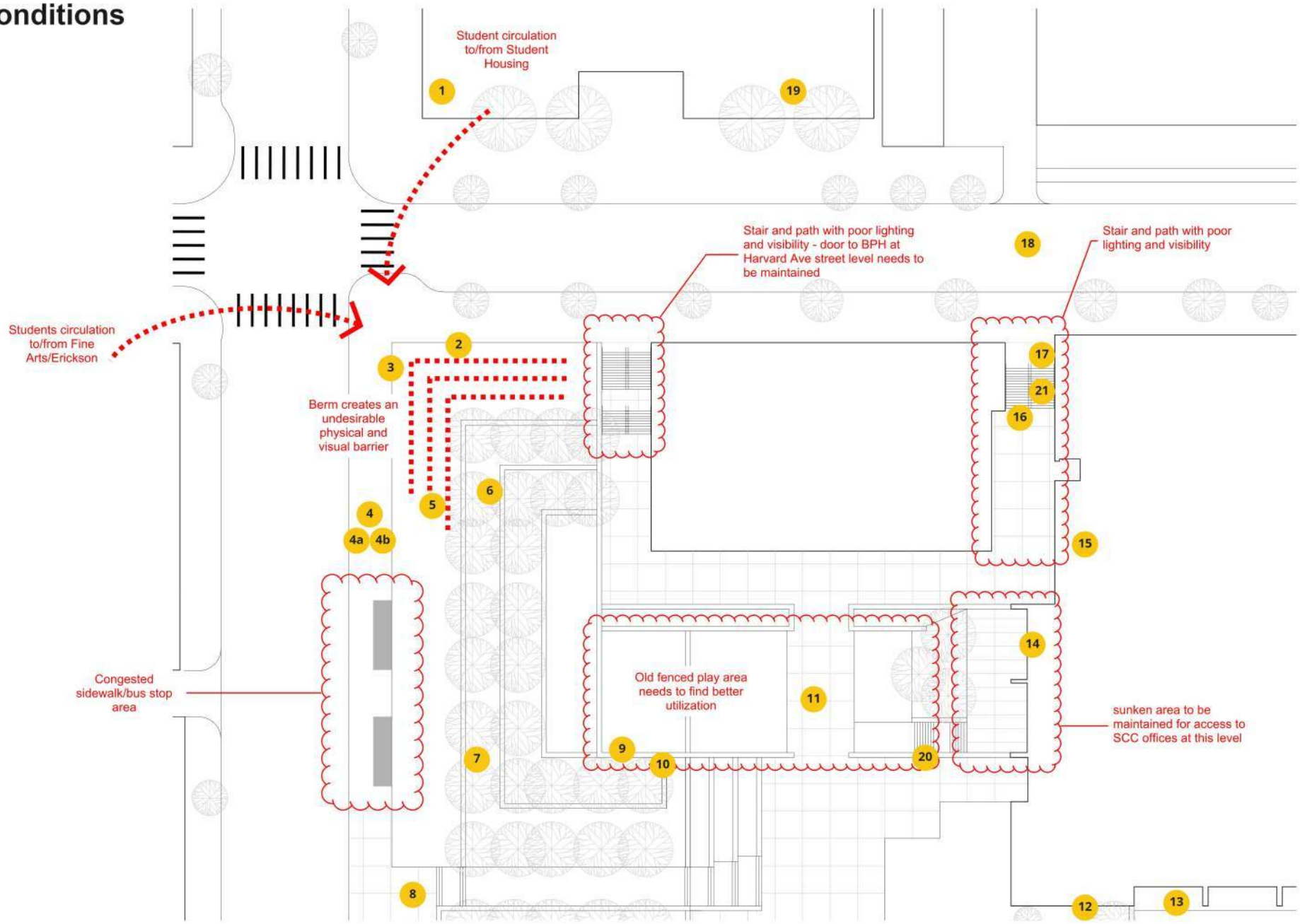
- | | |
|------------|-----------------------|
| Charette 1 | South Plaza Charette |
| Charette 2 | Pine Street Frontage |
| Charette 3 | Broadway Streetscape |
| Charette 4 | Howell Street Passage |
| Charette 5 | Nagle Place |

SEATTLE CENTRAL COLLEGE MAJOR INSTITUTION MASTER PLAN

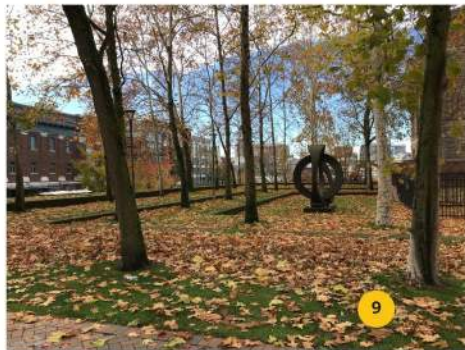
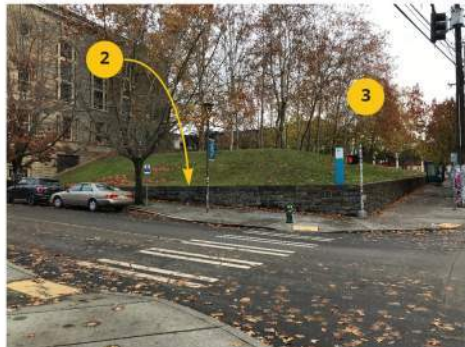
C.A.C. MEETING 10 - SOUTH PLAZA & HARVARD / E PINE ST.



Existing Conditions



Existing Conditions



Comments - Existing Conditions

1. consider improving existing and [tiering] in the frontage from the parking garage into southwest corner of the performance hall plaza "glen"
2. Locate at SW grade businesses within Pine frontage, underneath [re-designed] upper plaza.
3. This southwest facing wall creates the feeling of a fortress/closed off environment - should be gently terraced to maximize views of city, invitation and activation. The sunken nature of the 'glen' creates a perceived safety issue - trees aid this as they are sunken
4. I'd like to see the [berm] flattened
 - a. Yes!
 - b. Berm flattened but don't lose that wall
5. Use the grade change to create some micro-retail sites underneath south plaza.
6. Park furniture and small reading/relaxing nooks would be great, provided they remain visible to the street and plaza
7. Okay to lose the ultra linear form of existing tree plantings.
8. Opportunity for corner treatments of redesigned plaza to have a relation to each other, both at Broadway and at Harvard. Designer could consider symmetry, similar use of materials, colors, treatments, and be [interesting] to traverse from one corner to [another].
9. Could this former play area be used for farming tied to culinary programs?
10. In our community tour, we talked about gates as a way to make students feel more like the campus is theirs, similar to Seattle U
11. strange pinch point
12. not sure where this fits - a former SCC foundation board member proposed to have each window well lit up with a different color using floodlights to create more welcoming feel at night.
13. this is a mote currently - a drop off and should be utilized in a more effective way as street spillover, stormwater management.
14. Can the entrances be moved elsewhere? Or is the hollow necessary for sunlight?
15. Community members on my tour were supportive of pedestrian bridges to improve student experience and safety
16. There needs to be activity here like a cafe or some sort of student activity as per Alex Brennan
17. Close this [pedestrian] [connection].
18. Consider a partial street vacation of Harvard between Pine and Olive. This will help increase a ped friendly environment between the student housing and campus, and will direct cars to enter the garage from Boylston and Olive.
19. Consider consolidating all parking underneath the STEM/ITEC Building parking garage and build new commercial / student housing building. Partner with housing developer to add workforce / affordable / market rate housing in building. Use non-student housing as a revenue generator for the College.
20. Remove sunken, gated-off area if no longer expected to serve a specific purpose (e.g. daycare)
21. If this cannot be widened, I support closing this ped connection

Comments - Existing Images

1. Staircase needs to be greatly expanded and made into a "grand" entrance for people parking in the garage and walking into campus.
2. This wall needs to be retained because it is aesthetically desirable and helps to define the space which is important when surrounded by roads that are nearly out of human scale. Stone wall can be cut to allow passageways through.
3. Can the Broadway PH have entrances facing different directions so that it feels the building is facing both Broadway and Harvard?
4. Bus stops need to be improved. The old stops in the plaza and on Howell that are now smoking areas should be removed
5. Great asset. Blank canvas for art. Think of the State Hotel mural in DT Seattle
6. These views should be everywhere and supported as campus standard.
7. These lantern banners can be redesigned to integrate with other "new campus identification materials" such as large campus signs
8. This pit is such a dead space. Get rid of it.
 - a. agreed!!
 - b. Agreed!
9. I support redevelopment of the plaza and glen, but strongly encourage creation of greenspace. I would like to see the retention of trees, but don't want this to limit how you create highly-functioning greenspace.
10. History of the art? Who made this? What does it symbolize? Do we need these or can they be replaced?
11. Artwork on campus is important to curate and continue
12. I support the use of more art throughout campus. See Greenway's comments [comments in SCC_CAC_PublicComment_111620.pdf] for recommendations related to art

Precedent Images



1. Different ground materials and textures suggest different uses
2. Street furniture with simple forms provides a variety of uses



1. Continuous pavement with different colors
2. Planting beds minimize street, maximize sidewalk
3. Sidewalk and street at the same level
4. Festival street opportunity



1. Gathering space cut into grade
2. Enclosed activity area adjacent to public spaces



1. Use slight changes in level to create hierarchy and break down large spaces



1. Use parklets as furniture within larger open spaces



1. Incorporate signage to reinforce identity of place
2. Use as a visual and physical orientation element
3. Celebratory elements (graduation pictures, etc.)



1. Use similar paving on the street and sidewalk to indicate slow areas



1. Highlight crossing areas with contrasting paving
2. Use raised crosswalks for added pedestrian safety



1. Similar materials with different colors - delineates different zones



1. Create new seating opportunities built off existing site features (brick pavers)
2. Use different materials to convey different uses (warm materials for sitting)



1. Put seating at the edge of green spaces to encourage active edges



1. Design art/sculptures for multiple uses - beautification and shelter from elements



1. Introduce stairs and/or ramps to navigate elevation changes
2. Use negative space for planting beds, site furniture



1. Tiered plaza/walkway creates different zones
2. Seating looking towards pathway, focal points, site features



1. Locate seating at the edge of green spaces to encourage active edges



1. Integrate landscaping and site furniture
2. Create small spaces for retreat - be aware of the environmental noise in the area



1. Adjustable furniture gives users the option to create their own space
2. Plug-and-play variability



1. Design art/sculptures for multiple uses - beautification and shelter from elements



1. Introduce stairs and/or ramps to navigate elevation changes
2. Integrate stairs and ramp landings to maximize space efficiency
3. Use negative space for planting beds, site furniture



1. Street furniture provides seating and planting beds
a. Street furniture is built into the sidewalk - is not movable
2. Use of heavy materials to imply permanence



1. Pavement types suggest different zones
2. Street furniture provides seating and planting beds
a. Street furniture is not integrals/structural to sidewalk - is movable/changeable
3. Multiple small/medium scale furniture pieces can have different features



1. Embedded sidewalk lighting
2. Light the extents of a path or zone



1. Low-level bollard fixtures to light walkways and delineate paths



1. Large-scale lighting creates a focal point and gathering spot
2. Tying lighting and seating together



1. Uplights and broad surfaces can create implied separation and define zones of use



1. Artistic lighting animates an otherwise unremarkable path

Comments - Precedent Images

General Notes:

- I'd like to see precedent images that suggest gates / entrances to a semi-public space

Image 1:

- I really like the idea of breaking up the hardscape with greenery in this manner

Image 2:

- Image 2 scares me; makes me think of the confusion of cars/people near Pike Place

Image 6:

- if this type of raised crosswalk is [implemented] [on] Harvard, use muted colors

Image 13:

- Elevated spaces bisect open spaces in ways that they should not and aren't equitable

Image 21:

- I think this will a mgmt burden for the college.

Image 25:

- Both the central light and the usable art are tacky and in no way elegant.

Concept 1



Comments - Concept 1

1. I'd like to see a larger notch out of the student [housing] building and have the plaza space relate to this space at the housing building corner.
2. Keep pedestrian crossing at usual location, for safety
3. Cars must be greatly slowed when moving from Pine to Harvard
4. Really like the thought of sloping up into the site, but 0-18' with half the site is a lot. How would that feel in reality?
5. LOVE the widened staircase. Have resting spaces in the tiers and many eyes on it from near BAC
6. Claim the SW corner with a terraced landscape that emphasizes views, gateway to pine and downtown - and material language to marriage to new housing building across street
7. Please go visit the Good Shepherd Center in Wallingford, look at the NW corner, you won't be disappointed. The entrance is a beautiful, uncovered stone pathway that meanders and has seating built into it. Possible inspiration for what to do with the wall.
8. Add obstructions on this pathway to keep pedestrians safe (vs. fast-moving e-bikes, bikes, scooters, skateboards) without losing access for wheelchairs.
9. I really like the idea of creating public space analogous to Seattle U. It's an absolute pleasure traversing through Seattle U's campus. I go out of my way to walk through their campus rather than along Madison. That should be an outcome/goal for your plaza redevelopment as well.
10. I'm really intrigued by the idea of a more gradual slope through campus. An ADA-accessible ramp is key to the plaza's transformation.
11. Incorporate green stormwater features as landscape art incorporated with real art to define green spaces and provide WQ benefit.
 - a. I second this comment
12. Speakers' platform? Tiered gathering steps would be conducive to public discourse
 - a. *thumbs up* A speakers platform could not only serve a purpose during protests but could also be used by teachers if classes were held outside.
13. Add! gateway
14. Financial District and Shard District in London are amazing references and inspiration for small pedestrian moments.
15. It's key that the exterior of campus create a sense of belonging that makes students feel welcome!
16. Micro retail would ideally have spaces that stay open late to keep human traffic and eyes in the area - Such as streetfood.
17. Maintaining greenspace, greened areas at 40-50% min, is super important. Even the grey/beige areas on this concept must/need to include green planted areas.
18. Use whimsy throughout all these rethought spaces to allow people to make connections with art, treatments, signage, features that unite the space. Could be motifs, animals, etc.
 - a. YES!
19. You can retain greenspace, but not if it remains as uninviting and off-putting as it currently does. This space should be a noticeable respite and it's not currently.
20. Meandering path could be more attractive and wheelchair accessible
21. A hierarchy of space that emphasizes gathering - a central area that allows for large scale gathering
 - a. I second this comment.
22. These open spaces have to accommodate protests, farmer's markets, and daily student activities
23. I liked your suggestions in the inspiration images for soft, low lighting. I would like to see that lighting incorporated throughout the plaza and Harvard.
24. Add Street trees off the campus parcel between the curb and ped way (Broadway). Ensure Pine Street, street trees remain and are preserved.
25. The hardscape along Broadway would be the most used for public gathering and "soap box" events. This would be important to think what is the most accessible space along the street.
 - a. I agree with this comment.
26. Can the interface of facade to plaza be affected??
27. Like the idea of an iconic SCC backdrop. The big W at UW Tacoma is an inspiration.
28. Love the informal kiosk / entrepreneurship and incubator possibility for this space
29. Question: how do larger public gatherings like the Farmers Market have access to and effectively utilize this space?
30. Picnic tables or informal outdoor eating space would be great. Can it be covered?
31. Anyway to fill this space? It detracts so much unless you have some creative use for it.
 - a. +1
32. Maintaining this sunken area makes little sense. Is this really a necessary / primary entrance into the BE building?
 - a. I agree. It must be removed!
33. Ameer and students want a covered outdoor space because Seattle is rainy
34. Possible speakers platform or natural convening space?
 - a. I was thinking of a speakers platform near the tiered gathering steps given that people may naturally assemble there, keeping this area more protected for students
35. Add a cafe adjacent to the "student space" that allows for proximate spillover into the open space
36. I'm supportive of pedestrian skywalk to connect library from AC to II for students
37. This connection could be an asset, not vulnerability. It could provide an intimate space for activities or a small restaurants. Needs to be shut down at night though
38. I really like the idea of incorporating active uses on the ground floor to add the deep connectivity that Jaime talked about!
39. This space does not feel welcoming, even during full daylight.
40. There's so much opportunity for improving Harvard. Add low-level lighting per your inspo images.
41. Consider Fulton Center(San Francisco) for making open space from the student housing open space
42. Support closing off the staircase to Harvard
43. Activate the Pine street frontage

Campus Open Space

Open Space
Existing College
open space = 37% of
parcel areas owned

SCC - Propose
Decrease to 25%

Designated
Open Spaces

Designated Open Spaces
South Plaza/Green
Howell Street Plaza
Student Center Plaza

SCC - No proposed
Changes

Hardscape

Hardscape
Existing College
hardscape = 75% of
owned Open Space

SCC - Target a Decrease
to 65% of Open Space

Greenscape

Greenscape
Existing College landscape
= 25% of owned Open
Space

SCC - Target an Increase to
35% of Open Space

Harvard & Pine
Crossing



For ped safety,
minimize the area of
the shaded
"crossing" between
the plaza/tiered
steps and the
housing building

The Glen



Bus Stop



Yes, please improve the
bus stops on campus.
Shelter from the rain is key.
May be an opportunity to
incorporate art or campus
signage/placemaking.

Amenities to
Include



Lighting,
Materials, Etc.



Community Use
Space



Activites to be
Accomdated



Safety



Other

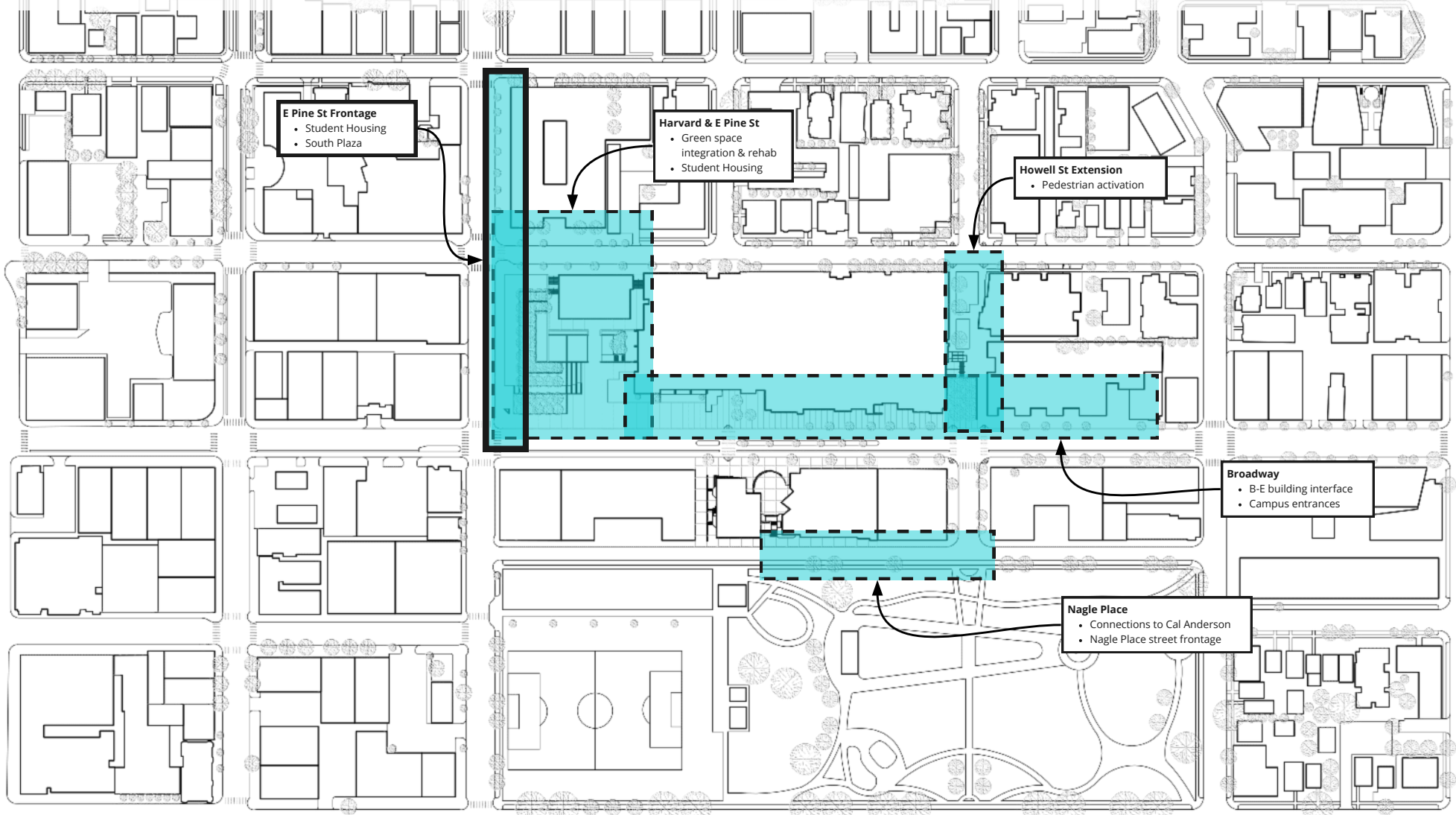


Other



SEATTLE CENTRAL COLLEGE MAJOR INSTITUTION MASTER PLAN

C.A.C. MEETING - PINE STREET FRONTAGE CHARRETTE



E Pine St Frontage

- Student Housing
- South Plaza

Harvard & E Pine St

- Green space integration & rehab
- Student Housing

Howell St Extension

- Pedestrian activation

Broadway

- B-E building interface
- Campus entrances

Nagle Place

- Connections to Cal Anderson
- Nagle Place street frontage



Design Guidelines - Existing Conditions

To the extent reasonable, retain existing physical context and blend with new development.

- When project development, includes existing structures, consider repurpose and/or reuse if the existing structures positively impact the surrounding context.
- Reinterpret common aspects of adjacent structures in new development such as use of similar materials, transparent street fronts, high-ceiling ground floors, expression of structure.

*No
comments*

Design Guidelines - Building Facade

Building façade articulation should blend with the established development pattern of the Pike Pine and Broadway street frontages

- Existing structures along Pike, Pine, Broadway, and Harvard generally match the originally platted lots and are characterized by building that are 50 – 60 feet or when on two lots are 100-120 feet in width. The scale of new structures are to reflect the rhythm of bulk and scale established by this context.
- Respond to topography by stepping facades so that floorplates generally match the street grade.

Avoid
blank wall
surfaces

encourage
small
storefront
businesses

Design for
anti-graffiti by
avoiding
design that
attracts it.

<https://campaign.ucsd.edu/impact/stuart-collection-alexis-smith-mural/>

<https://art.famsf.org/andy-goldsworthy/drawn-stone-20045>

Design Guidelines - Streetscape Connection & Integration

Connection to the Street. Integrate new structures with existing street patterns to maintain a cohesive streetscape

- Structures with street frontage facing Pike, Pine, or Broadway shall orient active street-level uses on these streets. Uses shall be generally those that support student needs, or related. Uses provided highly activated functions that will bring energy and interests to the street. (i.e. student lounges, gathering spaces, food services, meeting spaces, etc.). Office type uses should be avoided. yet bring student and community activities.
 - The uses should be transparent with visibility into and out of, the structures.
- Provide a strong 2 to 4 story street wall along frontages except where structures are pulled back at major building entrances.
- At major building entrances, provide active pedestrian transition areas between the street frontages and building entrances.
- All street fronts shall be provided with activity, visual interest, and variety. Consider appropriate use of:
 - Canopies or cantilevered structure at walkways along frontages with high levels of pedestrian traffic to provide weather protection.
 - Street furniture for College and Community Use. Include at areas to promote activity, and in locations that offer respite to the bustle of busy streets.
 - Plantings and trees that create a defined "palette" to reinforce the college "district".
 - Multilevel lighting (General frontage illumination, Pedestrian oriented lighting, Security lighting select building façade/site development lighting.
 - Signage that support definition of the College "district"
 - Building and Site materials that are of a pedestrian scale. Palette of materials should that support the definition of the College "district"
 - Enhance the pedestrian environment through inclusion art, societal, and other placemaking features.
 - Consider design features that visibly represent and promote the diversity of the Capitol Hill community.

Student space vs. community space

Student walkways vs. using the sidewalks

encourage and support intentional space for street art

create opportunities for refuge from street activity with plantings, seating

take advantage of south facing facades for outdoor activities

Placemaking through art (paving, walls)

Gates/arches to identify space as being a campus

Provide street trees and other natural environment elements to soften the streetscape

Stormwater treatment as an environmental benefit/feature
<https://atyourservice.seattle.gov/2019/07/01/project-spotlight-swale-on-yale/>

Add stormwater treatment features - Sitka development in SLU has stormwater infrastructure that acts as a site amenity & has information/signage

Create site feature(s) that can be placed around the campus to help identify the boundaries of the college. Ex: fountains fed by stormwater.

Streetscape Elevation - E Pine Street



Precedent Images



1
• Sidewalk cafe seating - open seating



6
• Trees/greenery in boxes



7
• Trees/greenery planted in ground



12
• Commercial entrance setback from sidewalk



18
• metal panel facades



23
• Downlighting to light alcoves/cantilevers



28
• Street-level facade is differentiated from facade above



2
• Sidewalk cafe seating - closed off seating area



8
• Parklet with cafe seating - no impact to sidewalk space



13
• Commercial entrance not setback from sidewalk



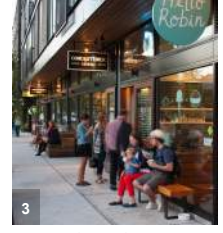
19
• Wood facade



24
• Downlighting to light facade and sidewalk



29
• Facade design is consistent at street level and upper floor



3
• Transparent Glass storefront
• Ground floor steps back from sidewalk makes space for people



9
• Seating areas separated from sidewalk flow with planting



14
• Ground floor facade with stem wall and mixed transparency and opaque surfaces



20
• Brick/masonry facade



25
• Low-level lighting specifically for sidewalk/paths



30
• New housing at Broadway & Denny



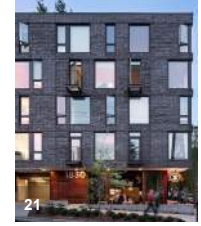
4
• Commercial spaces that open to the sidewalk



10
• Modular sidewalk furniture provides planter boxes and seating
• Different pavement types create zones on the sidewalk



15
• Ground floor facade with complete transparency



21
• Dark brick facade
• Small porches for units



26
• Raised sidewalk crossings for pedestrian safety and traffic calming



31
• Howell St passage - small scale spaces & seating



5
• Banners/ flags to announce the college



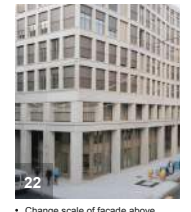
11
• Seating elements of various configurations and sizes



16
• Opaque awning - prevents light from passing thru; hides dirt from view



17
• Glass awning - allows light to pass thru; but dirt visible from below



22
• Change scale of facade above ground level
• Columns land beyond the ground floor windows



27
• Interior activity - dance studios - celebrated and visible to the exterior



32
• Howell St passage - small scale spaces & seating, planted areas

Comments - Precedent Images

General Comments:

-

Image 3:

- Like the building set-back at street level. Like that the bench is against the building; gives space for people to occupy but doesn't take over the sidewalk. Uses a tight sidewalk space well.

Image 4:

- Similar to image 3, I like how space is created for people to occupy, but it doesn't take up a ton of real estate like image 2 or image 1.

Image 5:

- I am supportive of signage for SCC!

Image 27:

- Identify programs that could have a public face and could benefit from it. A restaurant or cafe that's part of the culinary program? A gallery or exhibit space for art, design, textile courses?

Image 30:

- As a community member, I dislike how "blocky" these buildings are and how much they cut off visibility to Cal Anderson from Broadway

Image 31:

- This strikes me as a place to put an arch to identify this area as a campus community

Image 32:

- It took me a couple years of living in Capitol Hill to figure out that this was a walkway. I think it's because I can't see that the pathway continues on. I can be hesitant to walk where there's a blind corner (where the building juts out from the sidewalk)

Discussion

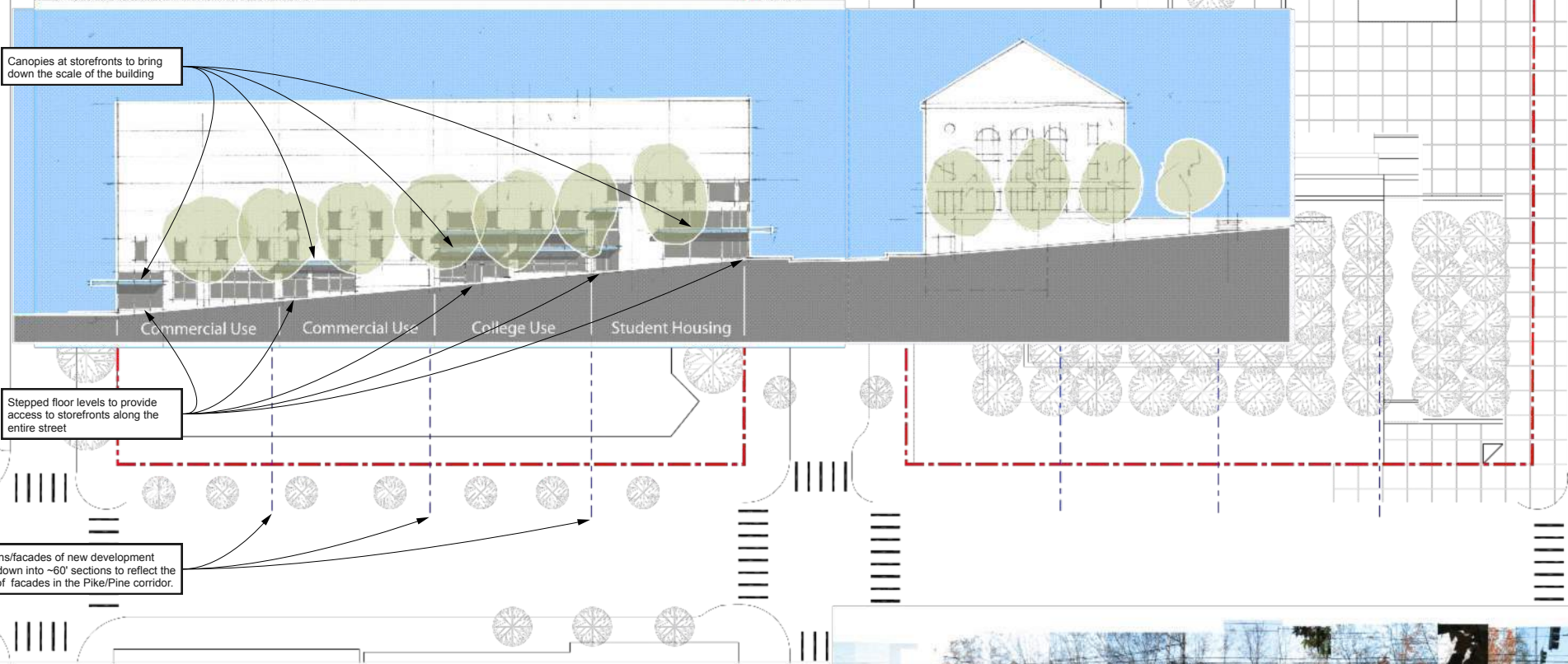
- On Boylston & E Pine, it really doesn't feel like you're on campus. It would be great to make that feel like campus; make the extents of the campus more clear/visible. It'd be great to see the college invest in strong materials and transparency that speaks to the public, create some iconic spaces.
- With the limited sidewalk space on Pine, finding creative ways to create space for people to occupy will be important. I like image 3 and image 4
- At the Fine Arts Building (SE corner of Harvard & E Pine), the regular passerby would have no idea that that's a college building. It's clear that the Egyptian Theater is there and someone could see movies, but someone would never know that it's a college fine arts building with studios. Making SCC's presence more visible at street level and celebrating that presence feels like a missed opportunity.
 - Agreed! On E Pine, the college is in buildings, but there's no public face. Maybe put a gallery for students to exhibit work from art, design, textile courses. Northwest or Northeast corner of Harvard and E Pine could be good opportunities for a space like this.
- I don't hate the red brick as a material on the existing building - it's more about the large, unbroken expanses of it and how the facade feels so dominating on Broadway, especially the northern part between the main entrance and E Howell St. I'd like to see more attention paid to human scale. Photos here have smaller expanses broken up by windows - not as monolithic as the Broadway Edison building.
 - Agreed. The red brick as-is is very monotonous.
- Nearby buildings that have a friendly relationship with the sidewalk and don't feel like they tower above humans:
 - Sunset Electric (SE corner of E Pine & 11th Ave) & Packard Building (SE corner of E Pine & 12th Ave) - the change in material between the bottom 1-2 floors and the rest of the building above helps breakdown the scale of the building. The setback of the floors above the "podium" helps as well.
 - Hugo House - entirely brick and very uniform, but the material changes at the balconies and the change in the facade at the street make it successful.
 - 12th Ave Arts - the setback floors above the podium.
- Lincoln Ferris noted there is a gallery space in the Broadway Edison Building. If that could be relocated to Pine and could host student work as well as some of the artwork the college has from the State collection. Could this be put on display at the street level in the Fine Arts building?

Concept 1

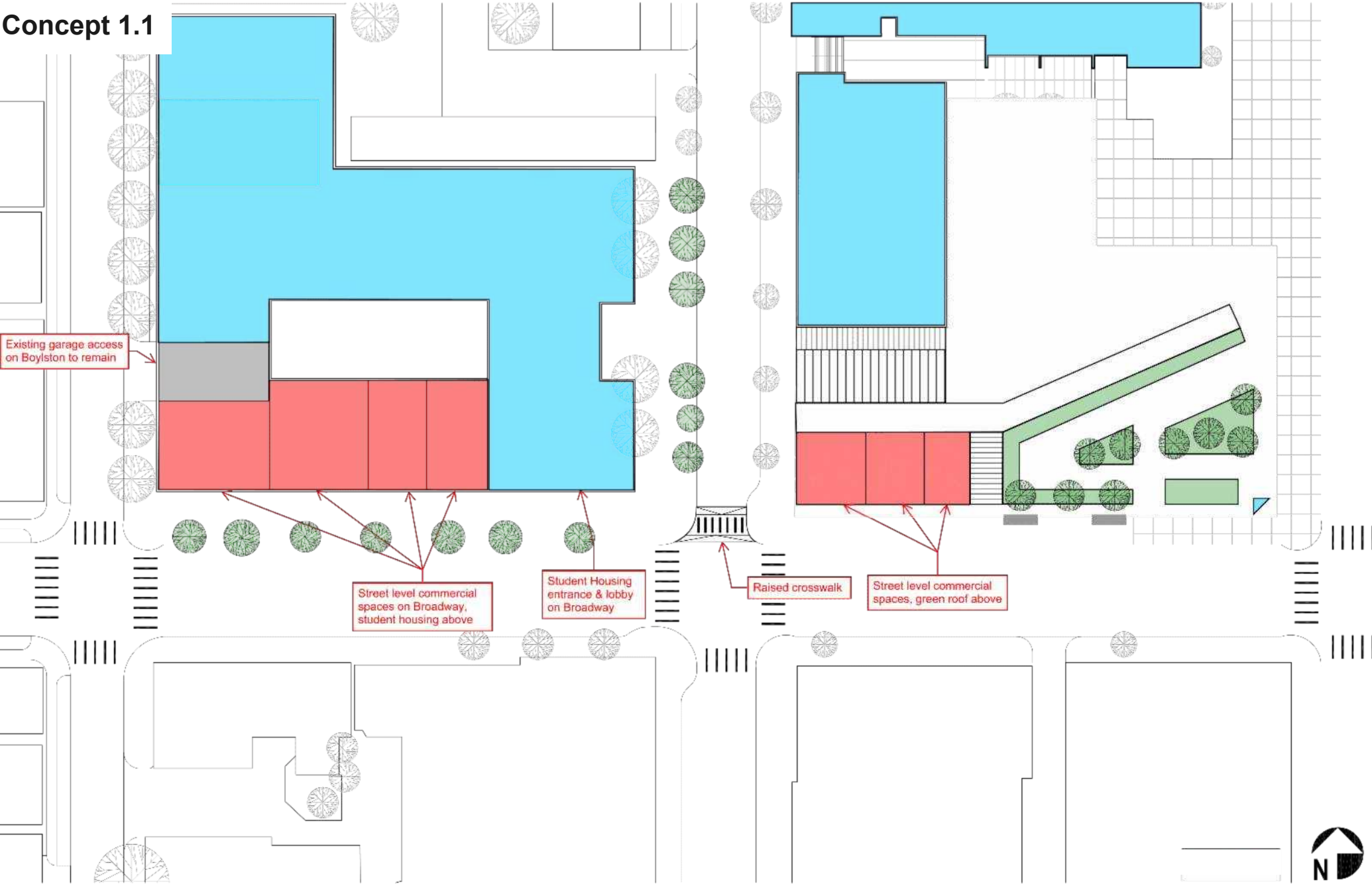
Canopies at storefronts to bring down the scale of the building

Stepped floor levels to provide access to storefronts along the entire street

Elevations/facades of new development broken-down into ~60' sections to reflect the rhythm of facades in the Pike/Pine corridor.



Concept 1.1



Existing garage access on Boylston to remain

Street level commercial spaces on Broadway, student housing above

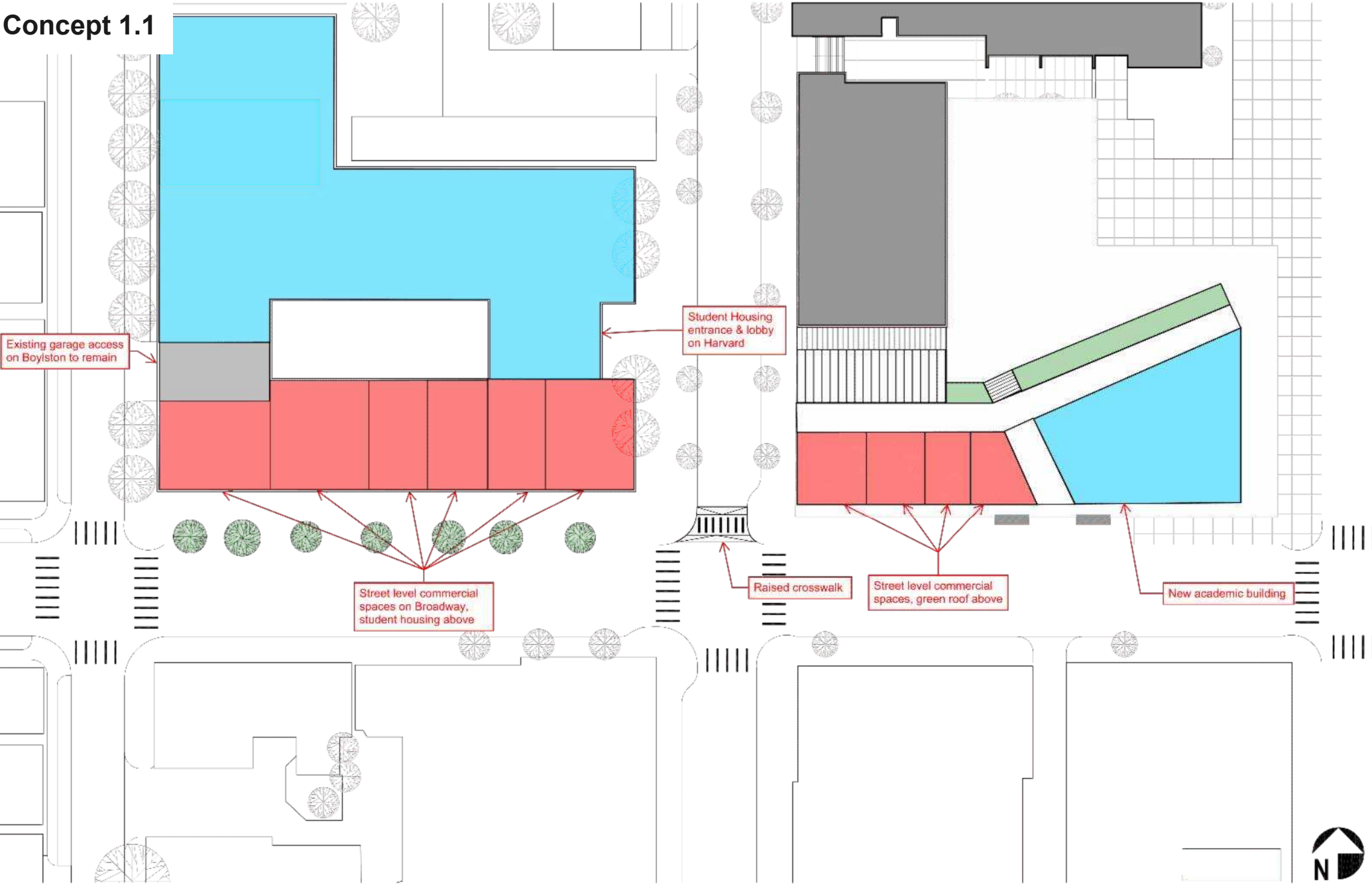
Student Housing entrance & lobby on Broadway

Raised crosswalk

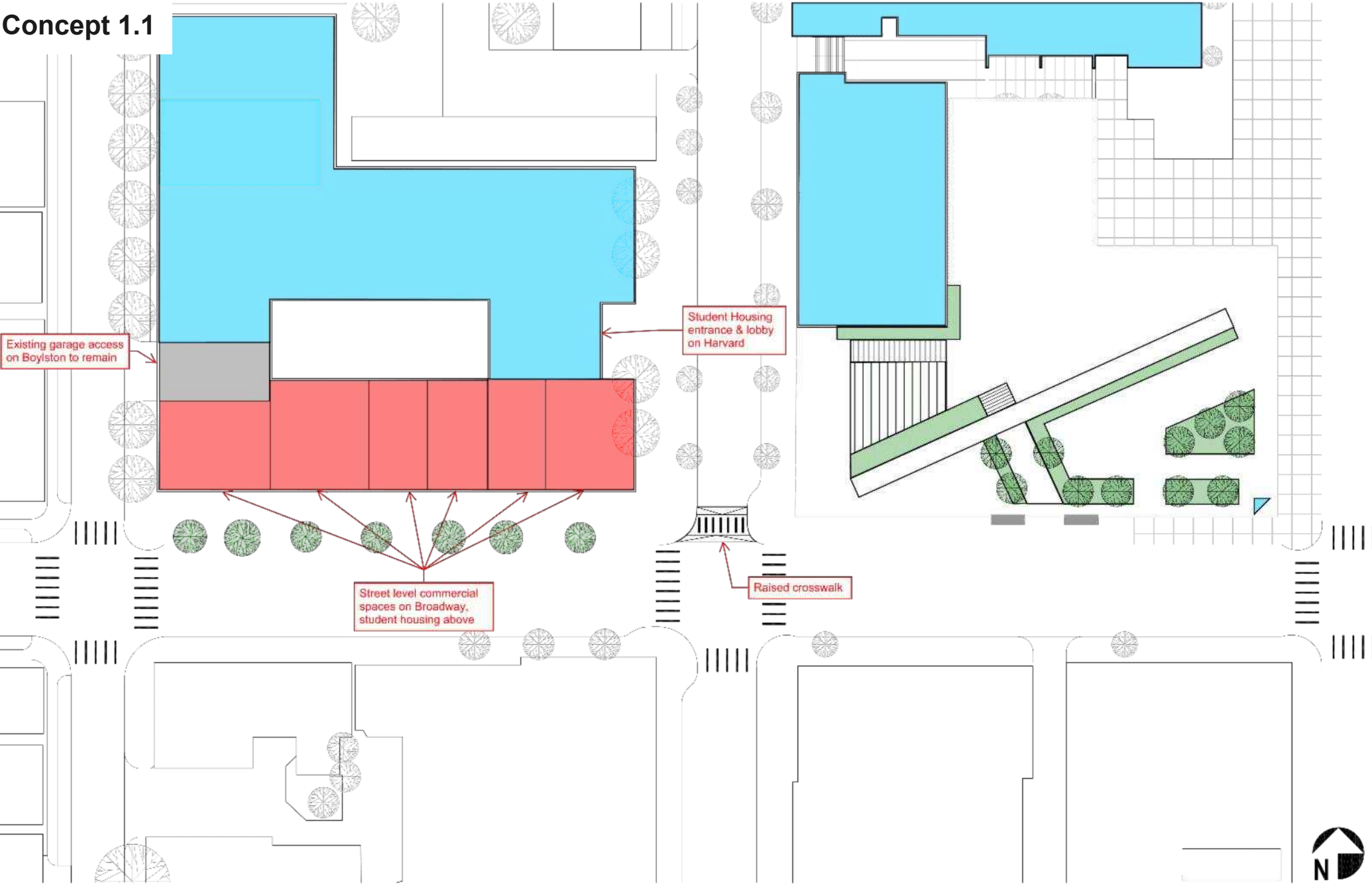
Street level commercial spaces, green roof above



Concept 1.1



Concept 1.1



Comments - Concepts

General Notes:

- CAC attendees liked the focus on providing commercial space along E Pine rather than having college uses at street level.
- Like the idea of having commercial/retail space on the corner of Harvard & E Pine - would add pedestrian activity and help reconnect the gap in the commercial corridor. REALLY like the idea of an accessible green roof above the spaces!
- Most new building don't have the small commercial spaces that you see in older buildings that can host micro-retail, pop-up stalls, etc. It would be great if the college could provide small retail spaces for micro-retail tenants and small start-ups.
 - Chophouse Row and Melrose Market are good precedents - small storefronts and retail spaces with centralized amenities and services spaces.
 - The new Vulcan development at 23rd and Jackson is creating space for pop-up kiosks, could be an interesting precedent.
- I'm struggling to express this, so bear with me. I've heard feedback that students don't feel safe on campus, and as such, they don't use the space. Given this, I'm inclined to make the space "more" for students, with the community goal of making the area more vibrant (by having more people - e.g., students - outside and as part of the community)
- I am inclined to support efficient pathways for students (including pedestrian bridges) to move about the campus, even if that takes away from the number of people on the sidewalks.

Concept 1.2:

- Other urban campuses typically have a central courtyard that is only for students. SCC students don't have that - everything faces out. Having that building could create more privacy or containment for students, while still keeping the space open to the public.
- I like the idea of a building on the corner of Broadway & E Pine to create more of a closed (but still publicly accessible) courtyard at the South Plaza for students. If that building is constructed, I'd want to see the college open up to Cal Anderson more; create a more meaningful and intentional connection to the park.
- I was initially opposed to a building on the corner of Broadway & E Pine because it's valuable outdoor space and I've used it as a refuge - but if it helps create a sense of place and provide a sense of ownership for the students, I could support that. I expect the courtyard/plaza to still be publicly accessible. As for the building, don't want it to be a tall building that puts a shadow over the courtyard.
 - I'd want the building(s) to be low-rise or pavilion-like. Allow southern or western sunlight into the courtyard.
 - A building that helps transition from Capitol Hill to the SCC campus.
 - Could be a library or something that would have lots of students moving in and out throughout the day.
- The mid-block access shown in the sketch is important to keep the courtyard porous and easily accessible to the community

Closing Thoughts / Takeaways

incorporate public art whenever possible

take advantage of public streetscape to increase visibility of college programs (apparel, textiles, graphic arts, theater arts, etc. etc.

Opportunities for inward facing activities & spaces for student

Architectural element to help define extents of campus (gates, arches, banners, etc. etc.)

incorporate green space of any size when possible

A thickened edge between campus and public spaces w/ small parklets, seating (a buffer between students and community)

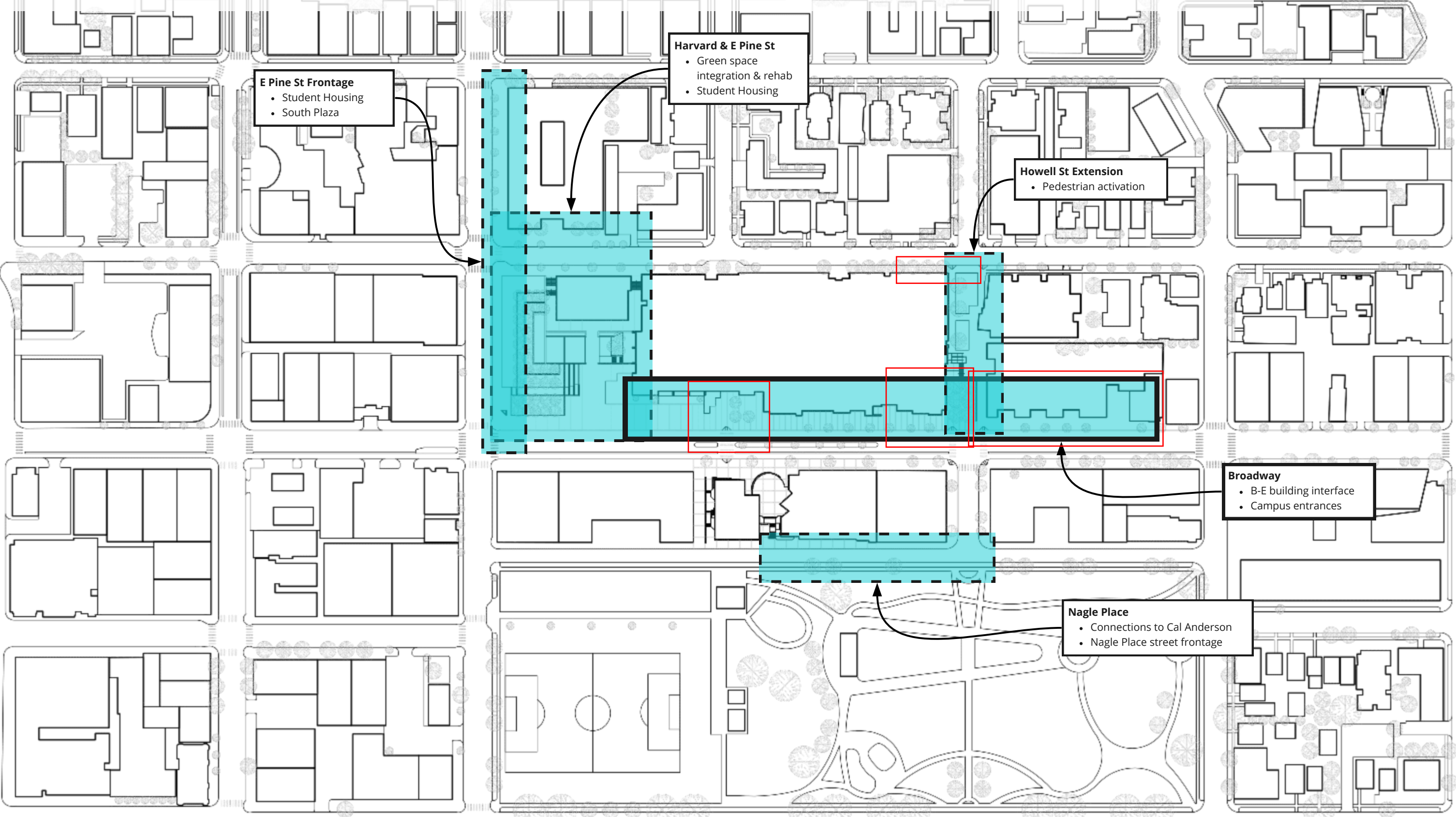
create public retail opportunities for micro business (pop-up, entrepreneurial, micro retailing.

Consider a college structure on the corner of Broadway and Pine

Look for opportunities to open the campus and Broadway connections to Cal Anderson Park (create visibility of routes through the MAC plaza)

SEATTLE CENTRAL COLLEGE MAJOR INSTITUTION MASTER PLAN

BROADWAY STREETScape CHARRETTE



Design Standards - Architectural Design and Character

Architectural Design and Character

Building Façades - Envelope articulation should blend with the established development pattern of the Pike Pine and Broadway street frontages

- Existing structures along Pike, Pine, Broadway, and Harvard generally match the originally platted lots and are characterized by buildings that are 50 – 60 feet wide, or when on two lots are, 100-120 feet in width. The scale of new structures is to reflect the rhythm of bulk and scale established by this existing context.
- Respond to topography by stepping facades so that floorplates generally match the street grade.

Building Materials – All college structures should make use of materials that convey a sense of permanence and importance suitable to a major civic institution

- Materials selections should have a long natural life. i.e., masonry, stone, steel, glass, concrete.
- Material palettes between existing and new structures should create a common visual aesthetic.
- Individual structure should visually represent the functions contained within.

Aesthetic – Building design should represent the highest effective use of public funds and current building technologies.

- Building design should maximize sustainable technologies. i.e., material conservation and reuse, daylighting, sunshades, high performance envelopes, stormwater reuse, energy systems, etc.
- Envelopes should be constructed for a 50-year life span.
- Materials and systems should be easy to maintain and operate.
- Consider solution that will deter vandalism and graffiti.



1 • Opaque awning - prevents light from passing thru; hides dirt from view



2 • Glass awning - allows light to pass thru; but dirt visible from below



8 • Make recesses brighter and more visible



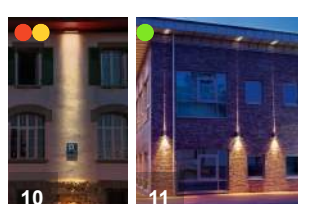
9 • Turn a shadowy area into an accent



3 • Highlight main entrance(s) with material, lighting



4 • Use similar materials at different scales to indicate primary, secondary, tertiary entrances



10 • Up/Downlighting to light facade and sidewalk



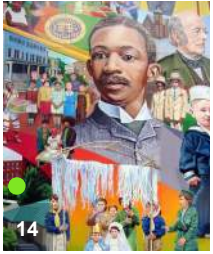
11 • Lights can break down elements of facade



5 • Brick/masonry facade to reinforce identity of campus structures



12 • Mural with a template, students invited to add their own stamp



14 • Murals that honor community members and builders



15 • Stylistic/ artistic murals



16 • Murals that speak to a time period



6 • New construction is distinct from existing



7 • New building respects scale of existing



13 • Art that uses the brick unit as a guide



17 • Broadway streetscape looking South



18 • Broadway streetscape looking North



19 • Broadway Edison Building main entrance



20 • ITEC building concept rendering



21 Anhalt Apartments



22 Seattle University Law School



23 Seattle Academy



24 Bullett Center

The depth of the wall is one thing I think is so imposing. How can the existing wall be reduced in mass?

The skaters may be considered a nuisance/destructive amenity on the building but they are a community. Can a skate area / skate park be incorporated to engage them in a safe way out of the walk space of pedestrians or away from the windows of staff?

Getting rid of the red brick sidewalks / public realm will likely have a tremendous affect of softening the overwhelming red brick of the buildings that exist on campus now

See UW Foster School of Business as a design precedent: the building brings red brick up to contemporary standards, incorporating brick, steel, glass, and wood accents for a warm, modern, inviting feeling with a nod to the aesthetic of the UW campus

accent: innovation in form

Limit the use (or exclude) of red brick as a material in new structures while maintaining a cohesive integration iwth the existing campus, and making aesthetic statements that speak to monumental institutes of higher learning. Use lighter bricks or differnet masonry shapes to provide interest.

Multi-level lighting solutions ex) Lighting up each window-well with varied colors to make the building less scary and more beautiful at night.

Generally more flood lighting at night but not in a creepy way.

lots of dead green space at southeast corner - opporutnity for streetspillover, outdoor office, classroom/gathering space - break up facade.

Articulate facade to allow for the "7 second rule," generate interest along the facade, through street spillover, seating areas, cafe/foodtruck

large blank east facade is a perfect opportunity for A - articulation/screen that divides facade and wraps within the module of the window height. B - SIGNAGE - large - school colors in above banding?

utilize alcoves and moted grass areas to extend the streetscape, perhaps clerestory where current windows are at adjacent to "notes"

Murals that deter graffiti!

Comments from Pine St charrette:

Avoid blank wall surfaces -

encourage small storefront business!

Design for anti-graffiti by avoiding design that attracts it.

<https://campaign.ucsd.edu/imp/act/stuart-collection-alexis-smith-mural/>

<https://art.fam.sf.org/andy-goldsworthy/d-rawn-stone-20045>

campaign.ucsd.edu
Stuart Collection Artist Alexis Smith Donates Cherished Mural - UC San Diego
They are carefully melded to the earth, absorbed into woodlands and swan into the fabric of existing structures. Meandering the 1,200-acre UC San Diego campus, you will likely stumble upon a treasure—a giant, vibrantly colored bird embellished with a g...

Arch Design & Character

CAC suggested Precedent Images



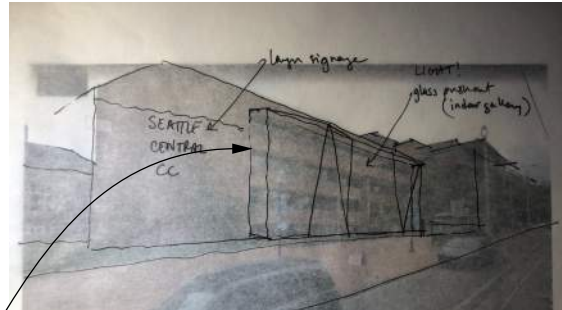
PACCAR Hall at UW
 • Brick facade with steel, glass, and wood - more modern and appealing use of brick on a large university building



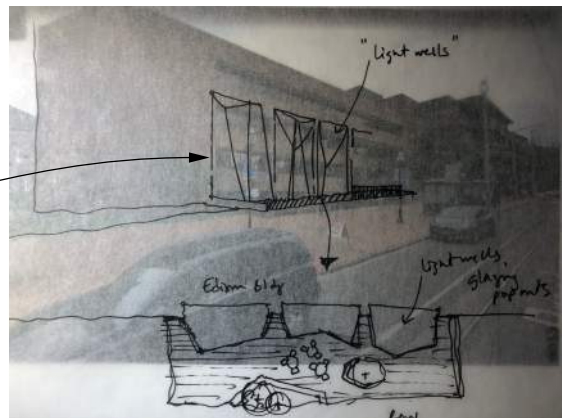
Interdisciplinary Science Center at Eastern Washington University
 • very regular brick facade with canted glass accent elements



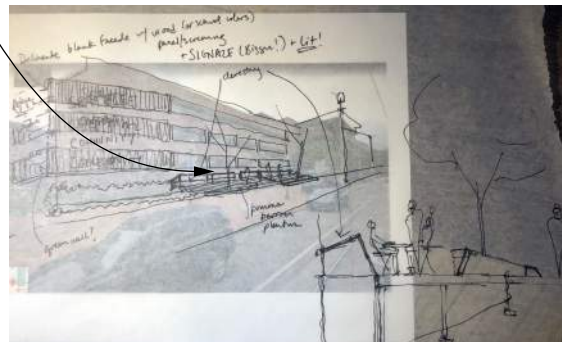
Interdisciplinary Science Center at Eastern Washington University
 • Rhythm of brick facade broken up by canted glass protrusions



• Glass protrusion on Broadway



• Glass "light wells" / "shards" with a small patio off the sidewalk



Cover grass hill to student space, add a small patio, add clerestories for daylight access. Add larger signage or wood panels or bands of color to blank portions of facade.

Architectural Design & Character

Comments - Precedent Images

Image 24:

- it just looks like garbage can with a too-large lid
- The articulation of sustainability of the building to the public is interesting and great!
- Bullitt center overuses metal, does not exude "northwest", is too much based on function over form.

Discussion

- Create a multi-level lighting plan for the campus that addresses different areas/spaces
 - a. General illumination - light street frontage and buildings adequately at the street level for pedestrian visibility and safety
 - b. Secondary lighting at building entrances, plaza entryways, pedestrian pathways
 - c. Accent lighting for artwork, and gathering spaces within larger areas
 - d. Artistic/pop lighting to create visual interest on building facades or in plazas.
 - e. In general, creating a lighting plan that reinforces activity, safety, and creates delight on campus.
- What if new buildings weren't brick and almost turned their back to the Broadway Edison building? By using different materials on new buildings, the emphasis on the brick could get diminished. The common aesthetic that ties the buildings and campus together could be a different element besides brick.
- At Eastern Washington University, the new Interdisciplinary Science Center has brick facade with a very regular rhythm that's broken up by canted glass protrusions. Could use that idea along Broadway to disrupt the monotony of the facade.

Design Standards - Streetscape

Streetscape - All street fronts shall be provided with activity, visual interest, and variety. Consider appropriate use of:

- Canopies or cantilevered structure at walkways along frontages with high levels of pedestrian traffic to provide weather protection.
- Street furniture for College and Community Use. Include at areas to promote activity, and in locations that offer respite to the bustle of busy streets.
- Plantings and trees that create a defined "palette" to reinforce the college "district".
- Use of stormwater detention features for street trees and paved areas.
- Multilevel lighting (General frontage illumination, Pedestrian oriented lighting, Security lighting select building façade/site development lighting).
- Signage that supports definition of the College "district"
- Building and Site materials that are of a pedestrian scale. Palette of materials should that support the definition of the College "district"
- Enhance the pedestrian environment thought inclusion art, societal, and other placemaking features.
- Consider design features that visibly represent and promote the diversity of the Capitol Hill community.



- Incorporate signage to reinforce identity of place.
- Use as a visual and physical orientation element.
- Celebratory elements (graduation pictures, etc.)



- Water retention/bioswale
- Varied pavement textures & materials suggest different uses



- Seating module reconfigured in different sizes and orientations



- Trees/greenery in boxes
- Trees/greenery planted in ground



- Low-level lighting for sidewalk/paths



- Textured/ high-contrast pavement to direct pedestrians



- Street furniture built into sidewalk
- Provides seating and planting beds



- Seating areas separated from sidewalk traffic with planting



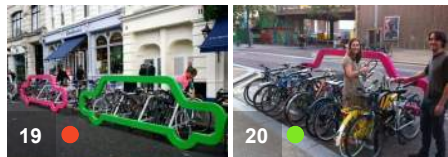
- Downlighting to light alcoves/cantilevers



- Visual texture/ orientation pavement to direct pedestrians



- Modular sidewalk furniture provides planter boxes and seating
- Different pavements create zones on the sidewalk



- Bike corral in street - statement on space provided for (1) car VS many bikes
- No impact to sidewalk area
- Protects bikes from vehicle traffic



- Banners/ flags to announce the college



- Different pavement materials and textures suggest different uses
- Street furniture with simple forms



- Create new seating opportunities built off existing site features (brick plinths)



- Bike shelter with minimal profile & footprint



- Wayfinding signage on sidewalk to direct students and guests



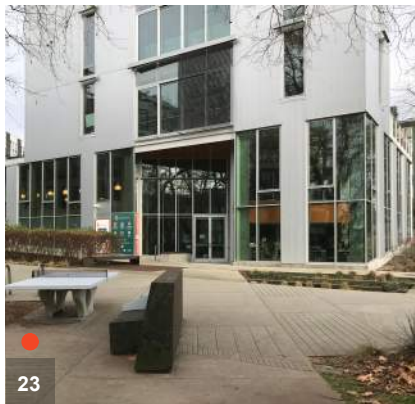
- Use materials with distinct textures
- Retain portions existing brick to suggest SCC paths/boundaries, replace other areas



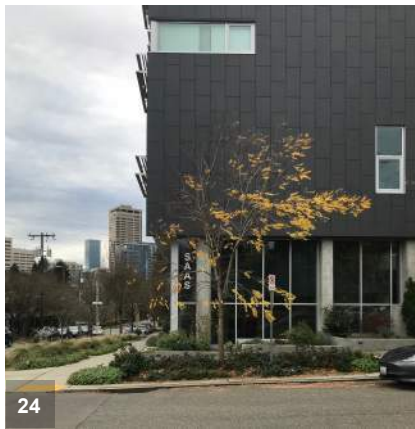
- Art installations that create a sense of place and opportunities for interaction



- Bike shelter with bigger footprint and bolder profile
- Creates a distinct boundary for bike parking



Bullett Center



Seattle Academy of Arts and Sciences



Hugo House



12th Avenue Arts

probably out of scope: change the "one of the seattle colleges" tagline above readerboard on pine and broadway...

design with input from students and/or alumni (with an eye toward quality and sustainability) SCCA is full of talent

opening up cafes and culinary restaurant to seem more public - whether with physical glass/ resurfacing or signage

incorporate greenscape wherever possible within the pedestrian expericne - to soften new AND existing hardscape

I see almost everything on Broadway should have elements of art or creative features to open up the street and create constant expression.

use concrete creatively with irregularly scored areas to define walk routes, public gathering spaces - all combined with colored//tinted and texturized concrete to be a public space material in place of brick.

encourage moving public facing activities and programs to the front of the building and allow for spillover and connection to the streetscape - e.g. culinary / bakeshop / design programs

space for student work features? under

provide a continuity of experiences with similar features, materials, amenities, art, etc. throughout the Broadway walk frontage with specific goals of tying the Howell open space to the south open space/green space.

incorporate music / sound into streetscape?

need more street trees and landscape pits from the mid block crosswalk south to the intxn at Pine St. This area is sorely in need of greenery and tree canopy. Can tree siting here be connected to any recon work onthe Broadway Perf Hall building?

Comments from Pine St charrette:

- Student space vs. community space
Britney Moraski: I'm struggling to envision this, so I hear with me. I've heard feedback that students don't feel safe on campus, and as such, they don't use the space. Given this, I'm inclined to make the space "neutral" for students, with the community goal of making the area more vibrant (by being more people - e.g. students, outside and as part of the community)
- Student walkways vs. using the sidewalk:
Britney Moraski: I'm inclined to support efficient pathways for students (including pedestrian bridges) to move about the campus, even if that takes away from the number of people on the sidewalks.
- encourage and support intentional space for street art
- create opportunities for refuge from street activity
- take advantage of south facing facades for outdoor activities
- Placemaking through art (paving, walls)
- Gates/arches to identify space as being a campus
- Provide street trees and other natural environment elements
- Stormwater treatment as an environmental benefit/feature
<https://atyourservice.seattle.gov/2019/07/01/projects-spotlight-swale-on-yale/>

Streetscape

Comments - Precedent Images

Image 16/17

- I prefer the plantings in ground to root into the street. the boxes seem temporary and fragile.

Image 19/20:

- I like the look of those bike racks and the implication of how many bikes can be parked in the space (1) car would take up.

Image 26

- I like the inset of the building to draw you deeper into the sidewalk. the storefront signage and activity allow more dynamic facade at a smaller streetscape level.

Design Standards - Street-Activating College Uses

Street-Activating College Uses - Integrate new structures with existing street patterns to maintain a cohesive streetscape

- Structures with street frontage facing Pike, Pine, or Broadway shall orient active street-level uses on these streets. The uses should be transparent with visibility into and out of, the structures. Uses should include highly activated functions that bring energy and interest to the street. Such as:
 - Campus Retail (bookstore, coffee shop, bakery, bistro)
 - Food services
 - Student lounges
 - Gathering spaces
 - Meeting spaces (student, college, community)
 - Academic Program Exhibition (makerspaces, digital sandboxes, art gallery, etc.)
 - Fitness Centers
 - Public Safety Offices
 - Performing Arts Venues
 - Community Service Centers
- Provide a strong 2 to 4 story street wall along frontages except where structures are pulled back at major building entrances.
- At major building entrances, provide active pedestrian transition areas between the street frontages and building entrances.

Love the micro-retail opportunities

Student work highlight opportunity - behind glass - like a showcase wall

it'd be nice to see a large glass curtain wall at grade for a mixed use indoor space that could easily be combined with adjacent outdoor space (Howell) to provide a larger public venue for school functions, public use and/or indoor/outdoor food services functions.

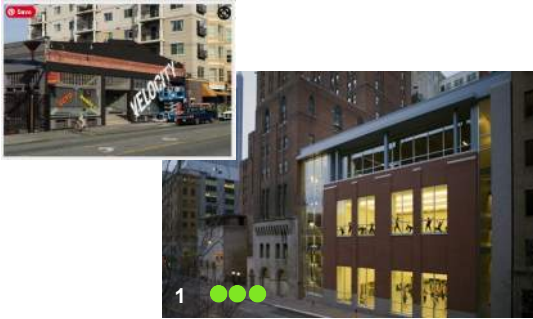
provide a rhythm of whimsy in public features along the streetscape - could be art, could be creative landscaping, hardscaping or building features that make you stop and go hmm

Art intermixed such as like a sculpture park would connect people together. Art for all.

See Westman Bagel on Madison and 15th as precedent for micro-retail space

consider alcoves adjacent to street - catering to bikes/bike cafe

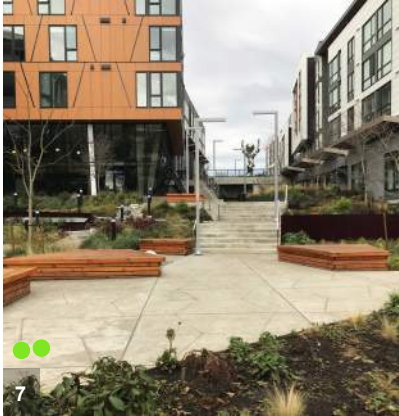
See public plaza and buildings surrounding El Centro de la Raza - restaurant; indoor spaces for public meetings at ground level right off of the plaza with rolling garage doors to easily open up to the plaza



1 Interior activity - dance studios - celebrated and visible to the exterior



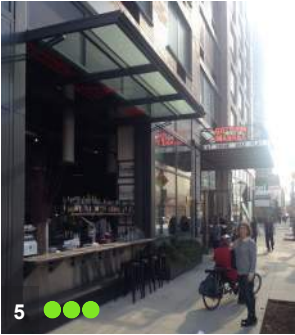
4 Brightly lit social spaces visible to street



7 Using elevation transition to create varying seating and landscaping settings. Use of varied nighttime lighting (general and accent).



2 Ground floor facade with stem wall and mixed transparency and opaque surfaces



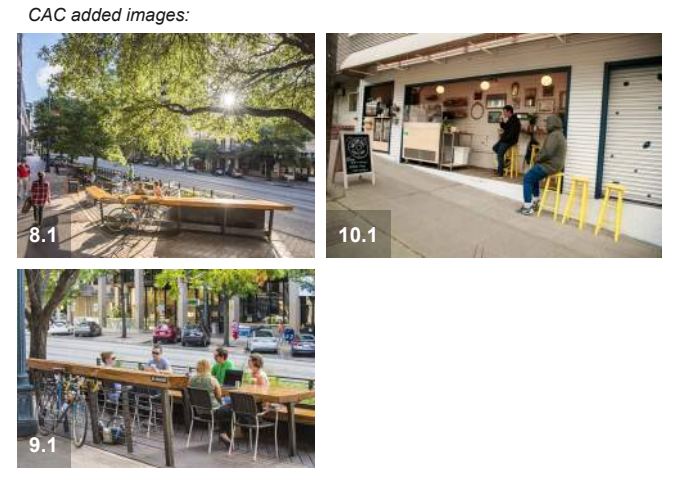
5 Food service that opens to the street



3 Ground floor facade with complete transparency



6 Closed-off sidewalk seating area



Street-Activating College Uses

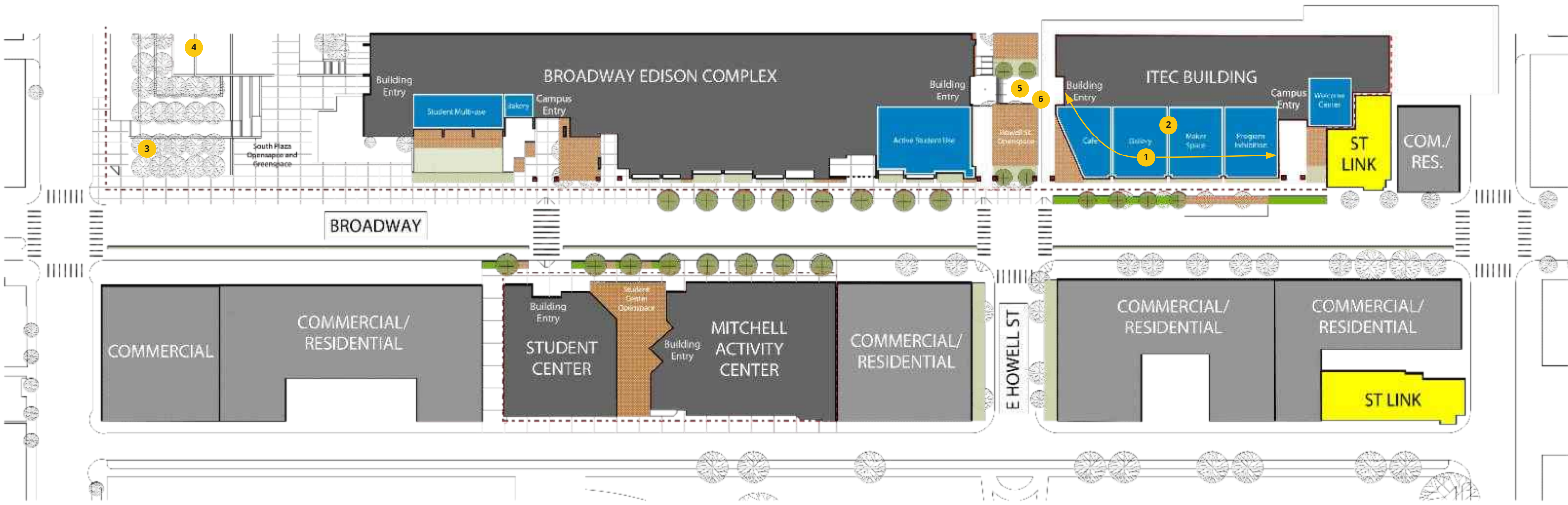
Comments - Precedent Images

Image 6

- Closed off sidewalk seating areas should only be used sparingly, as they break up the larger, public space and there's usually a high volume of pedestrians on Broadway.

Discussion

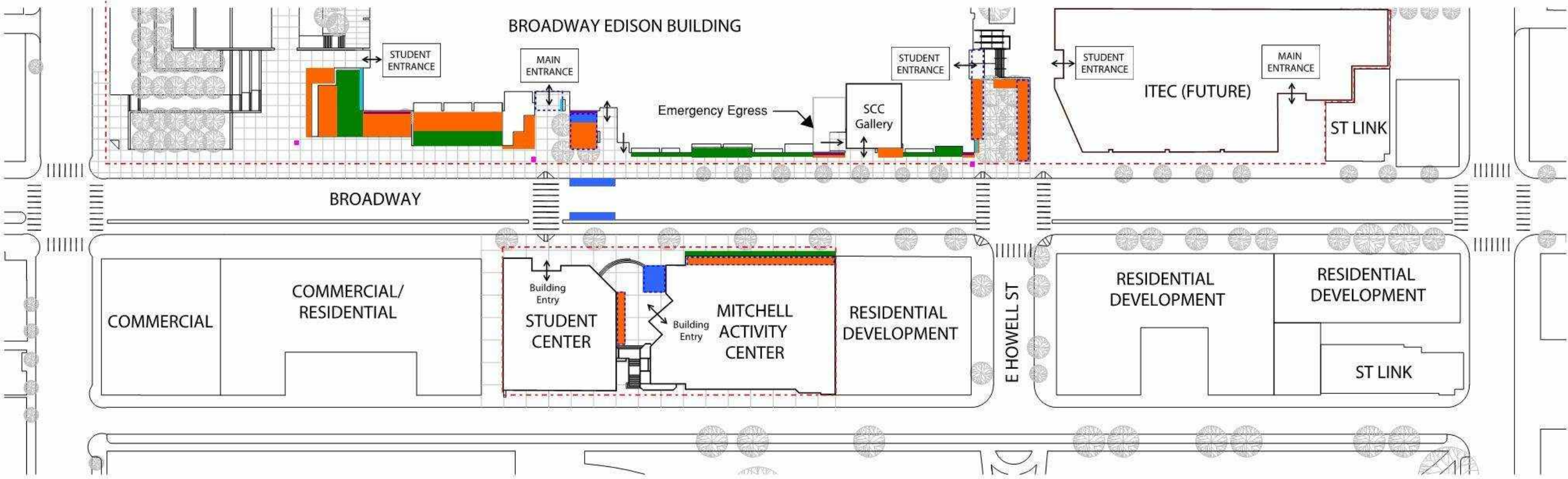
Concept 1



Concept 1 - Detail A

- LEGEND:**
- - SEATING
 - - LANDSCAPING
 - AWNING / CANOPY
 - - ART / MURAL
 - - BIKE PARKING
 - - SCC SIGNAGE
 - - SCC WAYFINDING / DIRECTORY

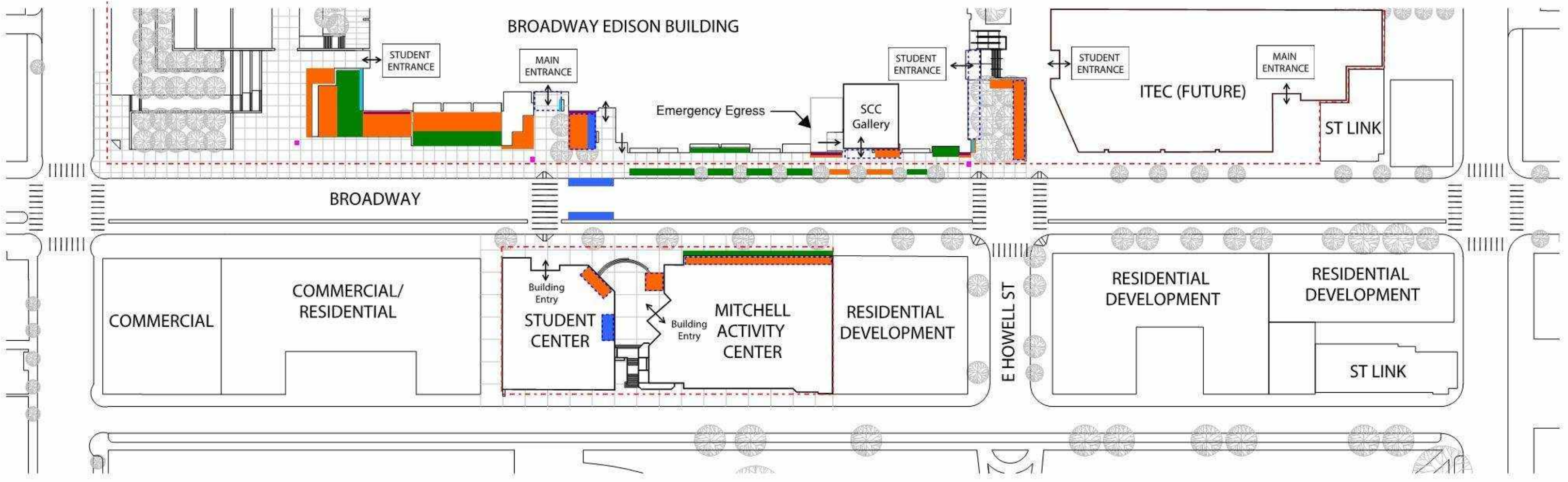
- Conceptual Approach:**
- Seating at edges near entrances
 - Landscaping between seating and building edges
 - Seating at building plazas & entrances
 - Canopies at entrances and seating areas
 - Murals on large blank walls
 - Wayfinding signage at street crossings and plazas



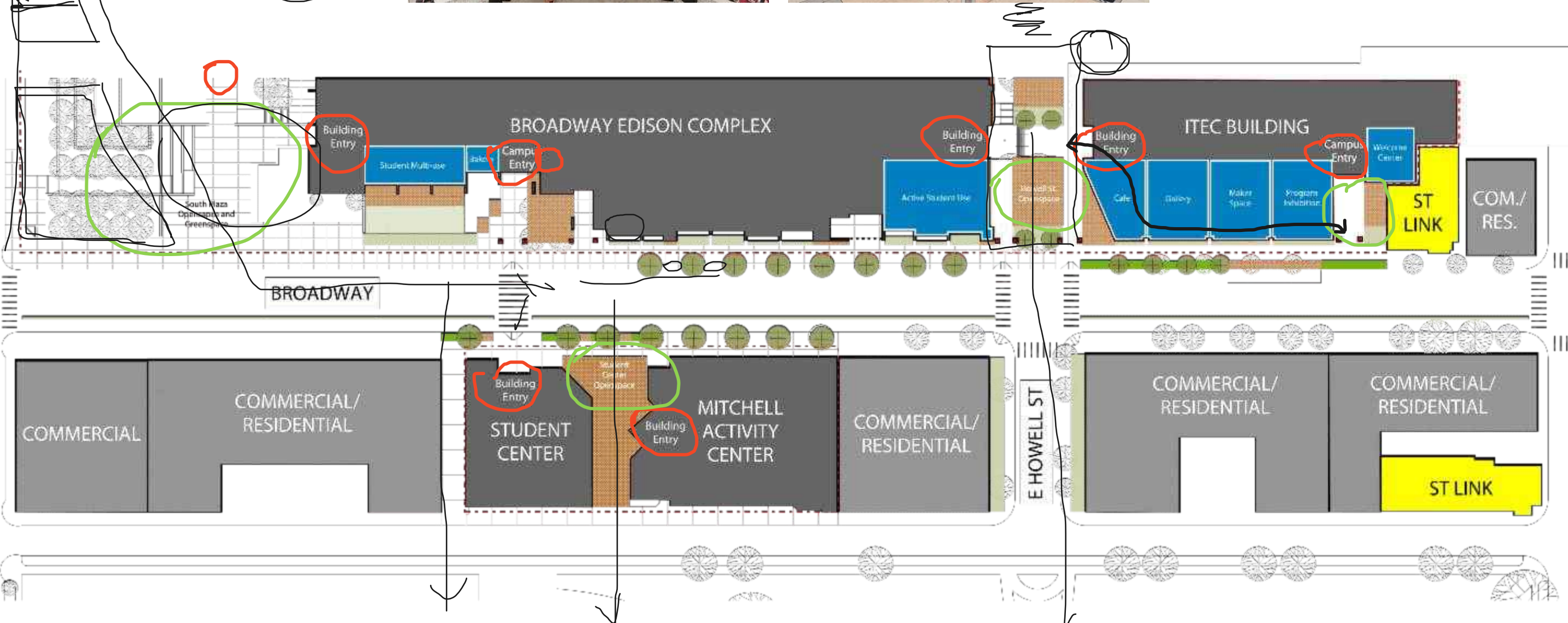
Concept 1 - Detail B

- LEGEND:**
- SEATING
 - LANDSCAPING
 - AWNING / CANOPY
 - ART / MURAL
 - BIKE PARKING
 - SCC SIGNAGE
 - SCC WAYFINDING / DIRECTORY

- Conceptual Approach:**
- Seating at edges near entrances
 - Landscaping between street trees and between building fins
 - Seating at building plazas & entrances
 - Add canopies at entrances and seating near entrances
 - Murals on large blank walls
 - Wayfinding signage at street crossings and plazas



Concept 1 (w/ mark-ups)



Concept 1

Conceptual Narrative:

- Prioritize programs and functions that bring student activity to street-level
 - At the future ITEC building:
 - Welcome Center for new and prospective students near ST Link station
 - Program Exhibition space for student shows, college activities, vendor shows, etc
 - Maker Space / Digital Sandbox
 - Gallery featuring student work and/or artwork from the State collection
 - Cafe with outdoor seating
 - At Broadway Edison Complex:
 - Active Student Use space in place of existing staff offices
 - Improve Student Multi-use space by renovating the adjacent outdoor space and creating a barrier between the sidewalk and adjacent outdoor space
- Open space:
 - Add ceremonial entry points or "gateways" at designated open space to define the college campus
 - Create open spaces of different sizes to offer different functions and uses along Broadway.
- Improve Broadway Edison Complex street-frontage:
 - At street-level windows - add a landscape buffer to soften the edge of the building
 - At solid walls - add seating, art/murals, and a change in pavement
 - Add canopies with lighting to provide pedestrian weather protection and additional lighting on sidewalk
- Add bioswales/stormwater management landscape features at the ITEC building and Student Center

Comments

1. Bring the pathway through the spaces for flexible space at the streetscape.
2. Ability to connect the cafe/ gallery/ maker space and exhibition to hold large campus events.
3. The South Plaza is very piecemeal. The sunken green space and plaza almost feel like a moat. The addition of one or two building along Pine could make the entire space more cohesive and more integrated into the city.
4. Remove weird fencing here.
5. My assumption is there will be SIP upgrades/requirements along Harvard as well and may serve as an opportunity to improve that frontage/pedestrian interface as well
6. If building entries to Science & Math, ITEC and Edison will essentially create a level plaza that extends almost to Harvard street - what is the proposed circulation down 15' to Harvard? will that encroach on the ROW? will it be accessible?

Discussion

- Is any of the work shown underway?
 - Of all the work shown, the ITEC building is currently seeking state funding, and students are planning a renovation/expansion to the Student Center and funding via a self assessed fee. All other work shown on the sketch would need additional funding.
- At ITEC building, I'd be interested in seeing the circulation come closer to the street frontage instead of being buried within the building.
- At ITEC building, having a looser spatial organization at the ground floor that's more open and meandering feels more powerful than the boxed spaces shown.
 - Show that ground floor area as more flexible and adaptable space.
- Will it be possible to renovate the Howell plaza and the stair between the Broadway Edison Complex and ITEC?
 - The plan is that funding for ITEC will include renovation of the Howell Passage, but the scope and extents of that renovation is unknown at this point. The State doesn't fund creation of outdoor spaces unless they are necessary in support of a building.
 - Note: The first floors of Broadway Edison, Science and Math, and the future ITEC are at the same elevation. Meaning any future development of the Howell Passage will ideally have all of those building entrances on a continuous plaza off Broadway.
-

Closing Thoughts

Breaking down facades by using approachable materials

Look at TOD paving development for similar paving solution. Would link a community project with the college.

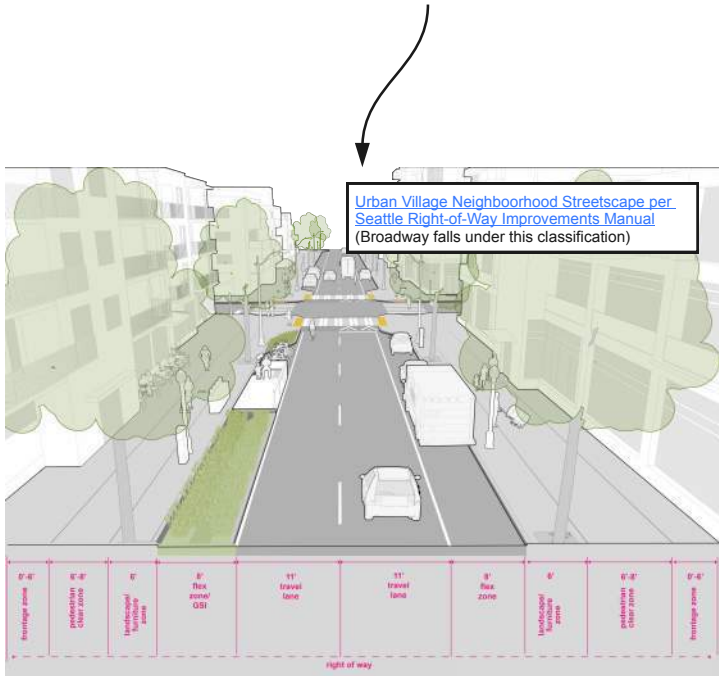
Accentuate the Campus entrance and welcome center such that its obviously located from pedestrian observation

Create cohesion between the open spaces along Broadway with a consistent material palette, site features, paving, etc.

Articulate the facade at street level to create eddys, seating areas for street spillover (i.e. pop-ups & temporary activities).

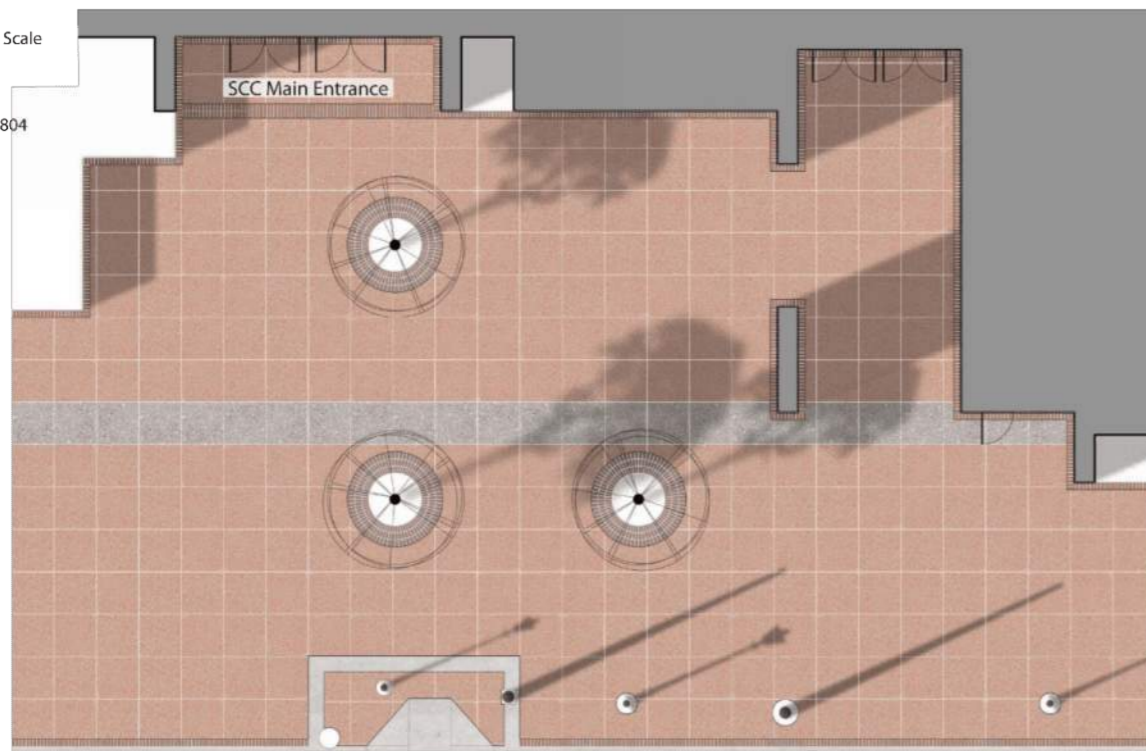
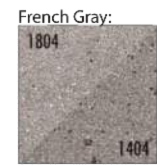
Reference Seattle R.O.W. Improvements Manual for Broadway streetscape improvements

Activate the streetscape with a combination of student and commercial activity (retail, coffee shop, pop-ups)



Related Work - Pavement Design Studies:

Manufacturer - Paver - Stepstone Inc
Linearity Pavers : Large Scale
CalArc Pavers
48" x 48"
Size in Image - Color(s) -
Field: Brick Red - 1816
Accent: French Gray - 1804
Border: Existing Brick



BROADWAY



Sidewalk Pavement Plan - Layout 1

SEATTLE CENTRAL COLLEGE - CAMPUS PEDESTRIAN IMPROVEMENTS

Manufacturer - Paver - Belgard
Moduline Series
24" x 60"
Size in Image - Color(s) -
Field: Graphite, Foundry, Linen
Border: Existing Brick



BROADWAY



Sidewalk Pavement Plan - Layout 2

SEATTLE CENTRAL COLLEGE - CAMPUS PEDESTRIAN IMPROVEMENTS

Manufacturer - Paver - Stepstone Inc
Linearity Pavers : Large Scale
CalArc Pavers
48" x 48"
Size in Image - Color(s) -
Field: Porcelain - 1413, Kona - 1821, Granada White 1801 & 1401
Border: Existing Brick



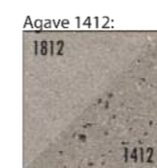
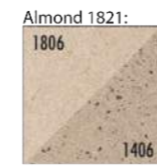
BROADWAY



Sidewalk Pavement Plan - Layout 3

SEATTLE CENTRAL COLLEGE - CAMPUS PEDESTRIAN IMPROVEMENTS

Manufacturer - Paver - Stepstone Inc
Linearity Pavers : Large Scale
CalArc Pavers
48" x 48"
Size in Image - Color(s) -
Field: Porcelain - 1413
Accents: Almond 1406, Agave - 1412, Brick Red - 1416, Kona - 1421, Granada White - 1401
Border: Existing Brick



BROADWAY

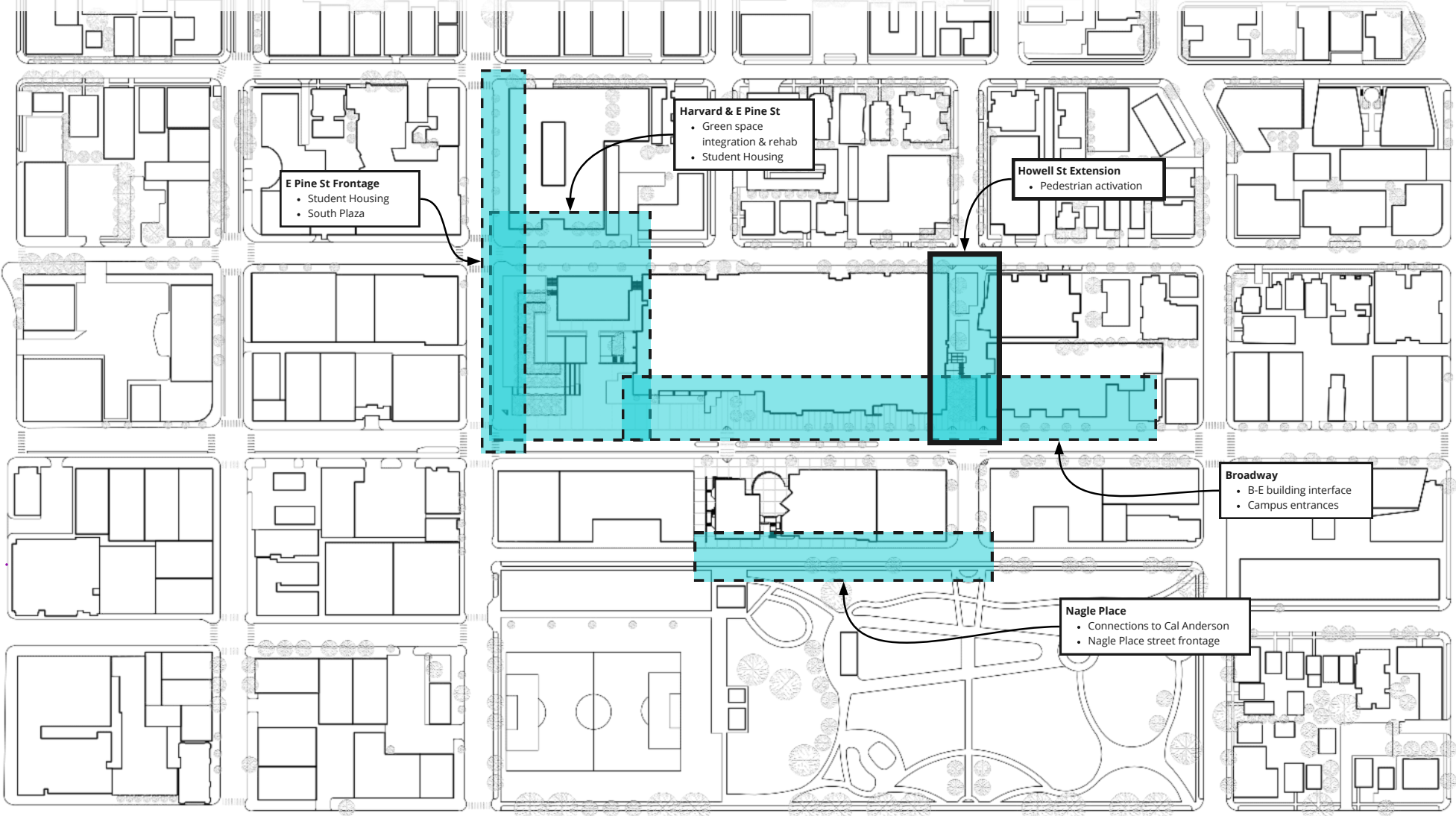


Sidewalk Pavement Plan - Layout 4

SEATTLE CENTRAL COLLEGE - CAMPUS PEDESTRIAN IMPROVEMENTS

SEATTLE CENTRAL COLLEGE MAJOR INSTITUTION MASTER PLAN

C.A.C. MEETING - HOWELL STREET CHARRETTE



Designated Open Spaces

Designated Open Spaces

Seattle Central College has three existing Designated Open Spaces. These spaces serve as focal points for users of the College and are points of interchange with the greater Community. These three areas stand out for their significant role on the campus/community context. They will remain and as opportunities are available (as part of a major capital project) they will be redeveloped and/or enhanced.

- **South Plaza** – a combination of open hardscape and green spaces serving many campus and community functions. This plaza includes a range of activities from highly active to passive and reflective. Proposed improvements include:
 -
 -
- **Howell Street Passage** – a major pedestrian plaza that links the residential neighborhood west of campus to Broadway and beyond to Cal Anderson Park. Proposed improvements include:
 -
 -
- **Student Center Plaza** - located in the middle of campus, directly across Broadway from the major BE Complex entrance, the Student Center Plaza sits at the nexus of student’s daily life on campus (Academics and Services at the BE Complex, Health and Wellness at the Mitchell Activity Center, and Student Life at the Student Center. Proposed improvements include:
 -
 -

There are no new Designate Opens Spaces proposed. The college does propose small open spaces be included at all new capital project where the propose building main entrance meet public street frontages. See Building Threshold Open Spaces for more discussion.

Consider
Open Space
on top of
buildings

Design Standards - Architectural Design and Character

Architectural Design and Character

Building Façades - Envelope articulation should blend with the established development pattern of the Pike Pine and Broadway street frontages

- Proposed college projects with frontages along Broadway, Pine, and the Howell Street Passage shall reflect the rhythm of bulk and scale established by this existing context.
 - Existing structures along Pike, Pine, and Broadway, generally match the originally platted lots and are characterized by buildings that are 50 - 60 feet wide, or when on two lots are, 100-120 feet in width, have expressive structural bay spacing of 25-30 feet, and are typically 5-6 stories tall with strong two-story street frontages at the property line.

Building Materials - All college structures should make use of materials that convey a sense of permanence and importance suitable to a major civic institution

- Materials selections should have a long natural life. i.e., masonry, stone, steel, glass, concrete.
- Material palettes between existing and new college structures should create a common visual aesthetic.
- Individual structures should visually represent the functions contained within.
- Use glass and other transparent materials at street-level to provide visibility of college activities both into and out of the buildings.
- Consider appropriate use of lighting as a material to animate the façades and surrounding public spaces.

Aesthetics - Building design should represent the highest effective use of public funds and current building technologies.

- Building design should maximize sustainable technologies. i.e., material conservation and reuse, daylighting, sunshades, high performance envelopes, stormwater reuse, energy systems, etc.
- Envelopes should be constructed for a 50-year life span.
- Materials and systems should be easy to maintain and operate
- Consider solutions that will deter vandalism and graffiti.



- Up-Downlighting to light facade and sidewalk
- Lights can break down elements of facade



- Highlight main entrance(s) with material, lighting
- Use similar materials at different scales to indicate primary, secondary, tertiary entrances



- New construction is distinct from existing
- New building respects scale of existing



- Skybridge to connect upper levels of adjacent buildings across Howell (not over the Right-Of-Way)



- Murals that honor community members and builders



- Stylistic/ artistic murals
- Add a pop of color to a blank facade



- Mural with a template, students



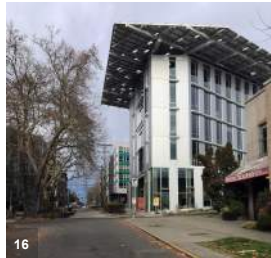
Ansholt Apartments



Seattle University Law School



Seattle Academy



Bullet Center



- ITEC building concept rendering

A contextual package of material without re-using red brick (size/color)

Paver materials that blend with adjacent public spaces. Variety in texture and color

Contextually relate new construction with rest of campus.

Add a guideline for inclusion of public art and its integration in the architecture.

Warm earthy institutional (monumental aesthetic) Stone, concrete (could be tinted)

whimsy, art, creative vibe of Cap Hill. The building/public space as art feature

use curvilinear linework across and between different buildings to continue one line across open spaces. Remove too many angled building points.

Add a layer of whimsy to the building features - facade, windows, lighting. Maybe something suspended above Howell Passage

Comments from Previous Charrettes:

Avoid blank wall surfaces -

encourage small storefront business'

Design for anti-graffiti by avoiding design that attracts it.

articulate facade to allow for the '7 second rule' generate interest along the facade, through street spillover, seating areas, cafe/foodtruck

large blank east facade is a perfect opportunity for A-articulation/screen that divides facade and wraps within the module of the window height. B-SIGNAGE - large - school colors in above banding?

painted or winding concrete sidewalk through brick plaza areas - removes trip hazards without tearing out 100% of brick

Getting rid of the red brick sidewalks / public realm will likely have a tremendous affect of softening the overwhelming red brick on campus now

Architectural Design & Character

Comments - Precedent Images

Image 11:

- BEC blank facades are an unrealized opportunity for murals.

Image 14:

- Like this building as a precedent for a transitional facade for ITEC - using brick to connect with Broadway Edison while providing more glazing and transparency in distinct massings.

Image 15:

- Like the different colored brick and creative use of glazing, brick, and sun shades - not sure if applicable for ITEC, but like that it's not a red brick.

Image 16:

- Don't like the Bullitt Center as an architectural precedent.
- The Bullitt Center has great sustainability features, but the facade bland and uninteresting - not a great precedent for this discussion.

Image 17:

- Safety is overwhelmingly salient on this campus where people consistently feel unsafe. I would want to see clear lines of sight with minimal hiding spaces.

Design Standards - Streetscape and Open Space

Streetscape and Open Space - All street fronts and open space shall be provided with activity, visual interest, and variety. Consider appropriate use of:

- Canopies or cantilevered structure at walkways along frontages with high levels of pedestrian traffic to provide weather protection.
- Street furniture for College and Community use. Include at areas to promote activity, and in locations that offer respite to the bustle of busy streets.
- Plantings and trees that create a defined "palette" to reinforce the college "district".
- Use of stormwater detention features for street trees and paved areas.
- Multilevel lighting (General frontage illumination, Pedestrian oriented lighting, Security lighting select building façade/site development lighting).
- Signage that supports definition of the College "district" Examples include banners, flags, and/or gateways/sculptures.
- Building and Site materials that are of a pedestrian scale. Palette of materials should that support the definition of the College "district"
- Engage will local artists, arts organizations, and community services to enhance the pedestrian environment thought inclusion art, societal, and other placemaking features.
- Consider design features that visibly represent and promote the diversity of the Capitol Hill community.



1 • Create a palette of paving materials for use throughout campus



5 • Create an implied boundary/threshold between the college and the street with art, sculpture, or an open gateway



12 • Use similar plantings, trees, and other landscape elements to reinforce the campus identity/boundary as well as for stormwater treatment/control



2 • Seams and patterns imply paths of travel



7 • Low-level lighting specifically for pedestrian paths



3 • Use different types of paving to imply different uses of space



8 • Site furnishings with integrated lighting



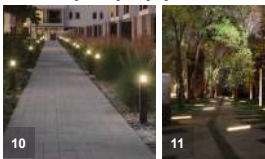
15 • Use large scale seating elements to define spaces



17 • Use changes in grade to create seating areas, planting edges, opportunity for pedestrian lighting. Avoid creating "walls" that isolate



4 • Use seating elements of different materials/scales for different users



10 • Bollard and sidewalk mounted lighting for pedestrian paths



16 • Use floating canopies to both define space, activities, and provide weather protection.

Funtional art.

Stormwater management as a district wide strategy - a holistic approach to site conditions

Greenify the environment of campus -

rainwater cisterns and flows. Incorporate with art.

pollinator pathway.

create independent Greenscaping guidelines.

consider defined palette of plants, trees, bushes, landscaping that helps to brand/define the college - use throughout the public openspaces

Look at Bothell stream/creek daylighting

OHSU Rood Pavilion rooftop park - rainwater re-use

[Pint Defiance Regional stormwater park](#)

Look at Buster Simpson' work in Belltown (Growing Vine Street, Beckoning Cistern)

Copenhagen, Denmark: [Enghaveparken - Climate Park](#)

Northgate Thornton Creek development and OHSU Rooftop Pavillion + Seattle USPS Garden for successful water features



Vine street rainwater management. <https://www.migco.com/work/vine-street-cistern-steps>

Comments from Previous Charrettes:

- Student walkways vs. using the sidewalks: Inclined to support efficient pathways for students (including pedestrian bridges) to move about the campus, even if that takes away from the number of people on the sidewalks.
- encourage and support intentional space for street art
- Placemaking through art (paving, walls)
- Gates/arches to identify space as being a campus
- incorporate greenscape wherever possible within the pedestrian experience - to soften new AND existing hardscape
- provide a continuity of experiences with similar features, materials, amenities, art, etc. throughout the Broadway walk frontage with specific goals of tying the Howell open space to the south open space/green space.
- use concrete creatively with irregularly scored areas to define walk routes, public gathering spaces - all combined with colored/limited and textured concrete to be a public space material in place of brick.
- opening up cafes and culinary restaurant to seem more public - whether with physical glass/ resurfacing or signage

Streetscape and Open Space Comments

- No separate comments - see Streetscape and Open Space page for comments and sticky notes

Design Standards - Street Activating College Uses

Street/Pedestrian Area-Activating College Uses - Integrate new structures with existing street patterns to maintain a cohesive streetscape

- Structures with street frontage facing Pike, Pine, Broadway, or Howell Street Passage shall orient active street-level uses abutting pedestrian areas. The uses should be transparent with visibility into and out of, the structures. Uses should include highly activated functions that bring energy and interest to the streets/walkways. Area-activating uses may include:
 - Campus Retail (bookstore, coffee shop, bakery, bistro)
 - Food services
 - Student lounges
 - Gathering spaces
 - Meeting spaces (student, college, community)
 - Academic Program Exhibition (makerspaces, digital sandboxes, art gallery, etc.)
 - Fitness Centers
 - Public Safety Offices
 - Performing Arts Venues
 - Community Service Centers
- Provide a strong 2 to 4 story street wall along frontages except where structures are pulled back at major building entrances.
- At major building entrances, provide active pedestrian transition areas between the street frontages and building entrances.
- At minor building entrances (student-only entrances) provide site furnishings to encourage student-ownership and presence.

A grand statement at the corner of Howell and Broadway



An opening in the building to expand the building into the public realm (Broadway/Howell/B oth



1 • Display student work in street-facing windows



4 • Student space protected from weather
• Create exterior space for students to occupy



2 • Interior activity - dance studios - celebrated and visible to the exterior



5 • Canopies with lighting to provide additional visibility



8 • Brightly lit social spaces visible to street



11 • Ground floor facade with stem wall and mixed transparency and opaque surfaces



3 • Protected exterior space directly connected to interior space
• Opportunity for outdoor learning



6 • Closed-off sidewalk seating area



9 • Food service that opens to the street

Comments from Previous Charrettes:

- Love the micro-retail opportunities
- Student work highlight opportunity - behind glass - like a showcase wall
- See public plaza and buildings surrounding El Centro de la Raza - restaurant, indoor spaces for public meetings at ground level right off of the plaza with rolling garage doors to easily open up to the plaza
- It'd be nice to see a large glass curtain wall at grade for a mixed use indoor space that could easily be combined with adjacent outdoor space (Howell) to provide a larger public venue for school functions, public use and/or indoor/outdoor food services functions.
- provide a rhythm of whimsy in public features along the streetscape - could be art, could be creative landscaping, hardscaping or building features that make you stop and go hrrm
- consider alcoves adjacent to street - catering to bikes/bike cafe
- closed off sidewalk seating areas should only be used sparingly, as they break up the larger, public space and there's usually a high volume of peds on Broadway

Street/Pedestrian Area-Activating College Uses

Comments

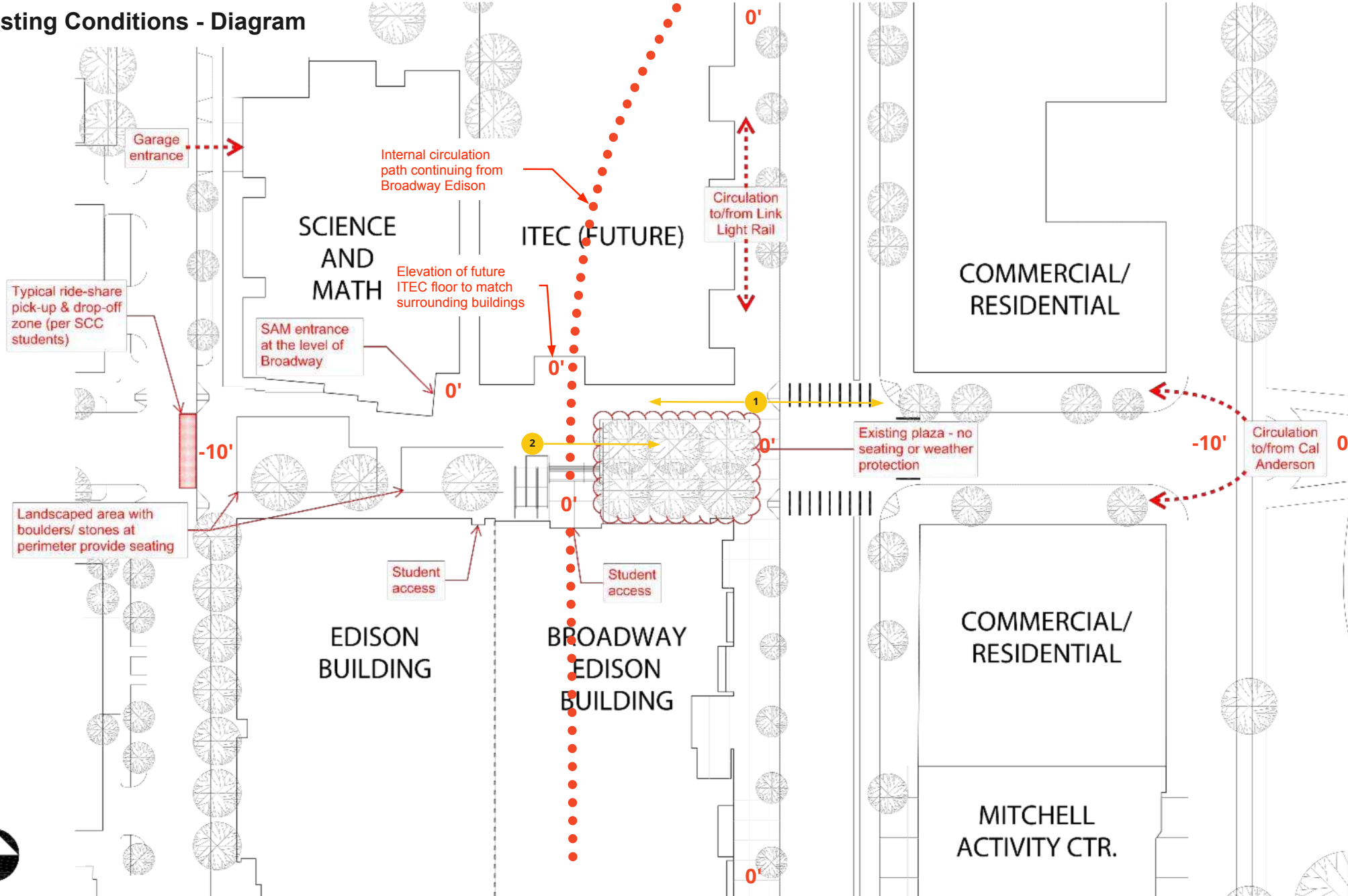
Image 3:

- Umbrella Walkway art found in many European cities good example of how this can look better

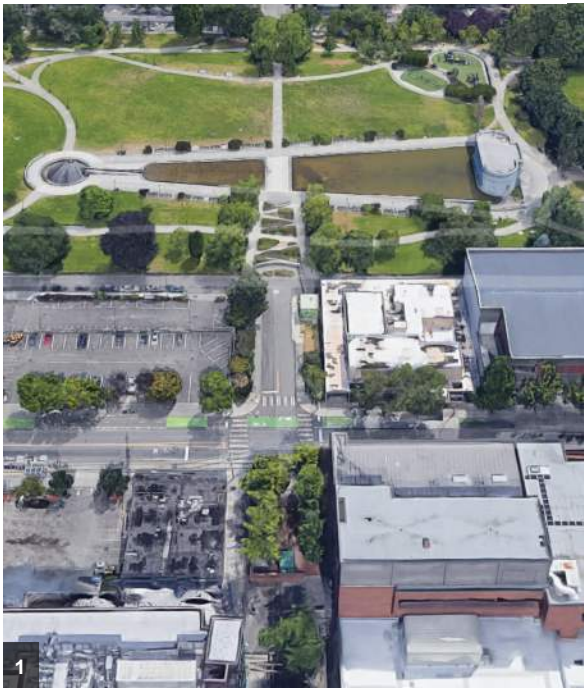
Image 5:

- We don't want features to make the Howell Street passageway feel more narrow and cramped than it really is

Existing Conditions - Diagram



Existing Conditions - Images



Existing Conditions - Diagram

Comments

1. Poor existing site lines
2. Establish a visual connection to Cal Anderson and to the west

Existing Images

Comments

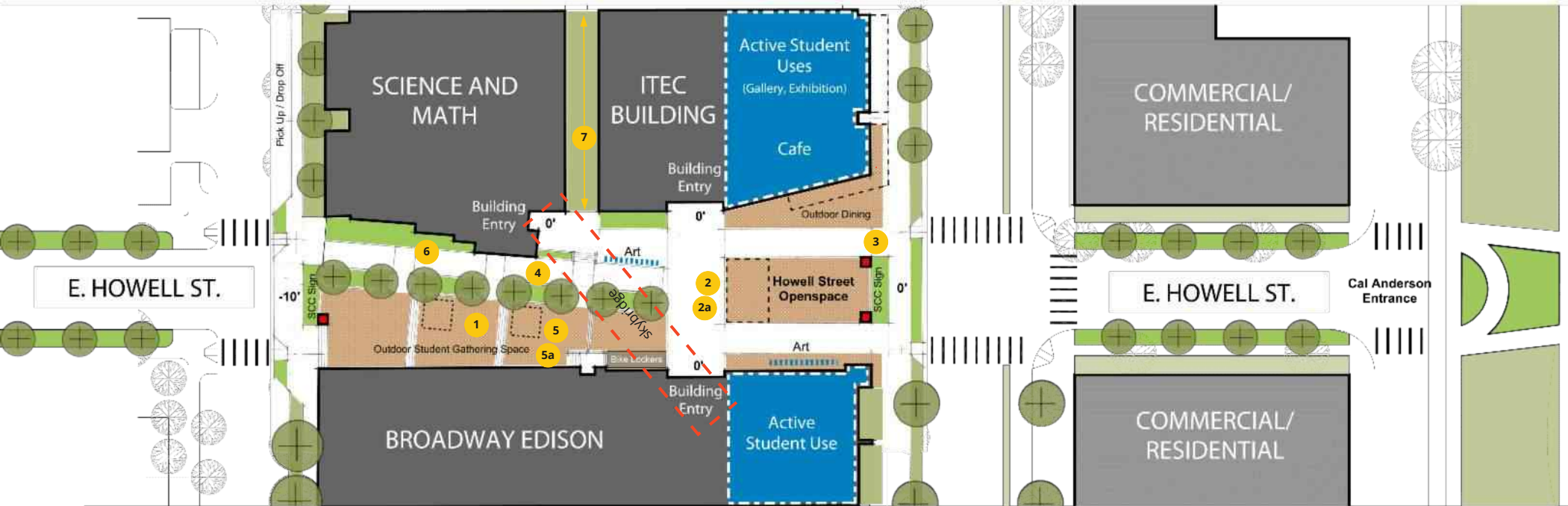
Image 1:

- Rounded building entrances increase visual interest and provide meaningful social space. Example: SW corner of Central Park in NY
 - When used across from open space corners*

Image 2:

- The large tree (behind the motorcycle) is out of scale with the pedestrian space and blocks any view through the passage. There may be corners of undefined space that would benefit having a large tree like this, but in the middle of the space, a tree 20'-50' tall would be better.

Concept 1



Concept 1

Conceptual Narrative:

- Provide a mix of student and commercial/public activity where Howell Passage meets Broadway.
 - Cafe with outdoor seating in ITEC.
 - Active Student use (such as a lounge or study space) in Broadway Edison.
 - Provide open space with seating, landscaping, and art.
- Connect the entrances of Broadway Edison, Science and Math, and ITEC with a continuous plaza.
- Create small-scale outdoor gathering space for students away from the noise and activity of Broadway
 - Provide canopies, bike lockers, and site seating.
- Use an straight ADA-accessible ramp to make-up the grade change between Harvard and Broadway (~10').
 - Ramp can also be used for service vehicle access to Broadway - something the College does not currently have.

Comments

1. Worried that if the space is too segmented and broken up into different areas aren't connected smoothly, it might feel too cramped.
 - a. Especially with the overhead features like the rain shelters and skywalks.
2. A value-statement earlier was that there's great value in having the open space focused on Broadway. The more that the space can be oriented to Broadway and be maintained as one-large open space, the better. Stay away from sub-dividing it too much.
 - a. This concept as shown feels very broken-up - better to have one large at-grade area.
3. Soften the edges/thresholds with smooth or organic shapes.
4. There is more value for the public and the college in creating a larger at-grade plaza with a switch-back ramp than having a plaza with many levels and a long, continuous ramp.
5. Not sure how student would use this space - very segmented. Concerned that it wouldn't be used and would be taken over by an undesirable use.
 - a. Agreed. Engage the college and students. Ask what they would like that space to be, how it could be used, what activities could happen there.
6. Lots of lines and angles. I'd look at Cal Anderson with it's segmented arches, and curved concrete paths as a reference to soften the hard edges and lines as shown. Could come through in the paving or signage as well.
7. Possible to reclaim this space between Science and Math & ITEC as interior building space, an art walk, a path to light rail, other?

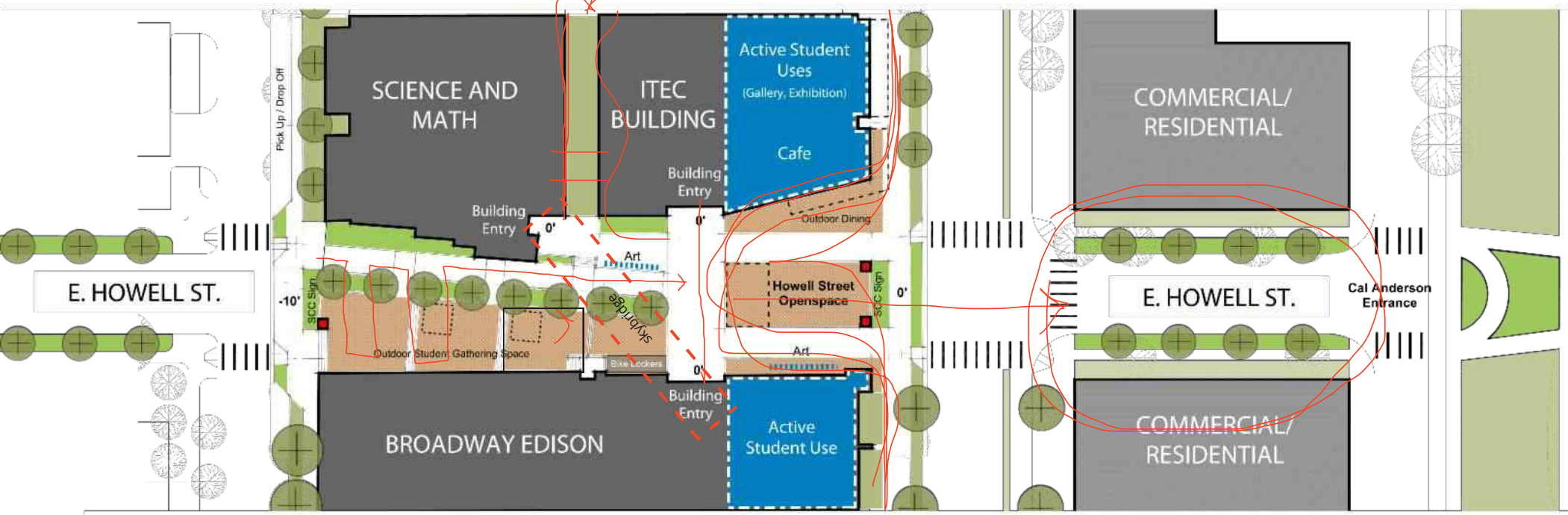
Discussion

- Create Landscaping Guidelines
 - Address how heritage trees will be dealt with. I would support the removal of heritage trees for the sake of defining new space and encourage you to make that statement in the guidelines.
 - Create a brand/identity for the entire campus
 - Language for landscaping in different outdoor spaces that could inform how the spaces are used.
 - Stormwater mitigation, rain garden features.
- Adding a well-designed service enclosure could be an option if it can add other positive attributes to the space (better views, better public space)
 - Service enclosure discussed would be on the west-end of Howell Passage and public space above it would be at the same elevation as Broadway. Alternate location discussed is the stairs between the Broadway Performance Hall and Broadway Edison. If those stairs are closed removed, could a service enclosure be added there.
 - Currently, college has more garbage than they have interior storage for. The garbage area in the loading/receiving area is at capacity which is why dumpsters are commonly seen on Howell and Harvard.
- A larger, more unified open space at the elevation of Broadway is more desirable than a terraced space.
 - Make the space feel as wide as possible - especially if there's a service enclosure accessed from Harvard, don't want the path/space to bottle neck.
- Skybridge;
 - Maximize transparency of any bridge
 - Make it as high as possible so it doesn't loom/dominate over Howell Passage - don't want it to block visual connections and site lines.
 - Push the bridge to the west/ mid-block as much as possible. Don't want a skybridge to feel like a continuation of the Broadway Edison elevation - want to preserve the break in massing provided by Howell Passage.
 - Making a skybridge wide enough to hold lounge space for students or host activity is a positive.
 - A diagonal bridge connecting Broadway Edison and Science And Math may provide a bridge with less visual weight than a bridge connecting Broadway Edison and ITEC.

Concept 1 (w/ markups)

Provide landscape guidelines

Stormwater/Raingarden



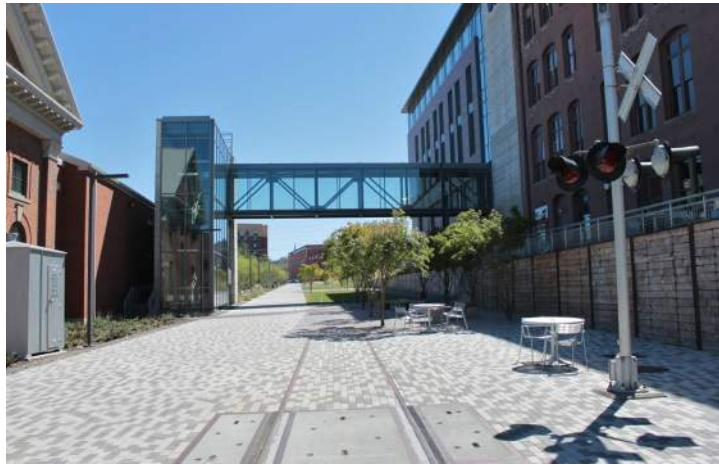
Skybridge Precedents

I think they need to be tall so as to not cause claustrophobia to pedestrians underneath. Having them be tall and wide with student space is an opportunity to add more student spaces AND is more aesthetically pleasing.

Concern is increasing shadows so it needs to allow for natural light through it and presents an opportunity to have eyes on the street.

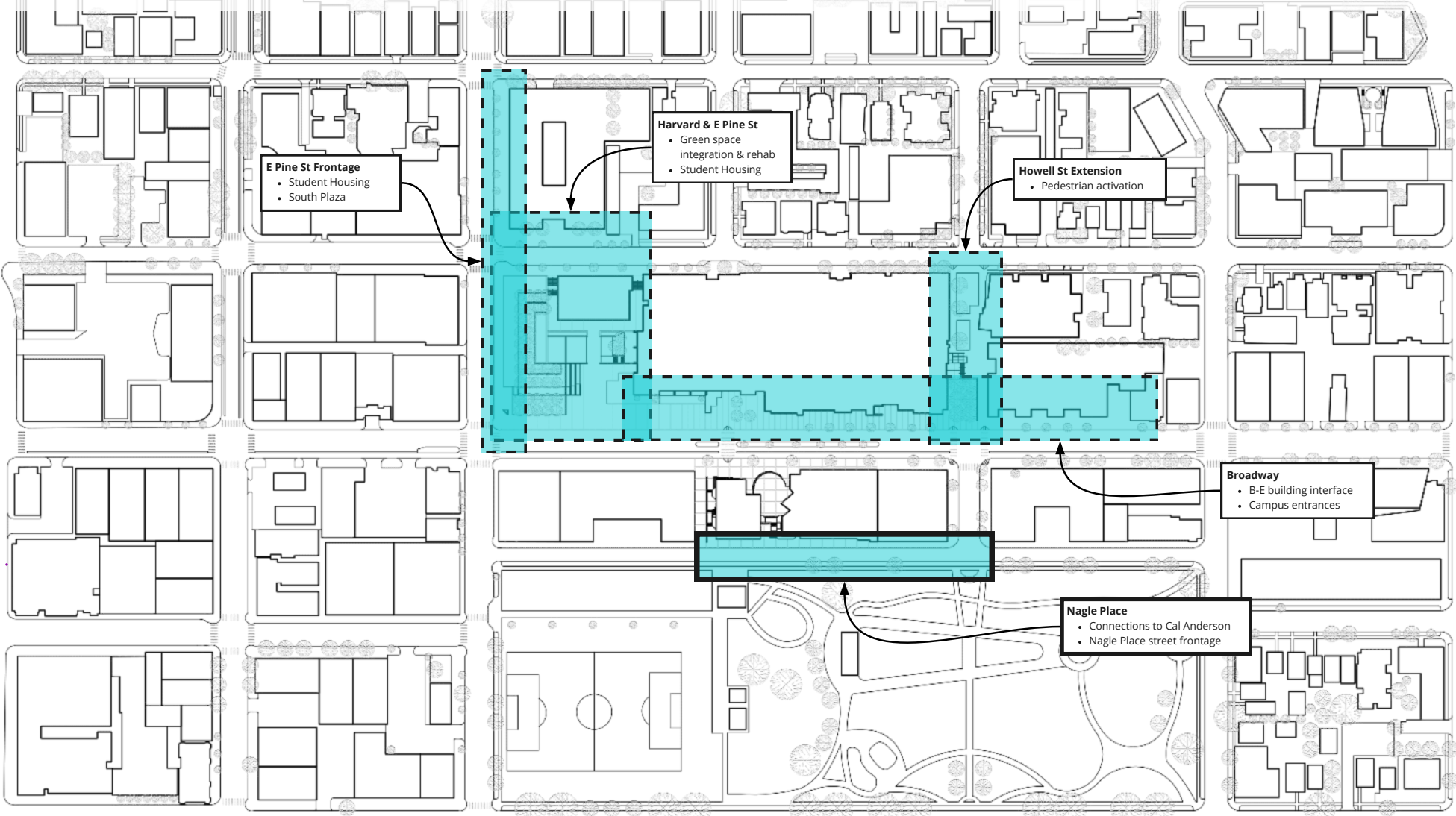
Great example: Allen Library skybridge connection to Suzallo Library (bridge uses a similar material palette as adjacent buildings)
 Poor example: Green River College satellite campus at Kent Station.

Don't like the massiveness of this skybridge - the space below looks/feels dark and cold. Want our space to be more bright & open

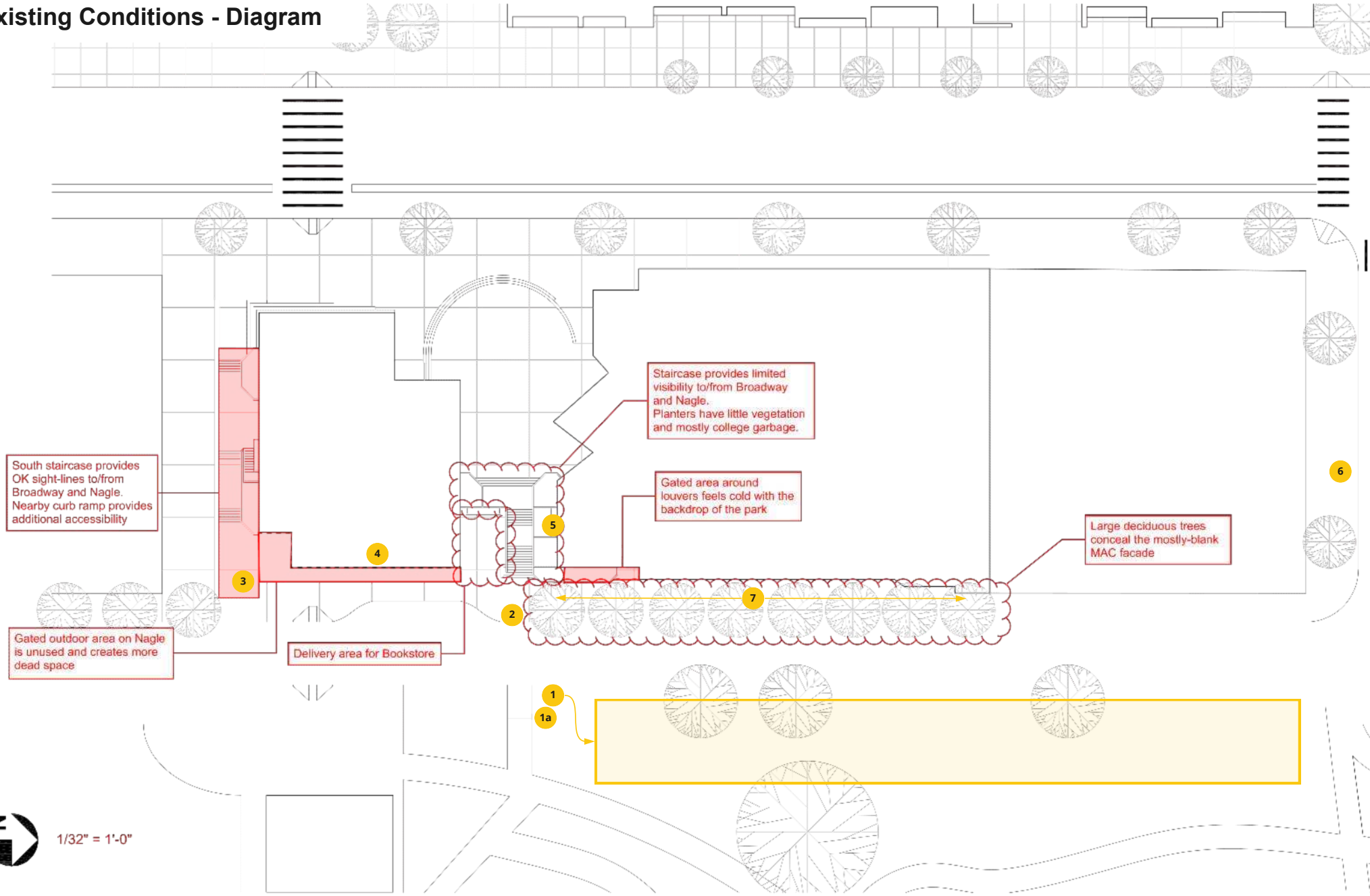


SEATTLE CENTRAL COLLEGE MAJOR INSTITUTION MASTER PLAN

C.A.C. MEETING - NAGLE PLACE CHARRETTE



Existing Conditions - Diagram



South staircase provides OK sight-lines to/from Broadway and Nagle. Nearby curb ramp provides additional accessibility

Gated outdoor area on Nagle is unused and creates more dead space

Delivery area for Bookstore

Staircase provides limited visibility to/from Broadway and Nagle. Planters have little vegetation and mostly college garbage.

Gated area around louvers feels cold with the backdrop of the park

Large deciduous trees conceal the mostly-blank MAC facade



1/32" = 1'-0"

Existing Conditions - Images



MAC-Student Union Plaza



South Stairwell - standing @ Broadway looking East



South Stairwell - standing @ mid-landing looking East



MAC-Student Union Plaza - top of stairs looking at Cal Anderson



Bottom of stairs looking towards Broadway



MAC service room door on Nagle Place



Delivery access for MAC & Student Union



South Stairwell



Nagle Place Streetscapes



Existing Conditions - Diagram

Comments

1. This area not used very much. Could this change to natural landscaping?
 - a. Right now this area is concrete...what if it became a grassy hill with a ramp for ADA access?
2. Generally in favor of closing the entrance from the MAC.
3. Improvements to this south stair are needed. Better lighting, more signage/visibility.
4. Want to bring more views to the park from the Student Center - more eyes on the park and visual access/connection.
5. Add more greenery on stairwell to transition to the park.
6. What is the slope of the sidewalk on Howell? Could someone in a manual wheelchair roll up that street?
7. Want visual connections from the MAC at Nagle to the street & park. More porosity and vibrancy from being able to see into and out of the building because this sidewalk is dead space right now.

Design Standards - Architectural Design and Character

Architectural Design and Character

Building Façades - Envelope articulation should blend with the established development patterns of Capitol Hill neighborhood

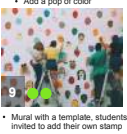
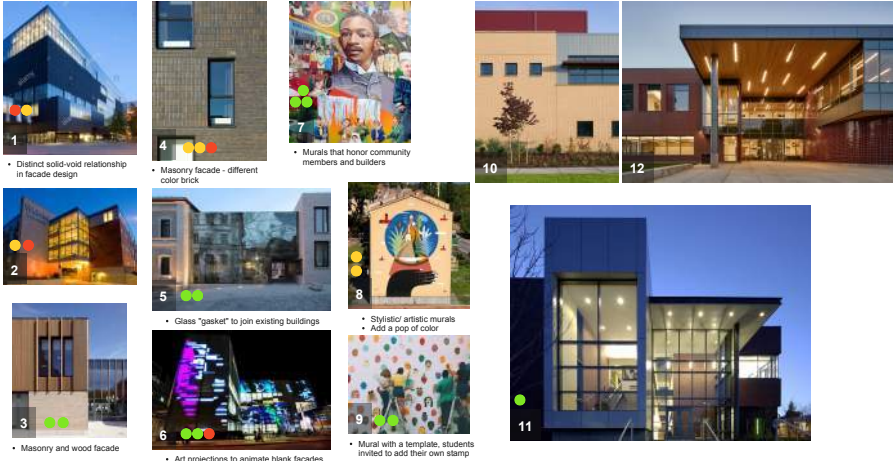
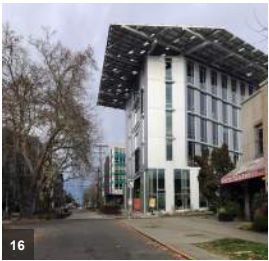
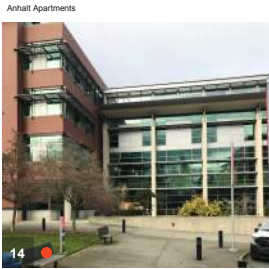
- Proposed college projects with frontages along Broadway, Pine, and the Howell Street Passage shall reflect the rhythm of bulk and scale established by this existing context.
 - Existing structures along Pike, Pine, and Broadway, generally match the originally platted lots and are characterized by buildings that are 50 – 60 feet wide, or when on two lots are, 100-120 feet in width, have expressive structural bay spacing of 25-30 feet, and are typically 5-6 stories tall with strong two-story street frontages at the property line.
- Proposed college projects with frontages along Nagle Place, Harvard Ave, Boylston Ave, and East Howell St shall exhibit details at street level to respect the pedestrian scale.
- Proposed college projects facing any street frontage shall endeavor to put activity behind the building facades to avoid creating dead space along the street.

Building Materials – All college structures should make use of materials that convey a sense of permanence and importance suitable to a major civic institution

- Materials selections should have a long natural life. i.e., masonry, stone, steel, glass, concrete.
- Material palettes between existing and new college structures should create a common visual aesthetic.
- Individual structures should visually represent the functions contained within.
- Use glass and other transparent materials at street-level to provide visibility of college activities both into and out of the buildings.
- Consider appropriate use of lighting as a material to animate the façades and surrounding public spaces.

Aesthetics – Building design should represent the highest effective use of public funds and current building technologies.

- Building design should maximize sustainable technologies. i.e., material conservation and reuse, daylighting, sunshades, high performance envelopes, stormwater reuse, energy systems, etc.
- Envelopes should be constructed for a 50-year life span.
- Materials and systems should be easy to maintain and operate
- Consider solutions that will deter vandalism and graffiti.



Comments from Previous Charrettes:

<p>Avoid blank wall surfaces -</p>	<p>encourage small storefront business'</p>	<p>Design for anti-graffiti by avoiding design that attracts it.</p>	<p>articulate facade to allow for the '7 second rule' generate interest along the facade, through street spillover, seating areas, cafe/foodtruck</p>	<p>large blank east facade is a perfect opportunity for A-articulation/screen that divides facade and wraps within the module of the window height. B - SIGNAGE - large - school colors in above banding?</p>
<p>painted or winding concrete sidewalk through brick plaza areas - removes trip hazards without tearing out 100% of brick</p>	<p>Getting rid of the red brick sidewalks / public realm will likely have a tremendous affect of softening the overwhelming red brick of the buildings that exist on campus now</p>	<p>Add a guideline for inclusion of public are and inclusion in the architecture.</p>	<p>A contextual package of material without re-using red brick (size/color)</p>	<p>whimsy, art, creative vibe of Cap Hill. The building as art feature</p>

Architectural Design & Character

Comments - Precedent Images

Image 1:

- This is too large and domineering. intimidates you and doesn't have a pedestrian scale to it.

Image 3:

- warmer and raw materials rather than crisp and hard materials. Makes me want to touch it.

Image 4:

- Feels boring.

Image 5:

- like the connection without blocking the view through to the park, but can create more interior circulations.

Image 6:

- Like using art to activate a space.
- Can be able to change over time with other schemes but has a technology to build upon.

Image 7:

- so much about the community and represent the history and evolution of the area in a pictorial way. people can see themselves in different parts

Image 11:

- like large windows and openings that encourage people to be in the space and connect to the outside. not necessarily personally engaging because the window is so large. see movement and people passively

Image 16:

- like the expression of sustainability at the Bullitt Center.

Image 17:

- Would invite tagging

Design Standards - Streetscape and Open Space

Streetscape and Open Space - All street fronts and open space shall be provided with activity, visual interest, and variety. Consider appropriate use of:

- Canopies or cantilevered structure at walkways along frontages with high levels of pedestrian traffic to provide weather protection.
- Street furniture for College and Community use. Include at areas to promote activity, and in locations that offer respite to the bustle of busy streets.
- Plantings and trees that create a defined "palette" to reinforce the college "district".
- Use of stormwater detention features for street trees and paved areas.
- Multilevel lighting (General frontage illumination, Pedestrian oriented lighting, Security lighting select building façade/site development lighting).
- Signage that supports definition of the College "district" Examples include banners, flags, and/or gateways/sculptures.
- Building and Site materials that are of a pedestrian scale. Palette of materials that support the definition of the College "district"
- Engage will local artists, arts organizations, and community services to enhance the pedestrian environment thought inclusion art, societal, and other placemaking features.
- Consider design features that visibly represent and promote the diversity of the Capitol Hill community.



1 • Handrail-mounted lighting at exterior staircases



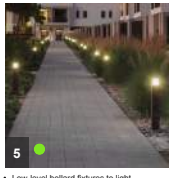
8 • Wash street-level facades with light
• Amplify texture/pattern of facade



14 • Subtle design for student-only entries to/from Nagle



2 • Overhead lighting at exterior stairs



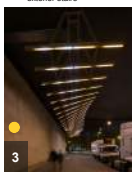
5 • Low-level bollard fixtures to light walkways and delineate paths



9 • Trees/greenery planted in ground



15 • Pavement types suggest different zones
• Street furniture provides seating and planting beds
• Street furniture is not integral/structural to sidewalk - is movable/changeable
• Multiple small/medium scale furniture pieces can have different features



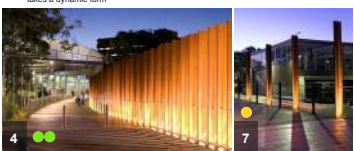
3 • Overhead lighting that takes a dynamic form



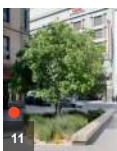
6 • Trees/greenery in boxes



10 • Wayfinding signage on sidewalk to direct students and guests



4 • Uplights and broad surfaces can create implied separation and define zones of use



11 • Use similar plantings, trees, and other landscape elements to reinforce the campus identity/boundary as well as for stormwater treatment/control



12



13

Generally in favor of closing off some of the entrances to the park

Other than the comment to the left, I really think you nailed it with the Streetscape and Open Space comments

Use pollinator plants and native landscaping

Wayfinding should be unique and inspiring so you want to find the next piece and follow it.

Comments from Previous Charrettes:

Student walkways vs. using the sidewalk:
Inclined to support efficient pathways for students (including pedestrian bridges) to move about the campus, even if that takes away from the number of people on the sidewalks.

encourage and support intentional space for street art

Placemaking through art (paving, walls)

Gates/arches to identify space as being a campus

Incorporate greenscape wherever possible within the pedestrian experience - to soften new AND existing hardscape

provide a continuity of experiences with similar features, materials, amenities, art, etc. throughout the Broadway walk frontage with specific goals of tying the Howell open space to the south open space/green space.

use concrete creatively with irregularly scored areas to define walk routes, public gathering spaces - all combined with colored/tinted and textured concrete to be a public space material in place of brick.

opening up cafes and culinary restaurant to seem more public - whether with physical glass/resurfacing or signage

consider defined palette of plants, trees, bushes, landscaping that helps to brand/define the college - use throughout the public openspaces

a plant palette that focuses on pollinating plants and creates a pollinator pathway.

Streetscape and Open Space

Comments - Precedent Images

Image 1:

- Add an arch / transition from campus to park

Image 4:

- like how this looks but would prefer not to have many blank spaces on campus (based on the likelihood of inviting vandalism)

Image 10:

- I support more permanent wayfinding like this for building identification than banners/flags.

Design Standards - Street/Pedestrian Area-Activating College Uses

Street/Pedestrian Area-Activating College Uses - Integrate new structures with existing street patterns to maintain a cohesive streetscape

- Structures with street frontage facing Pike, Pine, Broadway, Howell Street Passage, and Cal Anderson Park shall orient active street-level uses abutting pedestrian areas. The uses should be transparent with visibility into and out of, the structures. Uses should include highly activated functions that bring energy and interest to the streets/walkways. Area-activating uses may include:

- Campus Retail (bookstore, coffee shop, bakery, bistro)
- Food services
- Student lounges
- Gathering spaces
- Meeting spaces (student, college, community)
- Academic Program Exhibition (makerspaces, digital sandboxes, art gallery, etc.)
- Fitness Centers
- Student Centers
- Public Safety Offices
- Performing Arts Venues
- Community Service Centers

- Provide a strong 2 to 4 story street wall along frontages except where structures are pulled back at major building entrances.
- At major building entrances, provide active pedestrian transition areas between the street frontages and building entrances.
- At minor building entrances (student-only entrances) provide site furnishings to encourage student-ownership and presence.



1 • Create visual connections from basement-level gym to Cal Anderson Park



5 • Outdoor student patio
• Private space for students overlooking park



2 • Locate student lounge space against window overlooking park



6 • Interior activity - dance studios - celebrated and visible to the exterior



3 • Outdoor deck for students, provide views overlooking



4 • Collaborative student spaces against windows



7 • Collaborative student spaces against windows



Like the idea of outdoor student spaces overlooking the park

Windows that visually connect the workout spaces in the MAC with Nagle - overall just want more porosity

Comments from Previous Charrettes:

Love the micro-retail opportunities

Student work highlight opportunity - behind glass - like a showcase wall

See public plaza and buildings surrounding El Centro de la Raza - restaurant, indoor spaces for public meetings at ground level right off of the plaza with rolling garage doors to easily open up to the plaza

It'd be nice to see a large glass curtain wall at grade for a mixed use indoor space that could easily be combined with adjacent outdoor space (plaza) to provide a larger public venue for school functions, public use and/or indoor/outdoor food services functions.

provide a rhythm of whimsy in public features along the streetscape - could be art, could be creative landscaping, handicrafting or building features that make you stop and go hrrm

consider alcoves adjacent to street - catering to bikes/bike cafe

closed off sidewalk seating areas should only be used sparingly, as they break up the larger, public space and there's usually a high volume of peds on Broadway

A grand statement at the corner of Howell and Broadway

An opening in the building to expand the building into the public realm (Broadway/Howell/B oth

Street/Pedestrian Area-Activating College Uses

Comments - Precedent Images

Image 5:

- My view is that a private park can become an 'invitation' to the park; as students engage with the park on their own terms, makes them more comfortable with engaging with the park itself
 - +1 for this comment

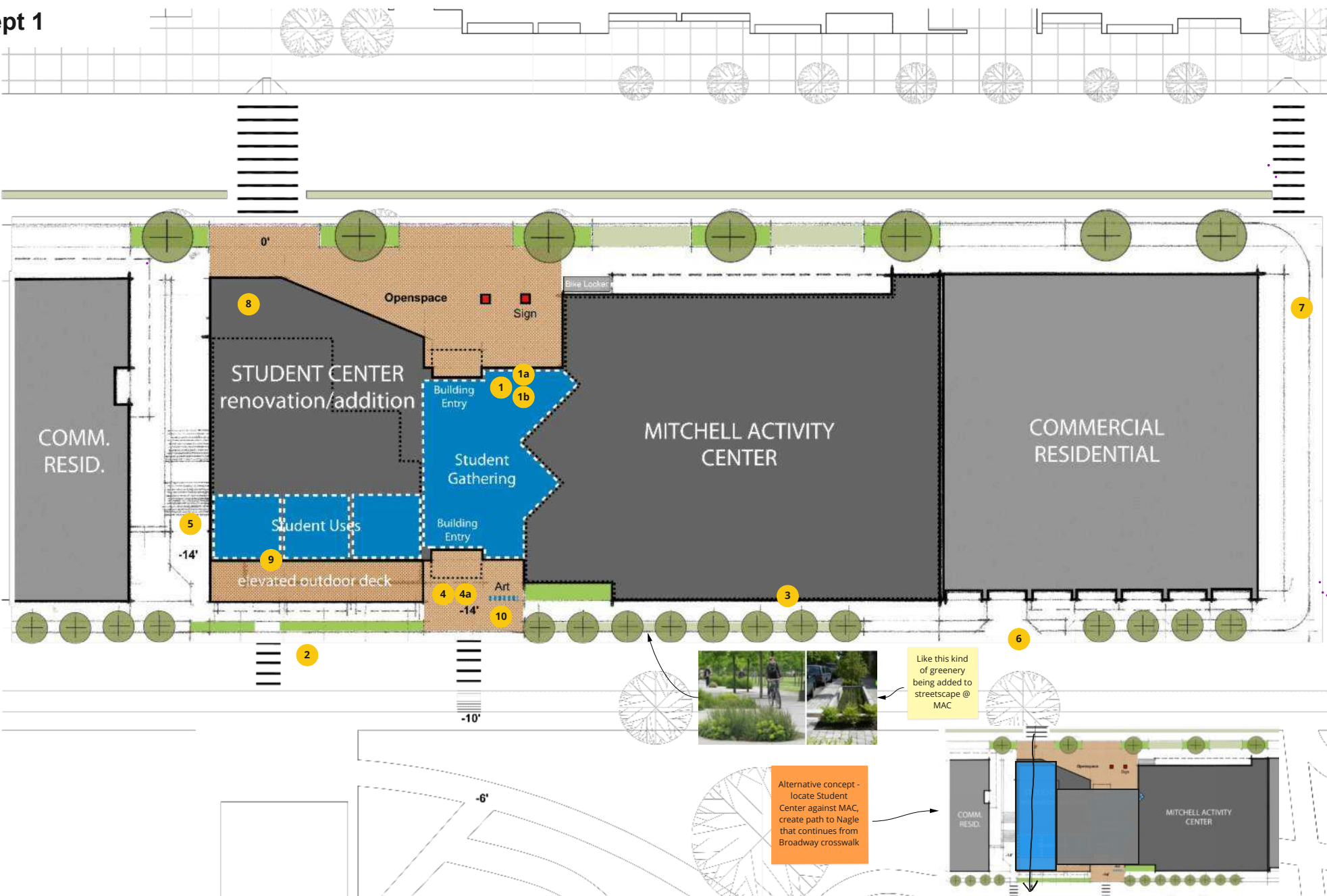
Image 6:

- I like the windows here, but it is overly urban for this area ... something more moderated but in the same vain is great

Image 8:

- Unless this is driven by sustainability, it looks...busy?
- The building interior is beautiful. The exterior façade is terribly ugly.

Concept 1



Like this kind of greenery being added to streetscape @ MAC

Alternative concept - locate Student Center against MAC, create path to Nagle that continues from Broadway crosswalk

Concept 1:

Conceptual Narrative:

- 20,000-30,000SF expansion and renovation of the Student Center - no changes to the MAC. Adding 1 floor the the building to make it 4 floors on the Nagle side, 3 floors on the Broadway side (would match heights of MAC and adjacent apartments).
- Funded via self-imposed student fees
- Propose closing the gap between the Student Center and MAC.
- At the south stairwell, extend the landing at Broadway and put active interior space with transparency to the stairwell.
- At the Nagle Pl level, add windows and porosity to the Student Center.
- At the Broadway Level, add active student uses looking out to the park
- At higher level(s), add outdoor deck(s) looking out over the park.
- Create a new Student Gathering space with entrances facing Broadway and Nagle.

Comments

1. Support closure of this entrance
 - a. Agreed
 - b. Me too
2. may benefit from a different shape to the back facing Nagle to activate the space without a wall to the park.
3. Gym, student lounges, meeting rooms with visibility to park
4. Want this to be a secure access point for students. If the stairwell to the south is kept, then I think this building doesn't need to be publicly accessible.
 - a. What about wheelchair users? Will they be able to use an interior elevator? Or can an ADA ramp be added?
5. Could this be a ramp instead of stairs?
6. I don't think this building has opened yet; how might we get residents' feedback? This will impact them the most
7. Can this become a public ADA space?
8. Would be great to move the Student Center to be against the MAC and have the Student Gathering/transitional space to the park line up with the crosswalk from Broadway Edison (see alt. concept)
9. With so much glass facing the park, any bird-safe design idea being considered? Cap Hill Eco District worked to create bird-friendly design guidelines.
10. Feels like the college has been waiting for Cal Anderson to offer a connection to it. Rather than wait, this seems like a great opportunity for the college to lead the way and make a meaningful connection to the park.

Discussion

- Why aren't any changes proposed for the MAC?
 - The MAC is functioning as needed for the students, so they don't see a need to fix something that is working fine.
 - Would like to see more streetscape improvements at the MAC.
- As part of the design guidelines, create 3 or 4 different types/hierarchies of paths and how those paths should be lit.
- Parks department hosted visioning workshops on park activation. May be good to see results from those exercises and what people want to see in the parks and how they are activated.
- Add comments to MIMP relating to how the city treats the park and how it does(n't) connect to it's surroundings.

SEATTLE CENTRAL COLLEGE
MAJOR INSTITUTIONS MASTER PLAN
DEVELOPMENT ADVISORY COMMITTEE

April 20, 2023

Nathan Torgelson Director
Seattle Department of Construction and Inspections
P.O. Box 34019

Seattle, WA, 98124-4019
Attn: Carly Guillory

Dr. Bradley Lane Interim
President Seattle Colleges
1500 Harvard Avenue
Seattle, WA 98122

RE: Seattle Central College Major Institution Master Plan Development Advisory Committee Comments and Recommendations Concerning the Preliminary Draft Major Institutions Master Plan and Preliminary Draft Environmental Impact Statement for Seattle Central College

Dear Mr. Nathan Torgelson and Dr. Bradley Lane,

In accordance with Seattle Municipal Code 23.69.032.D, the Seattle Central College Major Institutions Master Plan Development Advisory Committee (DAC or committee) submits the following comments on the Seattle Central College Preliminary Draft Major Institution Master Plan (PDMIMP) and the Preliminary Draft Environmental Impact Statement (PDEIS). The DAC examined all required components of the PDMIMP—the development standards component, the development program component and the transportation management program component. The DAC examined the analyses in the PDEIS to assess the potential impact of the MIMP. The DAC looked carefully at what the proposed expansion would look like and how the College’s planned decisions would impact the neighborhood and the range of people who live, work, go to school, or play in Capitol Hill. We believe it is our role to balance the growth of Seattle Central College with the long-term needs of the neighborhood. To that end we offer this comment letter with recommendations for your consideration.

Concerning Historic Preservation, Arts, and Cultural Spaces

There are several sites within the Major Institution Overlay (MIO) boundary that support arts or cultural uses, provide space for the community to gather, and/or are historic in nature, as detailed in Table 1.

Table 1: Historic, Arts, and Cultural Space Assets within the MIO Boundary

Site Name	Historic or Cultural Significance	College’s Plans
Broadway Performance Hall	This 295-seat performance hall is available to rent and hosts a variety of events and performances year-round. It is one of the largest indoor gathering spaces of its kind in the neighborhood available for civic and arts uses and is billed by the College as “Seattle’s Showcase for Local and Regional Artists.” The building is on the National Register of Historic Places and is the only remaining part of Seattle’s first high school – Broadway High. It is also used by the College’s Music Department.	Site of a planned project
Capitol Hill Presbyterian Church	Built to house the Westminster Presbyterian Church in 1923, the church ceased operations in 2018.	Site of a potential project
The South Plaza	The plaza and lawn occasionally serve as a place for the community to exercise free speech and as a central, outdoor gathering space for past protests and social movements including Occupy Seattle in 2011 and the Capitol Hill Organized Protest zone in 2020.	Site of a potential project
The Lenawee	A 78-unit apartment building, mostly consisting of studios and one-bedroom units. Built in 1918, a survey of this historical site found that this property would meet the criteria for both the National Register for Historic Places and the Seattle Landmarks Preservation Ordinance.	No project proposed, but part of MIO expanded boundary
The Fine Arts Building	The building is home to the 450-seat movie theater, the Egyptian, and is used by the College as fine arts academic instruction space. The building was built in 1916 as a Masonic Temple. A survey of the site found that this property would meet the criteria for both the National Register for Historic Places and the Seattle Landmarks Preservation Ordinance.	No project proposed; part of the existing MIO boundary
The Erickson Theater	A "black box" performance space that can hold up to 151 people. It is available for lease for events and is used as a resource by the Seattle arts community and the College’s drama students.	No project proposed; part of the existing MIO boundary

Collectively, these sites contribute to the identity of the neighborhood as a place where people from the region convene to share information and express opinions, watch a film or theater production, or attend a cultural event; where families and roommates can live together in a vibrant community near transit, parks, employment opportunities and academic institutions; and where one can walk down the street and experience the diverse built environment of the Pike/Pine and Capitol Hill urban villages.

Throughout deliberations, several DAC members expressed concerns that, through inclusion within the MIO boundary, the College will eventually demolish the buildings on these sites or repurpose the use/space, displacing tenants and resulting in a net loss of community gathering and performing arts space in Capitol Hill.

The DAC is sympathetic to the College’s need to make decisions that are in the best interests of students but would like to see a stronger commitment from the College to balancing their interests with the needs of the community when repurposing sites that are of historic or cultural significance.

Historic Buildings

The DAC understands that any changes proposed by the College to historic structures will most likely require landmarks review. The DAC previously requested that the College actively support this review process to ensure the review occurs with balanced consideration for college and performing arts space needs. The College appears hesitant to support the landmarking process, expressing an intent to prioritize maximization of the use of all facilities in support of the College’s mission and goals, in response to a prior DAC recommendation. The DAC would like to see the College meet their business needs in a manner that honors the neighborhood’s past.

Recommendation 1
The DMIMP should express the College’s commitment to affirmatively supporting the landmark process and advocating on behalf of the historic places and structures that are 50 years or older and will be affected when a Master Use Permit application impacting them is referred to the City’s Historic Preservation Officer.

Response 1
The PMIMP, See Chapter 4, Section 3 Development Standards – Historic Preservation Review, Policies and Practices, which requires the following, "As part of any project development, where an existing resource is eligible for the Nomination process, the College will work with the City and State to complete a Historic and Cultural Resources Assessment (HRA) that shall be a common reference material for historic preservation implementation."
The College believes the above statement sufficiently expresses commitment to the Landmarks Nomination process. This statement will remain in the DMIMP.

According to the Seattle Department of Construction and Inspection’s Seattle Historical Sites database, the Lenawee was built in 1918, not 1940 as the PDEIS states in the Housing Chapter.

Recommendation 2
The DEIS should accurately report the date the Lenawee was built.

Response 2
The date of the Lenawee construction has been corrected to ‘1918’ in Section 3.6, Housing and will be reflected in the DEIS

Refer to the *Concerning the MIO Boundary/Alternatives and Decentralized Options/Planned Projects/Potential Projects* section of this letter for additional recommendations about the Lenawee.

Indoor Arts Space and Gathering Space

Several DAC members expressed concern about the sustained loss of arts and cultural uses in the community, including in a planned project at Broadway Performance Hall that will result in the loss of the 295-seat gathering space for other College business needs. These members expressed concern that other assets in the MIO boundary would eventually experience a similar fate.

The DAC proposed a creative strategy for the College’s consideration for application if or when a building is sold but, were unable to identify a suitable strategy for a change in use. In response to the one creative strategy proposed, the College wrote that “the need to maximize the use of these venues to support College

mission and goals must be the priority. SCC acknowledges and supports the aspirational goal of maintaining community access to its cultural assets for arts and will commit to efforts to maintain it.” The DAC would like to see a more actionable commitment expressed in the MIMP.

Recommendation 3
The DMIMP should articulate a clear proposal from the College that minimizes or mitigates the loss of performing arts and gathering spaces for public use when the use of these buildings change, the buildings are renovated or demolished, or the buildings are sold. The College can look to the strategy proposed in the DAC’s last comment letter or work with the DAC and the Seattle Office of Arts and Culture to develop an alternative plan that balances the business needs of the College with the needs of the arts and Capitol Hill communities.

Response 3
The college has for many years and will continue to work with Arts organizations to assume occupancy of arts spaces by making spaces available via mutually beneficial lease arrangements.
The College has and is actively seeking Arts organizations to assume full access and responsibility for buildings by making them available via ground lease arrangements.
SCC property is owned by the State of Washington, as such, if the above efforts do not come to fruition, and the college elects to proceed with selling/dispossessing any parcels or buildings, it must be done in compliance with House Bill (HB) 2382. A summary of the bill is as follows:
Disposal of Surplus Property for Public Benefit
Any state or local agency with authority to dispose of surplus property may transfer property to any public, private, or nongovernmental body on any terms agreeable to the parties, including a no-cost transfer, if the property is used for a public benefit. Public benefit means affordable housing development, or related facilities, for households at or below 80 percent of the local adjusted median income. Such a transfer must include a requirement that the property will be used for a designated public benefit, as well as remedies if the property is not used for the designated purpose. Each government entity using the authority to dispose of public property must enact rules.
The authority to dispose of surplus property for public benefit is discretionary and may be used as an alternative to existing authority, but does not apply to state forest lands, common school lands, or other lands subject to legal restrictions. Such authority is expressly added as an alternative to the authority of the WSP and city governments to sell surplus property at fair market value. The disposal of surplus lands for public benefit is deemed a lawful purpose for any state or local agency that keeps accounts on an enterprise fund, and must be consistent with any applicable, local comprehensive plan. The disposal of property for public benefit is expressly exempted from the local government accounting requirement that intra-agency property transfers must be paid for at true and full value.
SCC’s recent disposal of parcels south of the Broadway and Pine intersection are examples of its disposition via HB 2382.

Outdoor Gathering Space

The DAC strongly supports renovations to the South Plaza and lawn to improve its function as a gathering space for students and the community. Towards the end of the DAC’s regular meetings with the College in informing the PDMIMP, the College announced a plan to include a district energy plant project beneath the South Plaza as a potential project. The DAC does not understand how this proposal affects what plaza renovations are likely to happen but supports changes to the plaza if there is no net loss or reduction in the quality, quantity, or access to public space by students and the public and is consistent with the design standards in the adopted MIMP.

Recommendation 4
The DMIMP should reflect that one of the goals for any improvements to the South Plaza and lawn is to improve its function as a gathering space for students and the public for uses ranging from public protests, outdoor learning spaces, student and public lounging, farmers markets and other community events , and commit to no net loss of reduction in the quality or access to public space by students and the public. The

DAC would prefer that there be no net loss in quantity of public space but is willing to accept quantity reductions if they result in significant increases to public space quality.

Response 4

In the PMIMP, see Chapter 4, Section 4 Design Guidelines – Open Space Design Guidelines and Green Space Design Guidelines, the guidelines define goals to be used for design of improvements for the South Plaza and lawn area. The College believes the above statement sufficiently expresses the goals for improvements. This Design Guideline will remain in the DMIMP.

The MIO Boundary

The DAC acknowledges the College’s need to plan for future expansion in the Capitol Hill neighborhood but has reservations that any future effort to acquire the Lenawee building (1629 Harvard Avenue) would be beneficial to the community or College. The DAC sees the Lenawee as an asset for the neighborhood because of the housing stock it provides as well as its architectural interest.

Should the College acquire the Lenawee, the College has committed to considering the highest and best use of the building for the College’s needs, including its reuse as student housing or other university functions such as administrative or classroom space. Per the DAC’s request for clarification, any mitigation for the loss of housing will be addressed by the College via the requirements of Seattle Municipal Code Chapter 23.69, according to the PDMIMP. Additionally, the College noted its recent parcel transactions substantively increased the availability of housing in the Capitol Hill neighborhood, although it readily admits that this additional housing cannot be considered comparable replacement housing for residential sites within the Boylston Expansion Area in which no change of use or demolition is currently proposed in the PDMIMP.

Seattle Municipal Code Section 23.34.124 states that the City “does not permit new or expanded MIO boundaries where they would result in the demolition of structures with residential uses or change of uses of those structures to non-residential major institution uses unless comparable replacement is proposed.” The College is not proposing comparable replacement housing at this time because they are not proposing a planned project at that site in the MIMP. However, the College states in the PDEIS that its intention is to consider a change of use from residential to office or academic space at a future date. It is the DAC’s opinion that, by virtue of allowing the inclusion of residential structures in the expanded MIO boundary, the Seattle Municipal Code requires comparable replacement housing if and when demolition or a change of use occurs.

Recommendation 5

The DMIMP should include an explicit statement about the College’s intent to propose comparable replacement housing for the residential buildings in the Boylston Expansion Area if any of those buildings are proposed for demolition or a change of use to non-residential major institution use.

Response 5

Under this proposed MIMP, the College does not intend any demolition or change-of-use of any residential buildings in the Boylston Expansion Area. Any changes to buildings in the Boylston Expansion area would require SCC to propose a new MIMP or MIMP Amendment. The college would be required to comply with replacement housing regulations included in the Seattle Municipal Code in effect at that time. No explicit statement at this time is needed.

Additional information about the impact of the proposed MIMP on housing, where available, would be helpful in assessing the potential impact on lower-income households or larger households if a residential building is converted to non-residential use.

Recommendation 6

The DEIS should include information, where available, to assess whether residential housing units included in the expanded MIMP boundary may be rented at rates affordable to low- or moderate- income households. Rent information may not be available, so other information may be used as a proxy to assess impact. In addition, the DEIS should speak about the net increase in housing units.

Response 6

Section 3.6-2, Housing, of the PDEIS identifies the net increase in housing that would occur under the Draft MIMP – an increase of 506 units as compared to zero units under existing conditions. Residential units in the expanded MIMP boundary are not proposed to be impacted by planned or potential projects identified in the DMIMP; therefore, rental information is not necessary to include.

With the proposed expanded MIO boundaries, the College would add existing housing units to the MIO and create new housing units with the proposed student housing project. By providing students with new housing options on campus, the proposed student housing project could decrease demand for more affordable units in the neighborhood and increase housing choice for other lower-income households, something the DAC strongly supports. The net new housing gain should be quantified and made clear in the DEIS Housing Chapter. At present, the PDEIS simply states the MIO would go from 0 to 506 housing units but does not state how many of those units are new versus existing. The PDEIS Housing Chapter provides varying unit numbers (e.g., Section 3.6-4 says 508 units and other sections say 506).

Recommendation 7

The DEIS should quantify and express the housing unit increase in terms of existing and new residential units to help assess the impact on the overall neighborhood’s housing stock. The total housing unit count should be verified

Response 7

See Section 3.6-2, Housing of the PDEIS, which includes the following information: ‘the housing stock in the neighborhood would increase by 2.7 percent.’

The DAC previously suggested that the College include the three parcels west of Harvard Avenue and south of the Presbyterian Church within the MIO boundary, as the buildings there are unlikely to be designated as landmarks and development of these parcels would result in less residential displacement than other proposed parcel redevelopments. The College replied that these three parcels are too shallow for redevelopment, especially after considering required setbacks, and would serve little to no use for the College’s instructional space needs. The DAC proposed that these structures could be purchased and renovated as student housing or redeveloped for potential future administrative space needs. The DAC understands that it is unlikely that the College would acquire these parcels but feels that it would be shortsighted not to include them.

Recommendation 8

The DMIMP should include the three parcels west of Harvard Avenue and south of the Presbyterian Church within the MIO boundary.

Response 8

As discussed previously with the DA and as documents in the PDMIMP indicate, see Appendix B Response to CAC Comments, Response 7, “the three parcels in question are of relatively small dimension (±60’ deep) which is not conducive to college needs of larger developments. As there are currently no college needs that are aligned with these parcels’ development characteristics, the College is not including them in the proposed MIMP Boundary at this time.”

Concerning Campus Security Guidelines

The DAC held several discussions about how to best implement design guidelines that would improve perceptions of personal safety and reduce feelings of risk for persons on or transiting through campus. While largely in alignment over the technical aspects of this, the committee found itself divided over the matter of territoriality. The majority opinion holds that the College must define campus grounds in a manner that both allows students and employees to feel safe and proud of their environment while also being entirely welcoming to all persons who wish to respectfully occupy or transit campus open spaces.

A minority opinion holds that SCC students and employees would feel safer if they were to possess exclusive ownership of campus grounds. From this perspective, visitors and passersby would still be welcome but open spaces would not be planned with them in mind and their needs would be treated as secondary.

The majority opinion disagrees on the grounds that the College, a public entity, should not represent a space of exclusivity and that increasing perceptions of personal safety can be balanced with full community access. By implementing design guidelines that seek to illuminate the campus, reduce hiding spaces and poor lines of sight, and convey a sense of order and security, the College can foster an environment that is safe and welcoming.

Recommendation 9

The DMIMP should reflect the College’s commitment to equally prioritizing college users and the community at large when planning for and designing the campus’s open spaces.

Response 9

See Chapter 4 – Open Space Design Guidelines. Also see PDMIMP Appendix B, Response to CAC Comments, Response 52, it is the college's duty and responsibility to prioritize students, faculty, and staff in facility planning. The College is committed to, and will work with community members, through the MIMP process, the DAC, and the IAC, as plans for campus open spaces develop.

Recommendation 10

The DMIMP should use consistent language throughout the lighting section in Chapter 4 that clearly states the level of commitment. For example, the second bullet point of the lighting section in chapter 4 page 1-21 of the PDEIS should include a “will”, “should” or “consider” commitment, consistent with all other bullet points in the section.

Response 10

The DMIMP will amend the specific design guideline referenced above as well as all other applicable design guideline bullet points to include the key terms – will, should, consider, or standard.

Unfortunately, the DAC was disappointed to find that the College had sidestepped our request to evaluate Crime Prevention Through Environmental Design (CPTED) principles on the merits of being inclusive, non-discriminatory, and equitable. CPTED principles can reinforce class hierarchies and systemic racism and disproportionately target Black, Indigenous and People of Color (BIPOC) and lower- income individuals.

Furthermore, the PDMIMP bears little mention to the College’s commitment to promoting social justice and equity while combatting discrimination, systemic racism, and classism within the built environment. The DAC urged the College to enshrine these tenets within the MIMP.

Recommendation 11

The DMIMP should reflect the College’s unquestionable support for making the campus a welcoming place for all persons, especially populations who experience higher levels of discrimination and surveillance such as the BIPOC and LGBTQIA+ communities as well as lower-income and unhoused persons. This support should be enshrined in language affirming of the College’s values to promoting equity and social justice.

Response 11

The College’s commitment and support to serve its highly diverse and underserved population is well documented throughout all aspects of its daily work. The College’s public commitment is also demonstrated via Resolutions and Statements the College has publicly made. See the college’s website - [Where We Stand | Seattle Colleges](#) for more information. In regard to its commitments and their inclusion in the MIMP process and documentation, see the PDMIMP [Chapter 2 – Mission & Guiding Principles](#) which states: *This document provides a set of guiding principles that clearly articulate the values and needs of the Seattle Central College (SCC) campus community with respect to campus planning. All components of the SCC Master Plan will support the accomplishment of the college's mission, values, strategic initiatives, and other guiding principles.* Also see the full text of this Chapter where commitments and support are noted throughout its Mission, Vision, Values and Strategic Plan.

Recommendation 12

The DMIMP should articulate how the College has evaluated CPTED principles to eliminate discriminatory practices. Furthermore, the College should explicitly indicate which CPTED principles are excluded in the design guidelines.

Response 12

The DMIMP will remove references to CPTED principles.

The college campus, in particular the South Plaza and its landscaped open area, has a long history of use as a civic gathering space and as a hub of community activity. The committee sees no reason that this should change and urges the College to promote community use of campus open spaces when redeveloping the South Plaza

Recommendation 13

The DMIMP should express the College’s commitment to holding usability of the South Plaza for civic and community uses as a central tenet when planning for and designing the plaza and its surrounding landscaped areas.

Response 13

See Chapter 4 – Open Space Design Guidelines of the PDMIMP, which states, “Preservation of public access and use is essential and **will** be maintained.” The College agrees that public usability and accessibility of outdoor campus open spaces, especially the South Plaza, will be maintained as a community asset.

Concerning Parking and Transportation Provisions

The DAC read the PDEIS’s Transportation Discipline Report and agrees with the recommendations made therein. The DAC also believes that these recommendations, voiced by experts in the industry of this section, should be firmly embraced and considered by the College.

Recommendation 14

The DMIMP should express the College’s commitment to abiding by all of the mitigation measures proposed in the PDEIS’s Transportation Discipline Report, prepared by Transpo Group. Furthermore, the DMIMP should include the program by which each mitigatory proposal will be implemented and monitored.

Response 14

The College is required to comply with mitigation measures, which will be conditioned by SDCI and SDOT as part of the approval. Annual monitoring will be required by SDOT to demonstrate that the College is meeting the goals of the transportation management plan (TMP).

Pedestrian Street Crossings

An issue repeatedly identified by the DAC in both the College’s PDMIMP presentations and design charettes is that current street crossings within the MIO boundary will not adequately meet needs for the growing Capitol Hill neighborhood. The student population is anticipated to increase by over 1,000 FTE units and with it, the number of College employees is expected to grow as well. The new Student Housing project is expected to increase the number of residential units within the MIO boundary from 70 to 500; this resident population will place an especial strain on Harvard Avenue crossings.

In response, the College committed to pursuing street crossing improvements and traffic calming measures along Harvard Avenue, East Pine Street, Howell Street, and Broadway. The College is also committed to improving street crossing access to Cal Anderson Park via Howell Street near the north end of campus. The committee appreciates these commitments to making the pedestrian experience safer and more enjoyable. However, the PDMIMP fails to address pedestrian street crossing improvements for Nagle Place.

The DAC believes this to be a simple overlook but reiterates the importance of Nagle Place to the campus. Nagle Place serves as both the principal barrier and primary connection to Cal Anderson Park from campus. Discussions between committee members and with students yielded that the typical pedestrian experience crossing Nagle Place is unpleasant due to its deprioritization of pedestrians. Service vehicles and residential traffic speed through nearly unhindered.

The Nagle Place crossing is not well designed for pedestrians but, if improved, could serve as a central axis connecting the Broadway campus block and the combined Student Union with Cal Anderson Park. Improving this connection would also encourage participation in activities at Cal Anderson and improve perceptions of safety in and around the park.

Recommendation 15

The DMIMP should express the College’s firm commitment to implementing pedestrian street crossing and streetscape improvements on Nagle Place between the new combined Student Union building and Cal Anderson Park. This should be modified in, but not exclusively to, the *Pedestrian Street Crossings* section in Chapter 4, page 1-18 of the PDMIMP.

Response 15

The "Community Connectivity - Planned" diagram in Chapter 5, pg. 1-3 of the PDMIMP shows Pedestrian and Streetscape enhancements. This diagram shows a proposed crossing of Nagle at the Student Union. A bullet

identifying proposed improvements to the Nagle Crossing at the Student Union will be added to the *Pedestrian Street Crossings* subsection and the *Pedestrian Connections and Access to Surrounding Amenities and Services* subsection. The College will advocate for improvements to the street crossing at Nagle Place and work with SDOT to implement acceptable improvements; however, the College does not have control over final design or implementation. Final design and implementation fall within SDOT’s jurisdiction. [As of May 11, 2023](#), Seattle Parks Superintendent announced to Parks Board Commissioners that SDOT has agreed to partner with the Parks Department on a study that will investigate restricting vehicle access on Nagle Place adjacent to Cal Anderson Park.

The PDEIS indicates the College’s intent to provide pedestrian safety mitigatory measures at the intersection of East Pine Street and Boylston Avenue. The DAC appreciates this commitment given that this is a high-volume pedestrian corridor that will only gain more volume through the lifetime of the MIMP. This intersection will especially need safety measures when the new parking garage is rebuilt as the primary entrance is currently planned to be located on Boylston Ave near the greenhouse structure. Said location will significantly increase the interaction between pedestrians and vehicles and in turn, likely increase the risk of collision.

Recommendation 16

The DEIS should expand the pedestrian safety mitigatory measures that are currently afforded to East Pine St and Boylston Avenue in PDEIS section 3.11-28 to also include E Pine Street and Harvard Avenue.

Response 16

The College will continue to work with SDOT and SDCI on the required mitigation measures for East Pine Street and Boylston Avenue intersection through the MIMP approval process, for Mitigation will become a condition of approval for the MIMP.

Street Network and Traffic

In the initial PDMIMP comment process, the DAC discussed writing recommendations asking for the College to partner with Seattle Department of Transportation (SDOT) and other relevant stakeholders on improving the street network adjacent to and surrounding campus to increase pedestrian and bicycle safety. Those recommendations were tabled until the time when the DAC could read and analyze the PDEIS to determine the level of commitment needed by the College. Having read the PDEIS, the DAC now makes the following recommendations:

Recommendation 17

The DMIMP and DEIS should reflect the College’s intent to partner with Seattle Department of Transportation and other relevant stakeholders on the reduction of pedestrian and bicycle collisions at the intersections of E Pike and Broadway and E Pine and Broadway, both of which are currently considered High Collision Locations.

Response 17

See Chapter 4, *Pedestrian Circulation - Pedestrian Street Crossing*. As stated in the PDMIMP on page 1-18, the College has committed to working with City of Seattle jurisdictions to support the development of traffic calming and pedestrian crossings consistent with a pedestrian-friendly environment at all crossings. The College will extend this language to included bicycles in the DMIMP

Recommendation 18

The DMIMP and DEIS should reflect the College’s intent to partner with Seattle Department of Transportation and other relevant stakeholders to improve the Level of Service at Boylston Avenue and East Pine Street from level F (poorly performing) to at least level D (better than poorly performing).

Response 18

The College will continue to work with SDOT and SDCl on the required mitigation measures, through the MIMP approval process, for the Boylston Avenue/E Pine Street intersection. Any required mitigation will become a condition of approval for the MIMP.

For the DAC’s information on SDOT Standards - The City does not have a LOS standard that prioritizes pedestrian, bicycle, and transit over personal vehicular modes. The LOS F operations are impacting personal vehicular traffic on the side-streets. The college can work with SDOT on improvements but achieving LOS D is unlikely. Also, the analysis is based on the peak hours during commute time and other periods of the day operations will perform better.

Recommendation 19

The DMIMP and DEIS should commit to not converting parking lanes on Boylston Avenue, or any other street within the MIO boundary, into vehicular travel lanes. This would remove a protective barrier shielding pedestrians on the sidewalk from moving vehicles; if parking lanes are removed, they should be reused for open space or the expansion of street front commerce into the public realm.

Response 19

The College acknowledges the DEIS notes that the existing vehicle parking on the west side of Boylston Ave could be removed to provide additional south-bound travel lanes to improve the Level of Service (LOS) when the entrance/exit to the Student Housing garage moves to Boylston Ave. The College does not seek to formally request or implement this change in the street layout as part of this MIMP.

Recommendation 20

The DMIMP should reflect the College’s intent to partner with Seattle Department of Transportation (and other relevant stakeholders on establishing more bike lanes, first, and sharrows secondly.)

Response 20

See Chapter 4 – Transportation. As stated on page 1-23 of the PDMIMP, SCC will work with City of Seattle jurisdictions to support the development of protected bicycle lanes. The majority of the College is situated on Broadway, which already features a protected 2-way bicycle lane.

The TMP and PDEIS did not have substantial data about the role of rideshare programs as a modal choice in and around the MIO boundary, to the DAC’s surprise. While the DAC acknowledges that rideshare can be a convenient supplement to traditional transit networks, at least for individual commuters, recent studies suggest that rideshare programs contribute to congestion and average trip duration and can harm transit ridership. Thus, the DAC cautions the College against relying on rideshare within the TMP framework.

Recommendation 21

The DMIMP should express the College’s commitment to periodically reevaluating and adjusting its rideshare programs in the Transportation Management Plan; treating traditional rideshare services as the equivalent of single occupancy vehicle trips and therefore something to be reduced. The frequency of evaluation should be listed in the TMP that is included within the DEIS or DMIMP.

Response 21

The approved TMP requires that the College submit annual reports to the City of Seattle on its progress in complying with the city approved TMP. The report includes efforts undertaken to enhance programs including progress toward reducing vehicle trips.

Parking Garage

The existing campus parking garage is aesthetically displeasing, inconsistent with the architectural character in the Pike/Pine neighborhood, and disruptive to the flow of surrounding building heights and massing. The site lacks space for beneficial community uses and is principally transited across instead of serving as a pedestrian

destination, especially on the north, south and west ends. At night, the site becomes a dead zone where concerns for personal safety increase.

Despite the obvious detriments to the community, the DAC understands that maintaining some amount of parking is essential for improving equity. Many SCC students and employees cannot afford to live near to the campus and many of those afar lack reliable and convenient access to transit options.

Fortunately, the College is planning to redevelop the site into a new Student Housing project atop a parking garage. The new building will feature retail/commercial frontages and garage structure screening consistent with Pike/Pine Conservation Overlay District design guidelines. This project will also reduce the amount of on-campus parking spaces, which the DAC supports given emerging remote work and study capabilities, and the relatively recent arrival of the Seattle Streetcar and Link Light Rail after the garage was built. The DAC applauds the decision to redevelop this site and eagerly awaits the hundreds of housing units and increased commercial activity that this project will bring.

The College has not committed to or even yet studied what shape this garage will take. Thus, the DAC would like to remind the College that they have a unique opportunity to improve this site and that the benefits and consequences of the decisions they make in redeveloping this site will affect the community for generations.

The DAC recommends that the College study all available options in constructing a financially viable below-grade parking garage to maximize street-level use, reduce the need for screening, and add eyes to the street. If an at-grade parking facility is ultimately proposed, the DAC recommends that the building design maximize opportunities to activate the street frontage along East Pine Street and avoid large blank walls along this street frontage. For additional information and recommendations on this topic, see the design guidelines section of this letter.

Furthermore, to assist the DAC in evaluating the DEIS, it would be helpful to know approximately how much of the at-grade space along East Pine Street and at the Pine/Harvard and Pine/Boylston intersections would be retail. The DEIS states the amount of retail in terms of floor area ratio but does not describe the retail location.

Recommendation 22

The DMIMP should reflect the College’s commitment to studying a below-grade parking garage option as part of the new Student Housing project to minimize negative visual impacts on the Capitol Hill urban fabric and increase the available at and above-grade space for residential, commercial, and open space uses.

Recommendation 23

The D MIMP should express the College’s commitment to studying how the new Student Housing project’s parking garage could be designed with the potential for adaptive reuse in mind.

Recommendation 24

The DEIS should specify the location of the retail in the Student Housing building.

Response 22

In early studies, SCC explored fully below-grade parking. It was determined that the cost of below-grade parking made the development of Student Housing not financially viable as it would require rents that were too high for SCC Students. See Chapter 4 – Student Housing. As stated on page 1-16 of the PDMIMP “the Pine Street façade should be highly transparent in nature with Street Activating Uses and be a prominent feature of the building design.” Also stated is the goal of incorporating micro/flexible retail opportunities along the Pine Street frontage. Additionally, see Chapter 4 – Community Services and Retail Uses, page 1-20, for notes on activating building facades that face Pike, Pine, or Broadway.

Response 23

Adaptive re-use of the existing garage structure was investigated by the college’s development partner. Cost to preserve and re-use was found to be economically not viable and would result in rents that were too high for SCC Students. Issues noted were the existing structural bay spacing, seismic improvements needed, and increased project cost (construction and total project)

Response 24

The PDMIMP notes the location of retail. See Chapter 4 – Community Services and Retail Uses, page 1-20, for notes on activating building facades that face Pike, Pine, or Broadway.

The location of retail within the building is not a relevant element of an EIS.

The DAC appreciates the College’s consideration of student and staff affordability when setting parking rates. However, most DAC members do not believe that the College should set parking garage rates below market rate. Doing so, in the opinion of the majority, represents a tacit subsidization of single occupancy vehicles driving trips which is a significant contributor to reducing pedestrian safety from traffic, decreased walkability, increased traffic congestion and the worsening climate catastrophe. This majority does not believe that increasing affordability of single occupancy vehicle modal share outweighs the detriments it causes.

This recommendation does not represent a consensus position for the DAC. Two DAC members voted against this recommendation, stating the following:

- The DAC should not provide recommendations related to parking rate policy; on this topic, we believe that the College should determine what is best for its students and staff.
- The DAC should not request that the College retire its current method for setting garage parking rates which seeks to balance student/staff affordability with maximizing revenue from public use.
- We support the College’s efforts to improve student/staff affordability, given the financial challenges students and staff may face.
- Additionally, we support maximizing revenue from public use of parking as an appropriate way for the College to manage its operations.

Recommendation 25

The DMIMP should describe how the College intends to set on-campus parking rates. The College should retire its current method for setting garage parking rates which seeks to balance student/staff affordability with maximizing revenue from public use. Instead, the College should conduct periodic market rate studies to set parking garage rates. Rates set any lower than this would constitute a subsidy and a tacit incentive for single occupancy vehicle mode usage.

Response 25

See the proposed TMP for more detailed discussion of parking policies and practices.

A Major Institutional Master Plan is not an appropriate regulatory document for addressing how parking rates are to be established and will not be included. The college regularly evaluates parking rates to strike an appropriate balance of availability and affordability to ensure equitable use by its students, faculty, and staff.

As of April 2023, the Harvard garage charges \$15/day for public parking. Based on a cursory review of parking rates in neighborhood parking lots and garages, the Harvard Garage rate represents a median price point for daily parking rates. The College does not offer a per-hour rate, in contrast to most neighborhood surface lots and garages. By not offering a per-hour rate, the College is discouraging short-term parking. Parking rates for employee and student passes are available on the SCC website.

Recommendation 26

The DMIMP should describe how the College intends to fund subsidized ORCA transit passes for students and employees when the existing parking garage – the current funding source for the passes– is demolished.

Response 26

The college will fund ORCA transit passes and other TMP subsidies from the Auxiliary Services Fund (does not include tuition funds).

As stated in the PDMIMP, SCC has no intention of demolishing the entire Harvard Garage. The number of parking spaces available will be reduced by approximately 50% with the Student Housing Project.

Incentivizing Bicycle Modal Share

Throughout the MIMP process, the DAC strongly supported bicycling as a significant tool in the College’s commitment to reducing single occupancy vehicle modal share participation and meeting the objectives in their Transportation Management Plan (TMP). In the DAC’s [Preliminary Draft Master Plan Comments](#) letter published on March 2, 2021, the committee requested that the College undertake several measures to encourage and incentivize bicycling to campus, including bicycle multimodal access via bus, Seattle Streetcar and Link Light Rail. These proposed measures included support for both personal bicycles as well as bikeshare programs.

The College largely agreed with our recommendations and, in response to some, but not all, of our requests, enshrined their commitment within the PDMIMP. However, several of the College’s responses to the DAC’s letter fell short of the committee’s expectations.

First, the DAC requested that the College conduct a survey of all bicycle parking and storage facilities on campus, to which the College agreed. However, the PDMIMP makes no mention of how or when this survey will be conducted.

Recommendation 27

The DMIMP should address when and how the College will conduct a bicycle parking and storage survey.

Response 27

A survey of existing bicycle storage facilities available on campus and on adjacent streets has been completed. The Bike Master Plan will be incorporated into the DMIMP Document. It depicts existing and planned bicycle parking and storage facilities.

Second, the DAC requested that the College explore methods for incentivizing and encouraging students and employees to bicycle to campus by providing amenities that support bicyclists. The College responded by agreeing to consult with college users on what bicycling facilities would be desired if demand exceeded current capacity. The PDMIMP does not detail whether existing bicycle facilities meet demand or user expectations, or if a study will be conducted to determine this.

Recommendation 28

The DMIMP should affirm the College’s commitment to providing amenities and other incentives that would convert single occupancy vehicle modal share trips into bicycling modal share trips. Additionally, the DMIMP should address any findings from the student and staff cycling survey recommended by the DAC.

Response 28

The Transportation Management Plan is developed for the purpose of shifting modal share trips away from single occupant vehicles. The College commitment is clearly demonstrated by the initiatives included in the updated TMP included in this MIMP.

However, at a later DAC meeting on January 30, 2023, the College presented a preliminary bike master plan for possible inclusion in the MIMP. The DAC expressed overall support for the plan, including the amount and diversity of bike parking options proposed.

Recommendation 29

The bike master plan should include the following improvements:

- increased signage around entrances so cyclists can find bike parking.
- a continued focus on adding secured bike storage options given the perception that security is a significant barrier to cycling to campus.
- partnership with Sound Transit on the siting of secured, publicly accessible bike parking, such as storage lockers, near the light rail entrance.
- consideration for covered bike parking that keeps bikes relatively dry.
- provision of video surveillance of secure access long-term bike storage.
- sufficient e-bike charging stations in secured parking locations; and

Additionally, the College should gather information from students and staff on whether the plan sufficiently addresses barriers to cycling to campus and report those results in the DMIMP.

Recommendation 30

The DEIS should articulate the College’s approach towards the following long-term bike storage concerns:

- Who will be eligible for long-term bike storage?
- What cost structure will be levied on long-term bike storage users?
- What security commitments will be made to facilities and amenities to increase user confidence in bicycles as a secure modal choice?
- How will priority access be determined in the event that long-term bike storage demands exceeds availability?

Recommendation 31

The PDMIMP should reflect that the College, if or when seeking to install new short term bicycle storage, commits to treating compactness and aesthetics as highly valued qualities of replacement facilities.

Response 29

SCC is committed to improving bicycle facilities and infrastructure to make it easier for students, faculty, and staff to bike to/from campus and improve resources available to the public. See the [Bicycle Facilities section](#) and [Transportation Design Guidelines in Chapter 4 Section 4](#) in the DMIMP for more information. The College agrees that presenting the plan to students, faculty, and staff is necessary to determine if it meets their needs. The College will collect feedback from presentations and make changes that are feasible.

Response 30

While the College agrees that the above concerns should be considered as it implements the bicycle storage components of the Bicycle Master Plan, the detailed elements of how it will be operated are outside the scope of a MIMP. These issues will be addressed as the plan is actually implemented and in conjunction with the TMP and its annual reports.

Priority of access to bike storage facilities on campus will be college faculty, staff, and students who commute to campus.

Response 31

This will be addressed in the DMIMP.

Additionally, a bicycle repair workshop currently exists within the Mitchell Activity Center, which is slated for renovation, thus calling into question the continuity of the workshop as a resource to cyclists visiting campus. The College’s bike master plan, as presented by the College at the January 30, 2023, DAC meeting, details an impressive list of proposed bicycle facilities and amenities, yet it is missing any mention of a bicycle repair workshop within the future bicycle framework.

Recommendation 32

The DMIMP should express the College’s commitment to providing a bicycle repair / maintenance workshop, either through continuity of the existing Mitchell Activity Center amenity or through new means.

Response 32

The Bike Master Plan, which will be included in the DMIMP, will include repair/maintenance areas of Bike Storage Room locations in the Planned Student Housing and ITEC projects. While there are no current plans to renovate the Mitchell Activity Center, it is important to note that the MAC is funded and run by the SCC Associated Student Government. The College is reluctant to make commitments to which may limit its student’s ability to maximize the uses of this valuable student resource.

Third, the PDMIMP lacks language supporting the community use of College bicycle parking and storage. The DAC understands that the College is tasked with, first and foremost, meeting the needs of its students and employees but would like to remind the College that it is one of the largest and most influential stewards of the public realm in Capitol Hill.

Recommendation 33

The DMIMP should reflect the College’s commitment to providing bike parking and storage for the College and the broader Capitol Hill community. The DMIMP, should also specify how the College plans to support multimodal bicycle access to campus via the Link Light Rail and Seattle Streetcar using bicycle storage and parking facilities on the north end of campus.

Response 33

The Bicycle Master plan will be added to the DMIMP and will represent the College’s commitment. The plan includes new facilities at the north end of campus as part of the ITEC center project. The College is committed to providing bicycle parking for students, staff, and community needs. Currently, there are ±98 parking spaces for bikes available for SCC student/staff and community use on campus. Based on a survey conducted in January 2023, there are bike racks with space for 32 bikes at the West Portal entrance to the Capitol Hill Link Light Station. Additionally, the College will endeavor to maintain accurate information on bike/scooter-share discounts available to students and faculty/staff, including [discounted fares](#). See the Bicycle Master Plan for more info.

Fourth, emerging micro mobilities, especially bikeshare, represent an opportunity to reduce single occupancy vehicle modal share and increase access to the campus for students and employees that live nearby without convenient or reliable access to transit.

In Recommendations 29g. & 29h. of the DAC’s [Preliminary Draft Master Plan Comments](#) letter published on March 2nd, 2021, the DAC requested that the College work with the community to designate bikeshare parking zones and to commit to not prohibiting or discouraging the use of bikeshare on campus. The committee appreciates the College’s previous attempts at encouraging and incentivizing bikeshare programs but expresses concern that the College did not commit to not prohibiting bikeshare use on campus.

Recommendation 34

The DMIMP should include language precluding the College from prohibiting or discouraging bikeshare or bikeshare parking in the MIO boundary at any point without consultation of the Implementation Advisory Committee (IAC).

Recommendation 35

The DMIMP should express the College’s intent to proactively partner with Seattle Department of Transportation to determine if designated bikeshare parking areas are needed.

Response 34

The College has no plans to prohibit or discourage bike/scooter-share use or parking on its properties or within the MIO boundary. In the past bike/scooter-share programs have not always been operated in a responsible manner on campus grounds. As such, the college is unwilling to use the MIMP as a means of regulating on-campus bikeshare, or bikeshare parking in the future.

Response 35

There is an existing designated scooter/bikeshare parking area in the ROW on Broadway next to the mid-block crosswalk between BEC and the Bookstore/MAC. The College is willing to partner with SDOT and will invite their review/comment of scooter/bikeshare facilities as the MIMP Process continues.

Concerning Internal Circulation and Open Space

The DAC understands that the College will make circulation and public space improvements throughout the campus in a piecemeal approach as funds become available for different projects. However, the DAC strongly encourages the College and the future IAC to ensure these improvements happen in a manner that results in coherent and cohesive pedestrian zones throughout campus, not in a disjointed and uneven fashion.

For this reason, the MIMP needs to articulate an overall vision for certain public spaces and pedestrian zones to ensure the improvements consistently connect with one another over time. For example, the PDMIMP currently commits the College to improving the space around the entrance of each project proposed along the Howell Street Passage but lacks an overall commitment to making the Howell Street Passage a thriving pedestrian zone.

Recommendation 36

The DMIMP should articulate a vision, and commitment to achieving that vision, for key public open spaces and pedestrian zones throughout campus. This articulation should ensure that the vision is achieved over time, in measurable achievements, as the College makes improvements through the implementation of their various proposed and planned projects.

Response 36

The vision and commitment to quality public open space is included throughout the entirety of the PDMIMP document. It includes scope commitments to proposed and planned projects. It also identifies “Aspirational” scope in public open spaces if funding is available. The needs of the College and community will undoubtedly evolve over time and making any further commitments at this time may not fit college or community needs in the future.

One unmet community need that the College is poised to provide for is a publicly accessible bathroom facility for use by community members, including the unhoused. The College can provide the space but needs partners to fund and safely operate such a facility. The College has expressed openness to helping meet this need but their efforts to seek funding and partnerships have not been fruitful thus far.

Recommendation 37

The City of Seattle should actively partner with the College and other community organizations to plan for and address the need for publicly accessible bathrooms in Capitol Hill through the provision of technical and financial support and to express this commitment in time for the College to include it in the final MIMP.

Response 37

The College has for several years sought to partner with the City and other community organizations to plan for public restroom facilities. The College is committed to supporting a solution. To date, there has been no viable funding made available to construct and operate public restroom facilities. If, and when, the city or other community organization is willing to engage, the College will participate.

The DAC understands that the College has no planned projects south of Pine Street. As a result, there is no state funding for alleyway improvements and the College has not yet identified any funding mechanism for them. However, the DAC believes that the College should commit to and prioritize exploring partnership opportunities to maintain, steward and improve the alleyways south of East Pine Street.

Recommendation 38

The DMIMP should reflect the College’s commitment to actively steward and promote alleyway improvements behind the College properties south of East Pine Street with community partners and adjacent property owners.

Response 38

As stated in PDMIMP, Appendix B [Response to CAC Comments](#), Response 35, the College has been and will continue to work with neighbors and jurisdictional agencies to maintain clean, accessible, and safe alleyways adjacent to its buildings and parcels.

After constructive discussion and having voted on the matter, the DAC aligned on a tentatively supportive position for the construction of skybridges on the SCC campus to improve the experience and accessibility of SCC students and staff. It should be noted that this was one of the most contentious matters deliberated by the DAC and that a sizeable minority of the committee initially opposed skybridges on campus.

The DAC wrote a recommendation to the College voicing this support for skybridges on campus if certain precautions, such as siting a skybridge at least three floors above Broadway, were met to minimize social and economic damages to the open space and streetscapes on campus. The committee feared that skybridges between campus buildings would drastically reduce the number of students and staff congregating and transiting through open spaces—especially the Howell Street Passage. This could potentially cause the space beneath the skybridge to become derelict, reducing both perceptions of personal safety as well as economic viability of at-grade street facing commercial uses.

In response to the DAC’s supportive recommendation, the College agreed with all skybridge design guidelines but requested flexibility regarding the height of the skybridge. The majority of DAC members are willing to provide flexibility on skybridge heights under the condition that the College obtain the written permission of the future IAC to deviate from this stipulation.

One DAC member expressed concern over the skybridge, voting against the change in giving the College more flexibility regarding the height. This member initially agreed to the original compromise, under the assumption that a skybridge on the uppermost floor will have much less utilization than one on a lower floor. But with the College’s request to remove this requirement, the initial dissenting DAC member, joined by a second DAC member who changed their position, could no longer support the skybridge, citing four major concerns:

1. The skybridge will draw pedestrian activity away from the Howell Street passageway, thereby worsen, not improve, campus safety for all users. The key to making campus feel safe for all users is to increase, not decrease, the amount of pedestrian activity.
2. The skybridge will benefit students and College employees alone. The College has a duty to balance the needs of people who go to school and work at the College with those of the broader Capitol Hill community that also uses the space. Campus improvements should improve open space and connect, not separate, students with the neighborhood in which the College is part of.
3. The skybridge proposal undermines the College’s own stated goals for community and connectivity/ circulation improvements of the plan in section 2-24 of the PDEIS, which seek to: “provide improvements to campus environs that are shared with the larger Capitol Hill community; increase the permeability of the campus; activate building frontages and streetscapes; and improve safety for students and the community.”
4. The skybridge would cast an unnecessary shadow over a significant percentage of the Howell Street passageway in summer and autumn. This could perpetuate potential concerns for personal safety and drive pedestrian activity away from the passageway in a negative feedback loop.

Recommendation 39

The DMIMP should include design guidelines and development standards for skybridge and the DAC’s preferred design guidelines for them as previously outlined in the DAC’s [Preliminary Draft Master Plan Comments](#) letter published on March 2, 2023. The DMIMP should also reflect the DAC’s firm stipulation that

any planned skybridges across the Howell Street Passageway must respect a third floor minimum unless the College obtains explicit, written approval from the IAC.

Response 39

The College understands the DAC’s concern regarding a potential skybridge connecting the Broadway Edison Complex (BEC) and the existing SAM and/or planned ITEC buildings. The College acknowledges that a skybridge would only benefit students, faculty, staff, and visitors of SCC. The College has a duty to provide accessible connections between buildings. People entering the BEC from the Howell Street Passage enter the BEC on the second floor. Requiring a skybridge to be three floors above grade would put the bridge at the fifth floor, the highest level of the BEC. The fifth floor of the BEC has a small footprint and limited educational and staff space. Locating a skybridge on the uppermost floor of the BEC limits its functionality and usability. The College proposes a two-floor minimum height above grade, with an agreement for further study of a third floor connection at the time of design, for a skybridge above the Howell Street Passage.

The DAC believes that the campus lacks meaningfully landscaped and greenscaped areas. Most existing natural open space is relegated to the glade by the South Plaza— an uninviting area ill-suited for the College’s and community’s current needs.

The DAC, in our previous comment letter, requested that the College commit to incorporating landscaped features into any new development or renovation. While the College acknowledged the committee’s position on the matter, and even incorporated some of our expectations into their MIMP design guidelines, they came short of committing to incorporating landscaping with every new project.

Recommendation 40

The DMIMP should express the College’s commitment to incorporating green landscaping into every planned development or renovation and increasing the habitat functionality of all existing greenspaces that are not currently planned for redevelopment.

Response 40

See [Chapter 4 – Landscaping](#) of the PDMIMP which states that landscaping, green infrastructure, and other site development requirements are included under Open Space Standards (page 4-8) and Guidelines (page 4-16). Also, as stated in PDMIMP, Appendix B [Response to CAC Comments](#), Response 54, the College will always work to provide high-quality, accessible public space to College users and the public. Green space & landscaping will be incorporated into projects on a case-by-case basis.

As the DAC has reiterated on numerous occasions, the current parking garage façade operates as a pedestrian dead zone. Most pedestrian activity consists of transiting across the site or from pedestrians heading to and from vehicles parked in the garage. At night, this effect is even more pronounced.

A conceptual rendering of the proposed Student Housing project in the PDMIMP (page 18) and the PDEIS (figure 2-8, page 2-20) show commercial spaces on the southeast corner of the building, sited where most pedestrian activity is already expected to occur. This rendering, and the limited information available in the PDMIMP and from the College itself regarding this project, do not indicate if there will be commercial uses along the western and southwestern edges of the site.

The DAC believes that distributed commercial space along the entire southern, and parts of the western, edge of the site will activate the streetscapes bordering the proposed Student Housing project.

Recommendation 41

The DMIMP should include language within the *Design Guidelines and Development Standards* section that affirms the College’s intent to activate the street frontages of the new Student Housing project with commercial uses (also see Recommendation 46)

Recommendation 42

The DEIS and DMIMP should include updated renderings for the Student Housing project that shows a continuous, active building façade along E Pine Street (also see Recommendation 47).

Response 41

See Chapter 4 - Community Services and Retail Uses, page 1-20, of the PDMIMP for notes on activating building facades that face Pine street.

In addition, Chapter 4 page 1-16 under Student Housing, of the PDMIMP includes statements about street activating measures and commercial uses along the E Pine Street frontage specific to the project.

Response 42

At the recommendation of SDCI and following other recent MIMPs, all concept renderings have been removed from the DMIMP. SDCI notes that including concept renderings may create obstacles in the future when the actual building is designed, and it may not look like the image in the MIMP.

The DAC appreciates the College’s commitment to host a design charette with the IAC to ascertain mobility obstacles. However, the DAC believes that the College should partner with the mobility- impaired community directly to ascertain these obstacles.

Recommendation 43

The DMIMP should better articulate the College’s plan for evaluating mobility obstacles and to express the College’s commitment to work with the mobility-impaired community to evaluate such obstacles.

Response 43

See the Universal Design Guidelines, Chapter 4 pg. 1-28 in the PDMIMP, these guidelines articulate the College's commitment to provide a campus accessible to all.

The College is further assured its facilities appropriately serves its mobility-impaired community by the following:

- All College projects are required by Law to be fully accessible.
- All projects are reviewed by the State Facilities Accessibly Advisory Committee (SFACC), not just compliance with federal law, but for enhanced equity for the mobility impaired. SFACC review is required prior to approval for construction.
- SCC’s Accessibility Resource Center is a resource used in the development of planned projects and is consulted as projects are executed.

The College undergoes a Civil Rights audit for campus accessibility every 10 years (on average). This audit is conducted by an independent third party. The audit provides the College with lists of required accessibility repairs and improvements to comply with all state and federal accessibility requirements.

Concerning Neighborhood Integration Design Guidelines

The DAC reviewed the PDMIMP design standards to ensure that the proposed campus improvements integrate well into the neighborhood and complement the design guidelines of the two urban villages within which the campus falls: the Pike/Pine and Capitol Hill Urban Villages. Each urban village has its own neighborhood design guidelines that inform new development.

Furthermore, the College is partially located within the Pike/Pine Conservation Overlay District, which aims to preserve the auto-row character and history of the buildings through façade preservation incentives, adaptive reuse, and complimentary architectural details in new construction. These regulations and incentives are in addition to the urban village design guidelines and new development, or redevelopment projects, must comply with both.

The southern end of campus that serves as the visual gateway to the college, is located within the Pike/Pine Urban Village and the Pike/Pine Conservation Overlay District. The DAC would like all projects within the MIO, which fall within the urban village and the overlay district, to comply with the stipulated design guidelines and reflect and honor the urban fabric of the Capitol Hill neighborhood.

The DMIMP indicates that all projects undertaken by the College in the Pike Pine Conservation Overlay District—whether renovations, additions, or new construction—will comply with the Pike/Pine Neighborhood Design Guidelines. However, it is silent on whether the College will comply with the overlay or the DAC request to complement, take cues from and honor the historic context, massing, scale and architectural features of the pre-war buildings along Broadway, the auto-row buildings along Pike and Pine, and the contextual urban fabric along Harvard when designing projects.

Recommendation 44

The DMIMP should indicate that:

1. Any further modifications, additions or renovations made by the College to the buildings within the Pike/Pine Conservation Overlay District will:
 - a. follow the requirements of the controls and incentives associated with the overlay program and
 - b. take inspiration from the renovated buildings along Pike/Pine.
2. New construction within the Overlay will:
 - a. comply with the design standards for new construction within the overlay.
 - b. reflect the fine, granular nature of the acclaimed auto-row building fabric along Pike/Pine and the similarly detailed, pre-war buildings along Broadway; and
 - c. honor the existing urban fabric, scale, and character along Harvard Avenue when integrating new structures and engender stewardship of the existing catalogue of historic buildings.

Response 44

The College understands the Pike/Pine Overlay District (SMC 23.73) was established to promote conservation of historic structures and architectural features of Seattle’s original "auto row" and find a balance between residential and commercial uses. The Pike/Pine Overlay District regulations do not align with the development needs of the College (specifically the limits on floor area and structure width) and therefore the College will not commit to follow in entirety with the Overlay.

SCC will commit to following the intent of the design requirements to the extent appropriate. This includes provision for space for small business establishments (27.73.008).

The College will direct design teams to take design inspiration from the renovated buildings along Pike/Pine.

- See the Design Guidelines on Façade Articulation and Project Specific Design Guidelines, included in the PDMIMP on Student Housing, for more information on the College’s intent to maintain the design standards established for the Pike/Pine Conservation Overlay District while meeting the needs and goals of SCC.

The proposed at-grade parking garage facility shown in the rendering of the Student Housing project in the PDEIS (figure 2-8, page 2-20) and MDMIMP (page 18) is not in keeping with the urban fabric of the Pike Pine neighborhood—one composed of active facades and an engaging and public realm. The proposed building design mirrors that of the current parking garage, with a majority of the E Pine Street frontage composed of a blank façade. Blank facades limit pedestrian interaction with the building and effectively deaden the street. The Student Housing building should not include a blank façade facing East Pine Street.

Recommendation 45
The DMIMP should include a commitment from the College to comply with the Pike/Pine Neighborhood Design Guidelines.

Recommendation 46
The DMIMP should include design guidelines specific to buildings with frontages on East Pine Street to ensure the student housing building design does not result in large blank walls facing East Pine Street. Where expanses of blank walls, retaining walls, or garage facades are unavoidable, the College should include uses or design treatments at the street level that are human scale and designed for pedestrians (also see Recommendation 41).

Recommendation 47
The DEIS and DMIMP should include a new rendering of the Student Housing project that reflects the design guidelines referenced in Recommendation 41 (also see Recommendation 42).

Response 45
As stated in PDMIMP [Chapter 4](#), pg. 1-13 under "Introduction," the Design Guidelines provided in the MIMP are "intended to be supplemental to the Capitol Hill Neighborhood, Pike Pine Neighborhood, and City of Seattle Design Guidelines... They seek to add additional clarity for projects and improvements developed by Seattle Central College." The College will direct design teams to reference the applicable design guidelines based on the location of the project.

Response 46
See [Section 4 - Project Specific Design Guidelines – Student Housing](#), of the PDMIMP which notes specific design guidelines for the Student Housing project which include creating a highly transparent façade on E Pine Street with street activating uses.

The underlying zone (NC3P) has stringent requirements for the maximum amount of blank walls. The College is not proposing to change those requirements of the underlying zone and is therefore required to abide by them.

Response 47
See response to DAC Recommendation 42.

Proposed new buildings, additions, or building modifications located within the Capitol Hill Urban Village should seek to further design standards set forth in the 2019 Capitol Hill Neighborhood Design Guidelines, which guide future development within the Capitol Hill Urban Center Village to maintain and further develop a healthy, diverse, and vibrant Capitol Hill. When Capitol Hill Neighborhood Design Guidelines were developed, the community advisory board did not craft design guidelines specially for the area within the College’s MIO boundary since that is under the purview of the MIMP and the design guidelines developed through that process. There was an expectation among the group that design of new projects outside the Capitol Hill Urban Center Village would seek to complement each other and that the College would look to these design guidelines when developing their own.

Recommendation 48
The DMIMP should indicate that the College will consider and seek to complement the Capitol Hill Neighborhood Design Guidelines when a project is proposed anywhere within the Capitol Hill Urban Village.

Response 48
See [Chapter 4, Section 4 – Design Guidelines](#), of the PDMIMP. Throughout this section are references to the applicable Capitol Hill Design guidelines that will apply to all campus development.

See [PDMIMP Appendix B Response to CAC Comments](#), Response 47, SCC reviewed the 2019 Capitol Hill Neighborhood Design Guidelines and included relevant items in the Design Guidelines and Development Standards (MIMP Chapter 4 if the PDMIMP).

The College incorporated some, but not all, of the Capitol Hill Neighborhood Design Guidelines into their own. They did not capture all elements of the guidelines, and their adherence is varied ("will", "should", "consider," and "standard" being the variance of commitment). The DAC generally finds this approach aligned with its recommendations but, is concerned that the guidelines don't use the four key terms ("will", "should", "consider," and "standard") defined in the key term guide, making it impossible for the DAC to discern the College’s level of commitment. Many of the guidelines missing a key term are of great importance to the DAC, and the DAC requests that the College fully adhere to these guidelines, preferring "will" over "should" and "should" over "consider" whenever possible.

Recommendation 49
The DMIMP design guidelines should make clear the College’s intent related to each guideline. The College should make every attempt to minimize the use of aspirational guidelines and use "will" and "should" to the greatest extent possible.

Response 49
See Chapter 4, Section 4 Design Guidelines – Introduction, of the PDMIMP, which provides clarity of the college’s intentions used throughout the MIMP documents.

The College appreciates the DAC’s feedback and agrees that directive items in the design guidelines will use one of the key terms (will, should, consider, or standard) to make intent clearer. These will be added for further review in the DMIMP document. The College will use the key terms to communicate its ability to follow through on commitments and guide future design and planning efforts. However, these are guidelines and should serve the IAC and the College as a reference, not explicit rules. The purpose is to provide the College and the IAC with an opportunity for dialogue if and when project designs occur.

[Inclusion of Key Capitol Hill or Pike/Pine Neighborhood Design Guidelines](#)

While preparing this letter, the DAC compared the proposed design guidelines in the PDMIMP with the Capitol Hill Neighborhood Design Guidelines and found several opportunities for the College to strengthen their guidelines consistent with community input that shaped the Capitol Hill Neighborhood Design Guidelines, discussed in the following section. The DAC agreed that the College should design public spaces that encourage transit use and active transportation, like walking and cycling.

Recommendation 50
The DMIMP design guidelines should express the College’s commitment to ensuring and maintaining adequate pedestrian volumes. The College should look to PL1.3.b. and PL4.1 in the Capitol Hill Neighborhood Design Guideline for possible language.

Relevant Capitol Hill Neighborhood Design Guidelines
PL1 Public Life, Connectivity, 3. Walkways and Connections, b. Pedestrian Volumes:
Provide ample pedestrian space along retail corridors and key pedestrian corridors that provide access to light rail facilities and the downtown core, such as E Olive Way, E John St., and E Denny Way. Use minor voluntary ground-level setbacks, structural setbacks, building overhangs, and high-quality hardscape finishes at the pedestrian level to ensure adequate space and durability for pedestrians, while maintaining the street wall and providing adequate space for sidewalk amenities that contribute to public life.

PL4 Public Life, Active Transportation, 1. Connections to All Modes
For buildings along corridors that provide direct pedestrian access to light rail station entries and other key transit access points - including: Broadway, 15th, E John St, E Olive St, E Denny Way, E Howell St, E Nagle

Place, and 10th Ave E below Thomas – locate primary entries to conveniently access transit and consider that secondary entries may also be required to maximize pedestrian access to transit.

Response 50

PL1 is referenced in the Open Space Design Guideline of the PDMIMP. An additional reference will be added in the DMIMP, to the Pedestrian Circulation Design Guidelines. A reference to PL4 will be added in the DMIMP, under Relationship of New Development to Surroundings.

DAC members expressed interest in using the combined Student Union project to improve the quality of the public realm and safety for pedestrians’ crossings along Nagle Place between Cal Anderson Park and campus.

Recommendation 51

The DMIMP design guidelines for the Student Union project should include the following, which draws inspiration from Capitol Hill Neighborhood Design Guideline PL1.1.a: “Design the Cal Anderson Park- facing facade to enliven and enhance the safety of the adjacent space. Orient entries, windows, decks, and other amenity spaces to face the park. Design the building facing Cal Anderson Park with active street level uses to support and reinforce its role as the “front yard” and civic square for Capitol Hill.”

Relevant Capitol Hill Neighborhood Design Guideline

PL1 Public Life, Connectivity, 1. Enhancing Open Space, a. Parks

Design buildings facing a park or P-patch to enliven and enhance the safety of the open space. Orient entries, windows, balconies, decks and other amenity spaces to face the park. Design buildings facing Cal Anderson Park with active street level uses to support and reinforce its role as the “front yard” and civic square for Capitol Hill.

Response 51

See Chapter 4, Section 4 – Project Specific Design Guidelines for the Student Union project, of the PDMIMP, which states: The design of Call Anderson facing facade **should** enliven and enhance the safety of the adjacent space. Orient entries, windows, decks, and other amenity space to face the park

The DAC discussed whether the College should meet the needs of the broader Capitol Hill community when designing public spaces or making design decisions that impact the public realm in the MIO. With a few dissenting opinions, the DAC generally agreed that the College should design public spaces that are inclusive, accessible to, and inviting to everyone, not just students and staff. The proposed guideline to prioritize student usability of open space over public usability is inconsistent with the Capitol Hill Neighborhood Design Guidelines, which seek to create an enhanced public realm that is visually and physically accessible to the public and inclusive. It also conflicts with other design guidelines proposed by the College, including:

- “Consider design features that visibly represent and celebrate the diversity of the Capitol Hill and Pike Pine neighborhoods so that the college environs contribute to a welcoming, supportive, safe, and inclusive public realm.”
- “When opportunities are available to improve underutilized green spaces, redevelopment will prioritize the needs of students, staff, and community at large. To assure the redeveloped spaces are transformed to high-quality, attractive, and accessible public space, the design guidelines included throughout this document should be utilized.”

Recommendation 52

The DMIMP should not include the guideline from the PDMIMP that states: “Student usability of open space will be prioritized over public usability.” The DAC recommends removal of this guideline and the inclusion of relevant language from the Capitol Hill Design Guidelines listed below Recommendation 53 to reinforce the principles of accessibility and inclusivity.

Recommendation 53

The DMIMP design guidelines should include a stronger and clearer commitment from the College to ensuring their open space can be viewed, used, and enjoyed by the public. The College should look to the following relevant Capitol Hill Design Guidelines when drafting this language.

Relevant Capitol Hill Neighborhood Design Guidelines

DC3 Design Concept, Open Space Concept, Open Space Uses and Accessibility, 1. Open Space Uses and Accessibility a. Ground Level Open Space

Consider providing multi-use open space (generous corner landscape treatments; courtyard entries) that can be viewed, used, and enjoyed from the adjacent sidewalk.

PL1 Public Life, Connectivity, 1. Enhancing Open Space, b. Right-of-way – Enhance open space connections, 2 Design sidewalk ROW and private space adjacent to the ROW to prioritize both pedestrian circulation (comfort and safety), and environmental sustainability. Use planters, seating, and landscape to provide an inviting, attractive, and safe streetscape for pedestrians while ensuring adequate space for pedestrian circulation. Special attention should be paid to Summit and Belmont (from E. Olive St. to E. Howell St.), on Bellevue (from E Loretta Place to E Harrison Street) and along the Melrose Promenade

PL1 Public Life, Connectivity, 2. Adding to Public Life, a

Maintain a continuous street wall along retail corridors to contribute to the area’s pedestrian- oriented, urban character. Minor variations in the street wall such as recessed entries and inset window bays are acceptable if they help contribute to the pedestrian scale. b. On major retail streets, locate any large open spaces in the interior of the block, where it would not disrupt the continuity of retail street frontages and maintain the desired intensity of commercial activity in the area. Provide clear visual access to the interior open space from the public sidewalk.

PL1 Public Life, Connectivity, 4. Outdoor Uses and Activities Pedestrian Amenities

Design any larger ground-level open spaces adjacent to the sidewalks for informal community events and gatherings, including temporary art installations, live music and dance performances by community and social organizations, as well as independent artists. Provide features and amenities necessary to ensure that spaces are versatile and functional, such as power outlets, flexible seating, sight lines, acoustic materials, and community poster or bulletin boards. Site spaces to allow visibility from the sidewalk without impeding pedestrian flow.

PL2 Public Life, Walkability, 1. Universal Access

Design the public realm and shared private spaces to encourage intergenerational use and maximize accessibility for all people regardless of ability, background, age, and socioeconomic class. Incorporate universal design strategies to ensure that the common realm is accessible to all. Walkways should include adequate lighting, slip-resistant hardscape finishes, and terraces, benches, and other places of respite for pedestrians. This is especially important near light rail stations, in steeply sloped areas, and along Denny, John, and other pedestrian corridors that connect to major employment centers.

PL2 Public Life, Walkability, 2. Inclusive Neighborhood

Consider design features that visibly represent and promote the neighborhood’s LGBT+ culture and identity, contribute to a more welcoming, supportive, and safe public realm, and remind everyone that Capitol Hill is an inclusive neighborhood.

Response 52

College Administrators and MIMP consultants went on a walk of campus grounds with seven members of SCC Student Leadership on October 27, 2020. Multiple students noted public open spaces of campus, like the South Plaza and associated tree grove, do not feel like part of the college and occupying them can feel uncomfortable. The College’s primary mission is to provide a safe and comfortable educational environment for students. If students do not feel comfortable occupying their own campus, the College needs to address that. The College has no plans to remove public accessibility of open spaces, and alterations to improve student comfort will likely improve the experience of open space for the public at large.

Concern for student safety, comfort, and use is paramount for decisions related to campus evolution. The College will not remove the guideline as requested.

Response 53:

See Chapter 4, Section 4 – Open Space Design Guidelines, of the PDMIMP, which lists the Seattle and Capitol Hill Neighborhood Guidelines, strategies DC3 and PL1. This appropriately indicates the College’s strong commitment. The DMIMP will add PL2 to the list of guidelines.

The DAC expressed a desire for landscaping that supports habitat and wildlife functionality and ensures that sustainability best practices are employed in plant selection and maintenance. The Capitol Hill Neighborhood Design Guidelines articulate a vision for a sustainable, connected, functional, and biodiverse landscape across the neighborhood. These guidelines were crafted with such specificity due to their importance to the community.

Recommendation 54

The DMIMP should include the underlined concepts listed in the relevant design guideline language to better support a sustainable, connected, functional, and biodiverse landscape across the neighborhood.

Relevant Capitol Hill Neighborhood Design Guideline

DC4 Design Concepts, Exterior Elements & Finishes Plant Materials & Hardscape, a. Beneficial Plants Use plant species that are suitable for site condition, climate, and design intent. Maximize the use of native and/or naturally growing (non-invasive) plants that are self-sustaining, low maintenance, drought and pest resistant, and durable in urban conditions. Encourage the use of pollinator plants and those that provide wildlife and avian habitat appropriate to the region. Avoid invasive species that may jeopardize local ecosystems, or species that require the use of petrochemical fertilizers or pesticides.

DC4 Design Concepts, Exterior Elements & Finishes Plant Materials & Hardscape, b. Diversity Plant diversity provides resistance to insects and diseases pests. As a general guide for larger sites, plant not more than 10 percent of any species, no more than 20 percent of any genus, and no more than 30 percent of any family. For smaller sites select species that contribute to plant diversity of the community.

CS1 Context & Site, Natural Systems & Site Features, 5. Water Features, a Consider sustainable design opportunities such as shared water systems for rainwater harvesting, greywater reuse, and blackwater processing/reuse. Reduce flows into the municipal stormwater system through stormwater management, green roofs and walls, and swales. Consider other functional solutions for sustainable water reuse and/or drainage that work well with the neighborhood’s soil condition and topography.

CS1 Context & Site, Natural Systems & Site Features, 5. Water Features, b Design landscapes that reduce potable water use for irrigation such as via the following strategies:

- Reuse captured stormwater, greywater, HVAC blowdown or condensate for irrigation.
- Specify plants, soils, and other features to be self-sustaining with natural precipitation only.
- Design planting zones so that plantings no longer require irrigation once established.

Recommendation 55

The DEIS should include the arborist’s recommendations that the College should attempt to increase tree species diversity by avoiding planting any new London planetree, pin oak, red oak, and sweetgum within the MIO boundary. These four species currently comprise approximately 44.4% of the tree species currently in the proposed MIO boundary.

In discussions, the DAC expressed interest in the College improving connections to the light rail station entrance.

Response 54:

See Chapter 4, Section 4 – Greenspace Design Guidelines, of the PDMIMP, which lists the Seattle and Capitol Hill Neighborhood Guidelines, strategy DC4.

The DMIMP will add CS1 noted in the DAC recommendation to Chapter 4, Section 4, Design Guidelines – Sustainability, Water.

Response 55:

See the response to DAC Recommendation 71.

In discussions, the DAC expressed interest in the College improving connections to the light rail station entrance.

Recommendation 56

The DMIMP should include a design guideline about enhancing visual connections and pedestrian flows to and from the Capitol Hill light rail station and the College.

Relevant Capitol Hill Neighborhood Design Guideline

CS2 Context & Site, Urban Pattern & Form, 1. Sense of Place; Distinctive Streets, a. Broadway Enhance visual connections and pedestrian flows to and from the Capitol Hill light rail station as well as the Seattle Central College campus.

Response 56:

The noted design guidelines will be added to the DMIMP to the Pedestrian Circulation section of the Design Guidelines.

The Capitol Hill Neighborhood Design Guidelines articulate design principles to foster active transportation, including walking. The College should establish design guidelines related to weather protection to facilitate the creation of a high-quality public realm around the campus that is used year-round.

Recommendation 57

The DMIMP should reflect the concepts articulated in the relevant Capitol Hill Neighborhood Design Guidelines about weather protection.

Relevant Capitol Hill Neighborhood Design Guidelines

PL2 Public Life, Walkability 3. Weather Protection

- a. When providing overhead weather protection, ensure the waterproof covering extends far enough over the sidewalk to provide adequate protection for pedestrian activity. Provide backslopes, drip edges and/or gutters to prevent rain runoff onto the middle of the sidewalk. Weather protection should extend all the way to the building edge without a gap between the coverage and the facade. In order to provide adequate protection from wind-driven rain, the lower edge of the overhead weather protection should be no more than 15 feet above the sidewalk.
- b. On less intense commercial streets, focus overhead weather protection around residential entries. Extend from the building far enough to provide shelter for 4-6 people to comfortably gather near common building entries.
- c. Where narrow sidewalks create conflict between providing weather protection and tree canopy, indent canopy portions at trees. Prioritize tree canopy retention and new large tree plantings over full width weather protection that would impact or eliminate trees.
- d. In areas with good access to sunlight, consider using canopies as an opportunity to provide green roofs. e. Optionally, consider using operable/retractable, but still durable, awnings that can be removed or reduced in good weather to allow greater sunlight to the street.

Response 57:

See Chapter 4, Section 4 Design Guidelines – Project Specific Guidelines, of the PDMIMP, for the ITEC and Student Housing which notes: Overhead protection at walkways will be provided for a minimum of 50% of noted street frontages. Notes will be updated in the DMIMP to be consistent with the Seattle Municipal Code. Additional notes will be added to Sidewalk Improvements under Pedestrian Circulation regarding weather protection along major frontages and near building entries.

The DAC would like the College to adhere to lighting practices that foster a sense of safety and follow emerging best practices that support dark skies, prevent hue emissions that are harmful for humans and prevent harm to habitat function and wildlife.

Recommendation 58

The DMIMP should include the underlined concepts listed in the relevant Capitol Hill Neighborhood Design Guidelines below that address the full range of lighting best practices:

Relevant Capitol Hill Neighborhood Design Guidelines

DC4 Design Concept, Exterior Elements & Finishes, Lighting

Use directional down-lighting and other dark-sky friendly lighting strategies to enhance the perception of safety and minimize light pollution. Avoid outdoor lighting with high blue light content or other attributes that could adversely affect wildlife behavior and reproduction. Use low-wattage, warm tone lighting wherever possible and diffuse exterior light to make it more consistent with the context.

Response 58:

The noted design guidelines will be added to the DMIMP to the Lighting section of the Design Guidelines. The Lighting Design Guideline includes references to dark sky lighting standards and minimizing light pollution. Notes have been added to address the DAC’s concern about color temperature.

The DAC expressed interest in design elements that uplift Capitol Hill’s history and culture and encourage the College to invest in placemaking when making public realm improvements. The Capitol Hill Neighborhood Design Guidelines articulate design principles that celebrate and uplift Capitol Hill’s history, culture, and sense of place. The College should establish more design guidelines related to these values to ensure the campus redevelopment embodies these community priorities.

Recommendation 59

The DMIMP should include design guidelines about incorporating art, historic, or cultural elements into building and public space design and establishing a sense of place. The College should look to the relevant Capitol Hill Neighborhood Design Guidelines provided when crafting their language.

Relevant Capitol Hill Neighborhood Design Guidelines

PL1 Public Life, Connectivity, c. Pedestrian Amenities, 1.

Enhance the quality of the pedestrian environment through art and other placemaking features. Art should interpret or acknowledge specific ecological aspects of the site or location, provide site-specific wayfinding or “centering the viewer”, provide a greater understanding of where the person is standing, and/or intend to delight passers-by and celebrate Capitol Hill’s culture and spirit.

CS3 Context & Site, Architectural Context & Character, 2. Placemaking, a-c

The Capitol Hill Neighborhood is a designated arts and cultural district. Art and culture should reflect the local history and values of the neighborhood and should be well integrated with future developments. Art should be designed for human delight and the celebration of culture, spirit, and place appropriate to its function. Capitol Hill strongly values the intact and positive examples of its physical heritage.

- a. Encourage and support street-facing cultural open and indoor spaces to provide flexible spaces for art performances and art installations and increase interaction with the street.
- b. Encourage the integration of art into the building design and associated open space.

- c. Consider engaging with a local artist or arts organization to develop a design concept rooted in the culture of Capitol Hill.

CS3 Context & Site, Architectural Context & Character, 3. Historical and Cultural References, a and c

- a. Where possible, preserve and incorporate existing historical elements and character structures into project design, such as sites along Capitol Hill’s commercial corridors, near designated landmarks, adjacent to notable Anhalt buildings or locations bordering the Harvard Belmont Historic District.
- b. Encourage the incorporation of historic and current cultural elements that express and explain how the neighborhood has transitioned over time including, but not limited to, LGBTQ community, Arts District, and EcoDistrict priorities.

Response 59:

Relevant references will be added in the DMIMP to Chapter 4, Section 4 Design Guidelines – Incorporating Art

Some DAC members expressed interest in bird friendly design, both in terms of plant selection as well as building design. This concept and community priority is supported in the Capitol Hill Neighborhood Design Guidelines.

Recommendation 60

The DMIMP should reflect the bird friendly design language. The College should look to the relevant Capitol Hill Neighborhood Design Guidelines provided when crafting their language.

Relevant Capitol Hill Neighborhood Design Guidelines

DC4 Design Concept, Exterior Elements & Finishes, Sustainable and Environmental Choices, c. Bird Friendly Design

Employ bird friendly design strategies for the upper floors of buildings with extensive glass, such as decorative screens, louvers, or patterns integrated into the glass to warn birds before they collide. Locate landscape carefully to not create reflected greenery which attracts/confuses birds.

Response 60:

The College agrees that designing façades with bird-safe design considerations is important. An aspirational guideline will be added to the façade articulation section of the design guidelines and will be included in the DMIMP.

The DAC expressed interest in making sure the College’s development supports active transportation uses, including planning for the needs of cyclists.

Recommendation 61

The DMIMP should incorporate additional concepts regarding planning ahead for cyclists. The College should look to the relevant Capitol Hill Neighborhood Design Guidelines language when crafting theirs.

Relevant Capitol Hill Neighborhood Design Guidelines

PL4 Public Life, Active Transportation, 2. Planning Ahead for Bicyclists, b.

Locate short-term parking bike racks and bike share hardscape areas near the intended uses, but maintain clear pedestrian movement along desire lines, and maximize sidewalk activation opportunities along the storefronts. Locate bike racks within sight lines of front doors, windows, or areas with visual security. In areas where bicycle parking is anticipated to be high, consider whether an on-street bike rack or corral may be appropriate.

Response 61:

Bicycle infrastructure is addressed in the Transportation Design Guidelines and Bicycle Facilities section. The Bicycle Facilities section includes notes on location and quantities of bike racks. The Bicycle Facilities section will be added to the DMIMP.

The Capitol Hill Neighborhood Design Guidelines articulate a myriad of design approaches that promote a sustainable and resilient design approach. One element missing from the PDMIMP is a commitment from the College to take steps towards reducing the urban heat island effect.

Recommendation 62

The DMIMP should reflect design guidelines that express the College's commitment to reducing the urban heat island effect. The relevant language below can serve as an example for the College.

Relevant Capitol Hill Neighborhood Design Guidelines

DC4 Design Concept, Exterior Elements & Finishes, 2. Sustainable and Environmental Choices, e. Heat Island
Design the building and open space to reduce the urban heat island effect. Use roofing materials with a high solar reflectance index or install a vegetated roof. Minimize the area of asphalt, concrete, and other hardscape. When used, consider coatings and colorants to achieve a lighter colored surface. Integrate plantings into passive design strategies for the building, e.g., use large canopy deciduous trees or a vine covered trellis to shade and cool a south-facing facade.

Response 62:

The College appreciates the DAC's thoughtfulness. Notes on reduction of urban island effect in regard to façade and open space design have been added to relevant design guideline sections and will be included in the DMIMP.

The DAC expressed an interest in minimizing the impact of parking entrances and carefully locating them to mitigate the impact to the public realm and pedestrian safety. The DAC also expressed confusion over the intended location of the parking garage entrance for the student housing project. The PDMIMP shows the location along Harvard Avenue and the PDEIS includes conflicting information about the location of the entrance. For example, on page 2-23 of the DEIS, the College states that "It is anticipated that vehicular ingress/egress to the parking garage would be revised to permit vehicle access from Boylston only and remove vehicle access from Harvard Ave," but the rendering (Figure 2-8) shows parking access off Harvard.

Recommendation 63

The DMIMP should include a design guideline that speaks to a commitment to reduce the visual impact of planned parking entrances. The relevant language from the Capitol Hill Neighborhood Design Guidelines can serve as an example.

Relevant Capitol Hill Neighborhood Design Guidelines

DC1 Design Concept, Project Uses & Activities, 2. Parking and Service Uses, a. Visual Impacts When it is necessary to locate parking entrances and service uses on street frontages, or in highly visible locations, use artistic treatments (e.g., murals or decorative metalwork on garage doors and adjacent walls) or lush landscape screening to reduce visual impacts. This is especially important in locations where commercial uses extend to streets with residential character (e.g., Nagle Place, Harvard Avenue E, 14th Avenue).

Recommendation 64

The renderings, site maps, and supporting text about the student housing parking garage entrance(s) in the DMIMP and DEIS should be consistent and accurately reflect the College's intent.

Response 63:

The design guidelines will be revised to incorporate a notes on Parking and Service Uses and will review the Capital Hill Neighborhood Design Guideline when preparing.

Response 64:

See response to Recommendation 42 above.
Site maps will be adjusted in the DMIMP Document.

The College states that the existing garage will undergo renovation in the project description of the Student Housing project in the PDMIMP Chapter 3.2. Throughout the renovation process, the DAC recommends the College and IAC pay close attention to the design in relationship to topography. Furthermore, to maintain a vibrant and engaging street frontage, the DAC recommends that the College prioritize the neighborhood's urban pattern and form in conjunction with the historic context in the Student Housing project. Articulation at lower levels will reinforce the urban pattern and form and minimize the visual impact of the parking garage within the Student Housing project.

Recommendation 65:

The DMIMP should include guidelines specific to local topography to ensure the design is carefully considered. The College should look to the language in the relevant Pike/Pine Neighborhood Design Guidelines when crafting theirs.

Relevant Pike/Pine Neighborhood Design Guidelines

CS1 Context and Site, Natural Systems and Site Features, I. Topography Design

Design the massing of larger structures to respond to the sloping terrain.

- i. Step the elevation of ground floors so that building entrances and ground floors roughly match the street grade.
- ii. Design the building massing to step with grade using techniques such as changes in the levels of upper floors, breaks in the roofline, and vertical and horizontal modulation.
- iii. Use existing grade changes to minimize service and access impacts on the Avenues in through-block developments.

Grade changes on through-block sites present opportunities for consolidating access points and "tucking" certain functions into the slope where they are less visible from the street and less likely to encroach on space desired for street-level uses. Through-block developments should be designed to take advantage of these opportunities by including vehicle drop-off, parking, and service and delivery areas within the development in a manner that efficiently accommodates these functions and minimizes conflicts with pedestrian activity along block perimeters.

CS2 Context & Site, Urban Pattern and Form, I. Location in the City and Neighborhood, I

- i. Architectural presence: Retain as much of the existing physical context as possible with new development. Redevelopment that is responsive to the existing context may include repurpose and reuse of existing structures, as well as reinterpreting common aspects of historic buildings in new projects, such as the use of durable materials, transparent street fronts, inviting, high-ceilinged ground floors, and straightforward structural design.

CS2 Context & Site, Urban Pattern and Form, II. Adjacent Sites, Streets, and Open Spaces, II

- i. Site Characteristics: Massing and articulation should respond to the established Pike Pine development pattern of street facing façade widths and mid-block depth. The original platting has generally dictated a development pattern that is characterized by structures built on one lot with a width of 50 or 60 feet, or on two combined lots with a width of 100 or 120 feet. These older structures are typically limited to a half-block in depth, or 100 to 128 feet. The scale of larger buildings is typically reduced through a rhythm of bays that relates to the scale of smaller structures.
- ii. Connection to the Street: Integrate new development with existing street patterns to maintain a cohesive streetscape:
 - a. Orient active street-level uses on Pike and Pine Streets, Broadway, and on streets requiring street-level uses east of Broadway.
 - b. Maintain the strong 2 to 4 story street wall along street lot lines established by existing development, while allowing for upper story step backs.
 - c. Design street frontages with a quieter, more residential character on north-south Avenues west of Broadway.
 - d. Design all street fronts for activation, visual interest, and variety.

- e. Design any setbacks from the street as pedestrian-oriented spaces enhanced with landscaping, street-furnishings, and high quality, well-detailed pavements between the sidewalk and the building.
- iii. Open Space: Consider providing additional open space and landscaped areas at key locations, including frontages at “gateway” intersections shown on Map 1 on page 3 and “bow tie” and “crossroad” intersections shown on Map 2 on page 4, where it may be possible to integrate such spaces with abutting right-of-way areas to create larger, functional spaces. III. Relationship to the Block
- iv. Corner Sites:
 - a. Design new development to address corners by taking cues from historic buildings. Historic buildings in Pike/Pine seldom incorporate unique or specially significant corner features, but rather “bend” the more standard elements of the building frame around the corner. This strategy responds to the corner, but in a subtle way that includes an element that is repeated elsewhere in the structure.
 - b. On corner sites at “bow tie” “crossroads” and “gateway” intersections shown on Maps 1 and 2 on pages 3 and 4, incorporate special architectural features, landscaping, or site elements that reflect the angle, orientation, and high visibility of the design at those intersections.

Response 65:
The design guidelines in the Façade Articulation section include specific references to stepping façades so floorplates generally match street grades and the width and character of historic façades along Pike, Pine, and Broadway. The College agrees this is especially relevant to the Student Housing project.

There is no mention specifically to East Pine Street streetscape improvements in the PDMIMP design guidelines, but an early rendering of the Student Housing project in the PDEIS (figure 2-8, page 2-20) and the PDMIMP (page 18) show a large, blank wall along East Pine Street. The DAC expressed strong interest in ensuring the design along East Pine Street results in an active and engaging street front and a high-quality pedestrian-scale experience. This extends to several different design aspects ranging from retail space design to materials.

Recommendation 66
The DMIMP should include design guidelines specific to the new Student Housing project frontage along East Pine St. between Harvard and Boylston Avenue.

Response 66
See Chapter 4 Section 4 - Project Specific Design Guidelines – Student Housing and Chapter 5 – Figure 5-1.3 of the DMIMP for specific information on improvements to the E Pine St frontage.

The College sees the Student Housing project as an opportunity to fill a critical gap in the Pike Pine retail corridor and looks forward to further development of these concepts presented in the DMIMP.

Concerning Plants and Animals

The DAC read the Arborist Report in the PDEIS and agrees with the recommendations made therein. The DAC believes that the College should embrace these recommendations.

Recommendation 67
The DMIMP should express the College’s commitment to abiding by all of the Arborist Report recommendations in the PDEIS and include the program by which each mitigatory proposal will be implemented and monitored.

Response 67
The DMIMP will add a section on Tree Planning and Preservations to Section 3 Development Standards, Landscaping. It will include, among other language, the following:

- Site planning around the identified exceptional trees will follow the requirements of SMC 25.11.050
- Site planning around the identified trees in Environmentally Critical Area will comply with SMC 25.09.070
- All pruning required for construction clearance should be performed by an ISA Certified arborist and conform to current ANSI A300 standards.
- Trees should be surveyed prior to construction and final impacts analyzed. Tree retention should be considered through the design process to ensure that trees with high retention value can be protected.

As the PDMIMP does not require or request the modifications of the South Plaza area to implement the power plant—the DAC assumes any modifications to the plaza renovations would likely be made in association with the planned Broadway Performance Hall project. Furthermore, the DAC previously discussed that the exceptional grove on the South Plaza can appear overly “linear” in areas and has negative impacts on the site—feeling closed off and unwelcoming to the public. Therefore, the DAC would like the College to commit to preserving the exceptional grove while also finding ways to mitigate some of its unwanted effects on the space and consult with the IAC preservation and mitigation strategies.

Recommendation 68
The DEIS should stipulate that any proposed design modifications to the South Plaza and Broadway Performance Hall should prioritize the preservation of the exceptional London plane tree and sycamore tree grove in the plaza (trees 73-108) and the DMIMP should reflect the College’s commitment to ensuring this preservation. Selective removal of part or whole of the exceptional grove must be executed with approval of the IAC.

Recommendation 69
The DMIMP should express the College’s commitment to preserving the exceptional grove while also finding ways to mitigate some of its unwanted effects on the space, in consultation with the IAC.

Response 68
The College agrees to provide additional clarity, in the DMIMP, of its commitment to an appropriate balance of tree preservation, South Plaza Improvements, creating quality publicly accessible space, and working in partnership with the IAC.
Response 69
See response to Recommendation 68 above.

The DAC supports the retention of high value trees throughout the MIO, not just in the South Plaza, and encourages the College to diversify the tree canopy when planting new trees.

Recommendation 70

The DMIMP should reflect the College’s commitment to protecting and retaining trees with high retention value.

Recommendation 71

The DMIMP should include urban forestry guidelines for selecting replacement trees and allowable proportions of any one species throughout campus in proposed tree development. The College should commit to abiding by these standards, intentionally planning for canopy diversity, and avoiding planting more of the predominant species currently on campus: (London planetree/Platanus x acerifolia, Pin Oak/Quercus palustris, Red Oak/Quercus rubra and Sweetgum/Liquidambar styraciflua)

Response 70

The college is committed to compliance with the City of Seattle’s Tree Protection Code and its regulation regarding limits on number, size, and types of trees that can be replaced. A reference to compliance will be added to the DMIMP.

Response 71

The College will consult industry professionals and designers when planning and designing landscape features. Consultation will take place at the time each project design commences.

- Selection of street trees – including approval of species, is regulated by the Seattle Department of Transportation.
- Selection and removal of trees in Environmentally Critical Areas are regulated by the Seattle Department of Construction and Inspections.

The college will consult with the Seattle Urban Forest Management guidelines (T4S) – Trees for Seattle for other tree species selections.

Concerning Height

The DAC carefully evaluated the proposed height for each project within the context of compatibility with existing neighboring development and whether the proposed height, bulk and scale ensured a quality experience for students, staff, local residents and the broader Capitol Hill community. While the DAC acknowledged some of the drawbacks to taller building (summarized below), it found the College’s height and articulation proposals acceptable, in keeping with the neighborhood fabric, and designed to allow the College to meet its business needs.

One factor considered by the DAC was whether taller buildings in the MIO would lead to further high- density development beyond what the existing infrastructure can support and/or larger developments and redevelopment of buildings of historic or cultural significance to the neighborhood. The DAC carefully evaluated the risk to cultural assets and believes that taller buildings won’t have a meaningful impact on the redevelopment risk of these sites. Furthermore, the DAC did not express strong concern over future demand on infrastructure.

The College states in the PDEIS an intent with the Student Housing project to balance the need for housing students, the floor area ratio, and allowable variances for additional height that may be requested at the time of development. In this location of the neighborhood, the building would be taller than other buildings in the Pike/Pine neighborhood. To mitigate the impact, the College would step back upper floors at higher levels of the building. With heights at this density along the street property setbacks, dark shadowed areas would occur limiting the comfort of the street level and minimizing landscaping strength at the pedestrian level. The DAC hopes that the upper floor setbacks will limit the shading impact.

Along Broadway, the buildings are much more public facing and open to all. However, the use of higher buildings limits the flexibility and options of students and staff to easily move between buildings, especially for those that require elevators or lifts that add time and inconvenience to get between classes. The DAC assessed this impact and ultimately supported the proposed building heights along Broadway as increased space for College expansion on Broadway lessens the need for taller structures on Harvard Avenue.

Some DAC members expressed concerns about future tall buildings on the east side of Broadway shading out parts of Cal Anderson Park—namely the wading pool and the Black Lives Matter community garden located within the sunbowl/meadow feature of the park during the warm summer months and into the fall growing season--and feeling somewhat out of place amongst the other buildings on that side of the park. The park is historic in nature, with its roots extending back to the late 1800s with the construction of the water reservoir. Shortly afterward, in 1901, the designers of New York’s Central Park, the Olmstead brothers, designed the area around the reservoir, which is located just north of the area that would be affected by shadows from the Student Union project. Furthermore, Cal Anderson serves as the front yard for thousands of Capitol Hill residents who lack private outdoor space, so additional shading further limits the utility of the park during these parts of the year. Ultimately, the DAC did not feel strongly enough to issue a recommendation limiting building heights affecting the park but did want to note these lingering concerns. In response to the DAC’s previous comments about height, as found in

Appendix B of the PDMIMP, the College has requested a significant modification in how deviations from the currently planned building heights would be secured. Currently, the College would need to pass a MIMP minor amendment; they would like to instead seek the approval of the future IAC to obtain building height changes as this would afford all parties greater flexibility.

Following a clarifying discussion between the DAC and the College, an agreement was made to simplify and streamline the College’s need for greater flexibility. The College proposed a range of heights for planned

projects and, provided that the final height of a project falls within this range, the DAC will not require the college to seek a MIMP minor amendment or future IAC approval. The agreement stipulates that proposed and potential structure heights must fall within a range of 5’ in either direction of the project height initially proposed in the PDMIMP.

Recommendation 72

The DMIMP and DEIS should include the College’s proposed range of building heights and their commitment to constructing all future buildings and renovations within this range

Response 72

The College commitment and intent is clearly articulated in the PDMIMP, Chapter 4 Section 3 Development Standards, Height Limits. This section will be included in the DMIMP and as appropriate in the DEIS.

Viewpoints and Aesthetics

Each viewpoint on campus as designated in the PDEIS includes diagrams on how proposed projects may affect views in and around campus. Many of the heights evaluated in the PDEIS are determined to “not significantly affect” the views, however, the massing and setbacks of the buildings are quite impactful, especially when buildings are at their maximum allowed MIO height. Based on the viewshed diagrams, structures with the maximum allowed 105’ height limit would block the amount and angles of views to the sky and would dramatically overshadow nearby buildings.

The DAC appreciates that the College has not proposed any projects that would meet the underlying maximum MIO boundary height. The College and the DAC previously agreed that any future College projects within the MIO boundary will have building height flexibility in the form of a range of 5’ higher or lower than proposed or planned heights presented in the PDMIMP. These are to include setbacks as prescribed by building height limits. Table 3.9-3 identifies the proposed setbacks that would align with existing buildings along Broadway. This alignment is appropriate, preserving the existing streetscape and views to the sky.

Recommendation 73

The DMIMP should reflect the College’s commitment to precluding any future potential appeal opportunities regarding proposed building heights and the 5’ higher / lower range that the DAC has previously agreed to. Furthermore, the College should commit to a 90’ maximum height limit for projects not yet planned or proposed in the MIMP to avoid detrimental impacts that poorly scaled structures would have on the surrounding neighborhood.

Response 73

The College appreciates the conversations and agreements reached with the DAC thus far and agrees to the proposed regulations as written. The City will enforce standards included in the MIMP, with the IAC providing advisory input. The College believe this process appropriately provides for input from the community and additional language within the MIMP is not necessary. Further, the College will not agree to any commitments against projects which are not identified nor included within this MIMP regulation and process.

Concerning Green Space

The DAC agrees that improved planting areas and greenspace on campus will enhance the quality of space and overall perception of the campus identity and the Capitol Hill neighborhood. Providing plants that are equipped to withstand climatic variability while also promoting habitat are not mutually exclusive with low maintenance, but it may require the College to employ modified techniques that necessitate some learning on behalf of the landscape staff.

Recommendation 74

The DMIMP should express the College’s commitment to establishing a set of maintenance guidelines to help the College landscape staff establish a standard of care and upkeep throughout the year.

Response 74

Seattle Central Colleges manages its facilities, and in this case, its grounds, as recommended by the Association of Physical Plant Administrators (APPA). The APPA provides standards for the performance of ground maintenance functions based on both a land-use inventory of the scope and nature of the grounds to be cared for and a determination of the standards of care. This reference will be added to the DMIMP.

Concerning Sustainability

The DAC supports the sustainability framework presented in the PDMIMP and encourages the College to actively pursue their aspirational goals. There exists an opportunity to educate students and the community about the sustainable strategies incorporated into the physical campus which the College should capitalize on.

Regarding the District Energy Plant project briefly proposed within the PDMIMP, the DAC does not currently have sufficient information to adequately evaluate it or its impacts on the surrounding neighborhood. The committee does, in a general sense, support the inclusion of projects that helps the College reduce its carbon emissions.

The DAC understands that comments on the District Energy Plant may fall outside of the MIMP’s purview but would still like to recommend that the College both pursue private partnerships to fund this project and explore using the installation process to make open space, landscaping, and internal circulation improvements to the South Plaza.

The DAC supports the District Energy Plant if it will not contain any features at or above grade that reduce quality of or access to South Plaza open space, or significantly reduce the amount of open space on the South Plaza. The committee assumes the College included the project in the MIMP simply to demonstrate that there may indeed be funding to support plaza improvements and to provide flexibility in where to locate the project. If these assumptions are not correct, the College and the City should seek DAC input before finalizing the MIMP.

Recommendation 75

The DMIMP should include, in significantly greater detail, the College’s realistic and aspirational plans for the District Energy Plant project. The College should commit to utilizing the District Energy Plant as a learning opportunity to educate the public about sustainable practices and ensure that no element of the project impedes the use of open space in the South Plaza.

Response 75

The College agrees that the potential District Energy Plant project can serve as an educational tool for students, faculty, staff, and the Capitol Hill community. The DMIMP will update the Sustainability Design Guidelines to include educational opportunities with the District Energy Plant. Regarding potential impacts on the use of the South Plaza, the DMIMP, Chapter 3 Potential Projects will address the infrastructure that will be required to properly operate and maintain the below ground facilities.

It should be noted, that below grade facilities that provide utility services and infrastructure are not subject to MIMP regulations. The College is providing information on this potential project in the interest of transparency with the community.

Additionally, the DAC originally requested, in recommendation 32 of the DAC’s *Preliminary Draft Master Plan Comment* letter, published on March 2nd, 2021, that the College include compost bins around campus. Following the release of the PDMIMP, the committee noticed a lack of language regarding composting. The DAC understands that the College may have practical concerns for not committing to provide publicly accessible compost bins on the campus but believes that these concerns should be detailed in the MIMP.

Recommendation 76

The DMIMP should articulate the College’s planned approach towards composting in the Sustainability Guidelines section.

Response 76

The college’s operations issues regarding composting are not an appropriate topic for a MIMP. The college does include composting as part of its operations and has collection facilities in all college buildings. The college fully supports, and complies with, the processes established by Seattle Public Utilities.

Concerning Earth, Environmental Health and Construction Impacts

The DAC analyzed the PDEIS for impacts from future construction activities and planned mitigations. The DAC supports the analysis of potential construction impacts and proposed mitigations. While the DAC understands some impact is unavoidable, we would like to see as many efforts employed as possible to mitigate these impacts.

Recommendation 77

The DEIS should express the College’s commitment to abiding by all of the mitigation measures proposed in the PDEIS’s Geologic Hazards and ECA Review, prepared by GeoEngineers, Inc. The DMIMP should include the program by which each mitigatory proposal will be implemented and monitored.

Recommendation 78

The DMIMP should express the College’s commitment to abiding by all of the mitigation measures proposed in the PDEIS’s Construction Impacts section. Furthermore, the DMIMP should include the program by which each mitigatory proposal will be implemented and monitored.

Response 77

The DEIS is a decision-making tool which identifies environmental impacts that could potentially result from implementation of the DMIMP, and mitigation measures which if implemented, could address or reduce impacts to non-significant levels. The MIMP and the EIS will go to the Hearing Examiner for review and recommendation to the City Council. City Council will approve conditions on the MIMP, including mitigation measures identified in the EIS. Mitigation measures that were identified for geological hazards and ECA areas are associated with the City’s Critical Areas code and would be enforced by the City.

Response 78

The DEIS is a decision-making tool which identifies impacts that could potentially result from implementation of the DMIMP, and mitigation measures which if implemented, could address or reduce impacts to non-significant levels. The MIMP and the EIS will go to the Hearing Examiner for review and recommendation to the City Council. City Council will approve conditions on the MIMP, including mitigation measures identified in the EIS. Mitigation measures that were identified would be enforced by the City.
The City will enforce mitigation measures during the permit review, approval, and construction processes.

The DAC acknowledges that construction on the College’s future projects must occur but has also expressed concern about the impacts that sustained construction will have on the campus and on the surrounding neighborhood. In particular, the DAC worries that: 1) construction workers parking on or around campus could force parking demand to exceed supply; and 2) the residential land uses west of campus could experience unusually severe disruptions.

Concerning Site Maps in the PDEIS

In Figure 3.10-1 of the PDEIS, the map in the top left corner has a significantly higher resolution quality than all other shadow study maps included within the section. In comparison to its neighbors, this map is much easier to read.

Recommendation 79

Figure 2-10 in the DEIS should label the area in front of Broadway Performance Hall as an open space improvement area and show the location of existing pedestrian crossings.

Response 79

Figure 2-10 will be updated in the DEIS.



May 17, 2024

Carly Guillory
Seattle Department of Construction & Inspections
700 5th Ave. #2000
PO Box 34019
Seattle, WA 98124-4996

Subject: 3034600-LU – Response to SDCI and SDOT Comment on Preliminary DRAFT MIMP -
Seattle Central College Major Institutional Master Plan
1701 Broadway

Dear Carly

We are pleased to provide the following response to comments received from SDCI and SDOT on the Preliminary MIMP Document previously provided. The comments were received in a variety of formats. We have assembled here and provided this single response document. References to how the comments were received are noted.

We look forward to submitting the DRAFT MIMP and DRAFT EIS Documents in the near future

Respectfully,

Stephen J. Starling, AIA
Principal
Starling Whitehead & Lux Architects

SDCI Comments embedded in the PDMIMP/PDEIS document

- Comment 1: Planned Projects. Page 18 of the Draft Master Plan (DMIMP) begins information describing planned projects. Renderings are also included, illustrating a possible design of these future projects. Question for consideration: Does it make sense to include conceptual renderings here, or will these images create obstacles in the future at time of development? For example, will the IAC, public, city, etc. expect the building to be substantially consistent with these images when a different design may also be consistent with the MIMP and Design Guidelines?
Response 1: We concur and believe that the efforts on behalf of the DAC are appropriately reflected in the Design Guidelines which will be found in Chapter 4, Section 4 of the DMIMP. The conceptual renderings will be removed from the DMIMP.
For clarification – all comments at this stage reflect the Preliminary Draft Major Institutional Master Plan (PDMIMP), not the Draft Major Institutional Master Plan (DMIMP). The next iteration of this document will be the DMIMP.
Comment 2: Potential Development. Page 21 of the DMIMP notes that Harvard Buildings I and II will be new construction. Please clarify that the intent is that these existing buildings, if acquired, will be demolished, and replaced with new construction for institutional uses.
Response 2: Potential development Harvard Building I is a surface parking lot with no existing structures. Any building development on this site would include removal of the existing parking lot. Potential development Harvard Building II is on the site of the existing Westminster Presbyterian Church. There have not been any assessments on the existing building for potential reuse by the College. Building development to fit College needs may include demolition of the existing Church.
Development will occur when the need for additional space (for instruction, administrative, housing, or other purpose(s)) is identified and funding is provided.
Comment 3: Campus Growth and Expansion, Uses. Page 18 of the DMIMP states that Student Housing is proposed and would include "Retail/Amenities." Future review of projects within a MIMP sometimes bring forth the question of permitted uses. Consider adding language in the MIMP clarifying that this "retail/amenities" space is related to the mission of the institution.
Response 3: Language has been added to clarify that the purpose and function of the retail/amenity spaces are tied to the mission of the College.
This comment is similar to SDCI PDEIS comment 5 – verify the response to this comment aligns with that response.
Comment 4: Gateway Locations. Page 22 of the DMIMP describes various locations as "new gateway development." Does the Design Guideline section include guidelines related to this gateway concept? Should each gateway location have its own guidelines? or does it make more sense to include the same guidelines for all "gateway sites?"
Response 4: Gateway locations are identified in the PDMIMP on Figure 10 Community Connectivity – Planned, pg. 55 and Figure 11 Community Connectivity – Aspirational, pg. 56. Gateway enhancements are reviewed in the PDMIMP in Chapter 4 Design Guidelines and Development Standards under Campus Identity. The Design Guidelines review what type of elements could be employed at gateway locations.
Guidelines for Campus Identity and Gateways are flexible so they can be applied to various gateway sites.
Comment 5: Parking. Page 24 of the DMIMP states that there are approximately 633 parking stalls. Are all stalls on campus? Consider including a map that highlights the location of these existing parking stalls.
Response 5: Yes, all of the parking stalls are on campus (within the MIO Boundary). A map of the existing parking and transportation conditions is in the DMIMP.

- Comment 6: Campus Entry Plaza. Page 24 of the DMIMP states that there is proposed a campus entry plaza as part of the ITEC building and Sound Transit station. Similar to the gateway location concept, should there be guidelines specific to this location, or rather to the gateway concept itself?
- Response 6: Gateway enhancements are reviewed in the PDMIMP in Chapter 4 Design Guidelines and Development Standards under Campus Identity. The Design Guidelines review what type of elements could be employed at gateway locations. These guidelines are flexible so they can be applied to various gateway sites.
- Comment 7: Broadway Cafe. Page 27 of the DMIMP lists a Broadway Cafe (aka Eldridge Tire) - is this the same site as the South Annex Parking Lot?
- Response 7: No, the South Annex Parking Lot is associated with the South Annex (aka Booth Building) located across the street from The Broadway Cafe. The Broadway Café (aka Eldridge Tire) is the site of the new Pride Place development.
- Comment 8: Development Standards. Page 29 of the DMIMP begins with a description of development standards. Recommend being crystal clear what dev standards are being modified from the underlying zone. Consider beginning this section with a matrix/table/etc. to clearly list those standards that are MIMP specific.
- Response 8: The structure of the Development Standards section has been revised to make the development standards proposed as part of this MIMP clearer.
- Comment 9: Development Standards. Page 29 of the DMIMP includes language from SMC 23.47A.014. It appears this is exact code language. What is the intent of including this code language in the MIMP? This code language could change before we see a project for future development, potentially necessitating a MIMP amendment or other discussion. Would it make more sense to summarize these standards, noting that future projects will comply? Or including this language only if there is a modification from code language requested?
- Response 9: The formatting in this section has been revised in the DMIMP.
- Comment 10: Development Standards, Height. Language on page 29 of the DMIMP ("The maximum height limit of 105 feet shall apply across the entire MIO District.") differs slightly from language on page 28 (page 27) that says all property south of Pine will be MIO-75. Please clarify/revise.
- Response 10: Proposed heights have been updated and are consistent throughout the DMIMP. MIO-105 is noted for all areas within the MIO boundary.
- Comment 11: Development Standards, Height. The table on page 29 of the DMIMP notes proposed height, allowable height by underlying zone, and the MIO allowed height. Recommend adding language here that clarifies that while 90-ft (for example) may be anticipated now that future needs may anticipate a taller building and that the max allowable would be per the MIO overlay (105').
- Response 11: The table noting heights of planned and potential development is the result of multiple discussions with the DAC. The College and DAC have come to an understanding that the proposed heights are acceptable and if the College desires to make them taller (i.e., to 105' high), the Implementation Advisory Committee (IAC) will be consulted for approval.
- Comment 12: Renderings, Height. Page 30 of the DMIMP includes massing studies of the proposed future development. Consider including these images, or like, in your response to the rezone criteria (SMC 23.34).
- Response 12: Comment noted.
- Comment 13: Development Standard, Lot Coverage. Page 31 of the DMIMP notes that lot coverage shall be calculated on all institution owned properties within the MIO boundary. Does it make sense to include all property and not differentiating institution owned vs. not? There is precedent for stating "institution owned" only.
- Response 13: The College does not wish to include buildings in the MIO district that it does not own in lot coverage calculations.
- Comment 14: Development Standards. Page 31 of the DMIMP includes a note that states, "The underlying zone has no lot coverage or open space standards" Is for non-residential uses. Thus, buildings could cover 100% of their sites." Is factually true? What about setbacks?
- Response 14: Based on the provisions and requirements of SMC 23.47A.014 (Setback requirements for Commercial zone), there is no required setback from 0' to 13', so a building may cover 100% of the lot. This does not account for required improvements in pedestrian zones, landscaping, or green stormwater infrastructure (GSI), which may limit the overall lot coverage of a development in the underlying zone.
- Comment 15: Density/FAR Table. The table on page 31 of the DMIMP includes numbers for total building area that appear to differ slightly from those earlier in the DMIMP. Please revise for consistency.
- Response 15: We agreed – the numbers have been revised for consistency. The gross square feet (GSF) calculation on page 14 of the PDMIMP identifies the GSF of existing campus development including below-grade and mechanical spaces. The Existing Campus Density calculation on page 31 identifies the square feet attributable to the floor area ratio (FAR) calculation. For FAR calculations, areas below grade are subtracted as well as 3.5% of the total area to account for mechanical, electrical, and plumbing (MEP) spaces.
- Comment 16: Central Utility Plant. The table on page 31 of the DMIMP includes a building labeled "Central Utility Plan." It appears this building is called "District Utility Plant" in earlier pages. Please clarify and revise for consistency.
- Response 16: All references to this project have been revised to "District Energy Plant" in this table and throughout the DMIMP.
- Comment 17: Right-of-way landscaping. Page 32 of the DMIMP states that landscaping will be included in the ROW. Consider adding language that clarifies that any future development of/within the ROW will comply with SDOT requirements (Street Improvement Manual).
- Response 17: A note will be added stating landscaping in the ROW will be designed in accordance with SDOT standards.
- Comment 18: Development Standards, Open Space. Page 32 of the DMIMP, under heading "Open Space" includes language that describes underlying zone requirements as being superseded by the provisions of the MIMP and green factor do not need to be followed. As written, it appears this language is not entirely accurate. Instead, : The development standards component in an adopted master plan shall become the applicable regulations for physical development of Major Institution uses within the MIO District and shall supersede the development standards of the underlying zone. Where standards established in the underlying zone have not been modified by the master plan, the underlying zone standards shall continue to apply (SMC 23.69.030.B). Please revise.
- Response 18: This section will be revised in the DMIMP to clarify a Green Factor for the College.
- Comment 19: Open Space. Page 32 of the DMIMP states that 31% of existing SCC parcels contain 31% open/green space, then include the term "sidewalks" within this calculation. Does this refer to public sidewalks? Recommend this # be specific to private property only, and not public right-of-way. Alternatively, differentiating between what is private vs public green space.

Response 19: Under the Open Space heading, it's stated that 31% of SCC owned/developed parcels as of January 2021 is open/green space. This number reflects the parcels owned by the College, not ROW. Currently, there is not any private green space on campus as all exterior spaces are open to the public except for a small portion of gated green space in the area of the South Plaza.

Comment 20: Facade Modulation. Page 32 of the DMIMP includes language describing facade modulation standards. Do these standards differ from those of the SMC? If so, recommend adding language specifying this - see previous note about adding a table or list specifically noting those dev standards that are being modified from code.

Response 20: Yes, the proposed façade modulation standards in the PDMIMP differ from the façade modulation standards required for the underlying commercial and multifamily midrise zones. Phrasing in the DMIMP has been modified to clarify that the proposed standard is different than the underlying zone.

Comment 21: Development Standards, Overlays. The institution is within a number of overlays, such as the Pike Pine Conservation Overlay. Are there dev standards from these overlays that are proposed for modification?

Response 21: Yes, there are multiple overlays within the proposed MIO Boundary. Generally, these Overlay districts speak to commercial and residential uses and do not directly align with SCC's Institutional use. The DAC recommended that the College commit to following the requirements of the Pike/Pine Conservation Overlay District (PDMIMP Recommendation 44). The College reviewed and considered the various overlay standards and, to the extent appropriate to its mission, has included/proposed standards in this document.

Comment 22: Development Standards. Page 32 of the DMIMP includes the term, "ground floor clear ceiling height." Recommend using the same term/language from the code "floor to floor height" for ease of calculation/measurement/etc.

Response 22: Phrasing has been changed to "floor-to-floor height" for consistency.

Comment 23: Landmark Nomination. Page 34 of the DMIMP notes that the 2001 MIMP includes a condition related to historic preservation. Are you proposing to retain this condition in the new MIMP?

Response 23: A reference to the 2001 MIMP was found on page 33 of the PDMIMP. Not specifically, however previous condition has been incorporated in this MIMP, negating the need to reference the 2001 MIMP.

Comment 24: Design Guidelines. Does the proposed format, of guidelines for each building, create repetition and/or conflict? Would categories of guidelines that are applicable to all development (such as pedestrian circulation, public realm, architectural concept, etc.) be a more palpable approach for implementation?

Response 24: Through the DAC review process, development-specific concerns have been brought up that differ between sites. Those items are addressed in the Project Specific Guidelines while the remainder of the guidelines will be used to address other elements of these developments and the remainder of campus.

Comment 25: Design Guidelines. The term "usability" is included in the campus-wide applicable guidelines. "Student usability of open space will be prioritized over public usability." How will usability be measured? Is there a different way to say this?

Response 25: The DAC has contended that the College needs to make a clear statement that public use be prioritized over College use. This response is included to specifically state that their priority and mission is to first serve its students, staff, and visitors, then the college community.

Comment 26: Design Guidelines. Some guidelines on page 41 of the DMIMP do not include the term "should," "will," etc. Recommend revising for clarity.

Response 26: The guidelines have been updated to include key terms (will, should, consider, standard) in each item.

Comment 27: Street Crossings. The map on page 48 of the DMIMP includes "street crossings." It appears these crossings may be proposed at locations that differ from those supported by the SMT or Streets Illustrated. Recommend consulting with SDOT and either removing or adding language to clarify that these crossings are desired and will be analyzed at time of future development.

Response 27: A sentence has been added under the Implementation of Safety and Security Design Strategies noting that all ROW improvements will be coordinated with SDOT.

Comment 28: Signage and Lighting. Page 49 of the DMIMP includes guidelines for signage and lighting. Have you considered including a campus wide conceptual signage and/or lighting plan? Does the institution want a consistent language for all signage? lighting?

Response 28: Yes, the intent is to create consistent signage and lighting. Absent a detailed design, design guidelines have been provided to encourage consistency throughout the campus. Design guidelines for lighting, light levels, and signage are provided in the DMIMP.

Comment 29: Harvard ROW Improvements. Page 50 of the DMIMP includes renderings from Streets Illustrated. Is it the intent that future development along these streets will proposed/construct these ROW improvements?

Response 29: The images shown (chicane and neckdown) are examples of infrastructure used to calm traffic and slow down vehicles. Future development may propose these ROW improvements or other improvements listed in Streets Illustrated or other nationally recognized publications on traffic calming and pedestrian infrastructure such as NACTO Urban Street Design Guide.

Comment 30: Design Guidelines. Page 52 of the DMIMP feels a bit repetitive of guidelines. Is there a way to consolidate or structure differently to eliminate repetition and facilitate ease of implement at time of future development?

Response 30: The Universal Design Guidelines on page 52 of the PDMIMP are provided in a single location to reinforce important considerations for universal design. The comment's note on repetition is understood, but as a public institution the College feels it's necessary to include these guidelines to support inclusive and equitable use of its facilities.

Comment 31: Comprehensive Plan Goals. Page 59 of the DMIMP describes consistency with Neighborhood Plans. Include a "description of the ways in which the institution will address goals and applicable policies under Healthy Growth, Aging, and Lifestyles and Lifelong Learning headings in the Community Well-Being Element of the Comprehensive Plan" per SMC 23.69.030.

Response 31: This is included in the DMIMP in Chapter 5.

Comment 32: Map Legibility. Regarding the map page 26 of the DMIMP: consider removing the base layer colors (green, gray, beige) and adding a hatched or other overlay to illustrate the extent of each overlay and how each relates to the other.

Response 32: Diagrams graphics have been updated in the DMIMP.

Comment 33: Historic Resources. Page 33 of the DMIMP includes a reference to structures greater than 25 years of age. Is this a state requirement? City SEPA policies and SDCI's MOA with DON note that structures greater than 50 years in age will be evaluated for historical significance. Revise to clarify.

Response 33: The City of Seattle Landmarks Preservation Board considers buildings more than 25 years old as eligible for Landmark Status. DAHP/State of Washington requires that buildings/structures be at least 50 years old.

Comment 34: Egyptian Theater. (Page 33 of the DMIMP). Is SCC proposing to keep the existing condition of the theater facades?

- Response 34: Yes, SCC is proposing to maintain the existing theater façades. The College has no plans or intentions to make changes to the existing façade of the Fine Arts Building (AKA Egyptian Theater).
- Comment 35: (Page 49 of the DMIMP). The term "unwanted behavior" is used. Is there another way to state this? Consider positive language describing what is desired.
- Response 35: This text has been revised in the DMIMP.
- Comment 36: Design Guidelines. (Page 52 of the DMIMP) The universal guidelines include similar or same guidelines as contained earlier in the document. Is there a way to revise the guidelines to improve future implementation?
- Response 36: See Response 30.
- Comment 37: Definitions. Definitions are included in the DMIMP, beginning on page 60. Is the intention that these definitions differ from those of the SMC? Consider deferring to the definitions in the code for ease of future implementation and understanding.
- Response 37: The intent is to match the definitions in the SMC. Some definitions expand on the SMC definitions for additional context with the MIMP document (like Gross Floor Area).
- Comment 38: Renderings. (Page 36 of the DMIMP). The rendering for the student housing/ garage - does this "blank façade" meet minimum SMC dev standards for the underlying zone? Should the MIMP include a requirement to have greater than underlying zone blank façade requirements? If putting this rendering in MIMP as "example," recommend being clear it is a concept image only (or is it?) and that the concept should meet MIMP dev standards/SMC standards.
- Response 38: The concept image provided in the PDMIMP was provided outside of the MIMP process. Due to the concerns noted in this and previous comments, this concept image has been removed.
- Comment 39: ROW improvements. Pages 40-41 of the DMIMP illustrate ROW improvements. Recommend adding language clarifying that ROW improvements will be required pursuant to the SMC and Streets Illustrated at time of future development.
- Response 39: References have been added.
- Comment 40: Definition. Page 40 of DMIMP - what is "ADJ?" Does not appear to be defined.
- Response 40: We were unable to find the reference to "ADJ" on page 40 in the PDMIMP. Please let us know if you can find it in the document.
- Comment 41: Update references to CAC to the new committee's name.
- Response 41: References to "CAC" have been updated to "DAC" in the DMIMP.

SDOT Comments in Memorandum Letter Dated March 6, 2023

- Comment This Memorandum letter noted that parking projections were based on data from 2015.
- Response The discussion of parking will be removed from the EIS because parking impacts are no longer part of the SEPA requirements, so this comment is no longer relevant.
- SCC will implement a transportation demand management program and the parking supply represents the maximum potential parking on-campus. SCC will continue to evaluate parking supply and ensure parking is at a level to meet parking demands but is not in excess such that it encourages vehicle travel to campus."

SDOT Comment Log dated March 8, 2023

- Comment 1: Page 65 includes a comment and response that references e-Park. Seattle no longer maintains this program, though there are private vendors/websites that provide information on garage rates and availability. <https://www.seattle.gov/transportation/projects-and-programs/programs/parking-program/e-park>
- Response 1: Noted. The noted reference to e-Park was provided in a comment from the DAC.
- Comment 2: Page 66 references existing ADA spaces in the public right-of-way on Harvard and page 65 discusses potential traffic calming street improvements on Harvard that may include physical changes. Any changes to Harvard should include updating parking management on this street by addition of paid parking and review of load zones to meet existing critical building access needs. In addition, if the street is being revised and ADA spaces are intended to be retained, design of the street should reflect new accessibility standards (PROWAG) for ADA spaces in the public right-of-way.
- Response 2: This comment references comments and responses to the DAC. Per SDOT Seattle Parking Map, between E Pine Street and E Howell Street the east-side of Harvard Ave is a paid parking zone and the west-side does not allow parking. PROWAG Accessibility standards will be reviewed for guidance on accessible parking located in ROW. Comments above are noted and will be reviewed when any project planning and design occurs.
- Comment 3: While supportive of the change to the Seattle Central parking garage to add housing, the garage does serve a neighborhood benefit, particularly for nightlife goers and employees of nearby bars/restaurants. People working the late shift until 1-2-3 AM have fewer transit options. College should explore if there is a way to support continued availability of public nighttime parking spaces following changes to the garages.
- Response 3: The College has no plans to limit or suspend public access to the renovated parking garage as part of the Student Housing project.
- Comment 3: While supportive of the change to the Seattle Central parking garage to add housing, the garage does serve a neighborhood benefit, particularly for nightlife goers and employees of nearby bars/restaurants. People working the late shift until 1-2-3 AM have fewer transit options. College should explore if there is a way to support continued availability of public nighttime parking spaces following changes to the garages.
- Response 3: The College has no plans to limit or suspend public access to the renovated parking garage as part of the Student Housing project.
- Comment 5: page 47 mentions adding secure bike parking, weather protection for bike racks, and long-term bike parking for student housing. Consider adding something about E-bike charging in the long-term secured area. and add a bullet point about adding signage to direct users to bike parking and avoiding routes to long-term bike parking with stairs or multiple doors/level changes.
- Response 5 A Bicycle Masterplan has been included in the DMIMP. It included information on the above points.
- Comment 6: in areas where most pedestrians need to access student open space by stairs, make sure there is another ADA accessible way to enter the space that is easily and close by. consider adding bike rails to stairs so students can easily bring their bikes upstairs to access racks by building doors.
- Response 6: Noted – accessibility to all College open spaces is a state requirement and is reviewed by the State Facilities Accessibility Advisory Committee (SFACC) as part of project development.
- A Bicycle Masterplan has been included in the Draft MIMP. It includes information on the above points. Where Major Capital Projects occur, accessibility issues at existing outdoor areas will be addressed

Comment 7: Having ITEC parking vehicles access the building off Harvard/Howell should allow for the closure of the driveway on Broadway. How do parking vehicles access the ITEC Building from Harvard/Howell? Will there be a parking garage entrance on the east side of this intersection (this area is also identified as a pedestrian corridor)? Maintaining the Broadway driveway would adversely impact streetcar, bus, bicycle, and motor vehicle operations on Broadway; the proximity to the pedestrian crossing could also lead to an unsafe scenario.

Response 7: If technically viable, vehicles will access the ITEC garage via a connection from Harvard Ave through the existing SAM garage entrance. This approach will preserve the Howell Street Pedestrian Passageway. This note has been added to the DMIMP in Chapter 3, under the Parking subheading. Existing driveways serving the surface parking lot on Boardway would be removed as part of the ITEC project.

Comment 8: Please revise Figure 14 to show parking/motor vehicle access points

Response 8: Parking/motor vehicle access points have been added in the DMIMP.

Comment 9: Referencing chapter 4, pg. 1-26 & chapter 4 pg. 1-18 - We understand the desire to implement traffic calming on Harvard, however we rarely implement chicanes, and they may not be worth presenting.

Response 9: We understand and appreciate the comment, however, please see SDOT comment 11 acknowledging chicanes and other traffic calming measures.

Comment 10: Referencing Chapter 3, pg. 3-13 - SDOT supports the proposed net reduction in parking spaces from 633 to 519. SCC should consider further reductions beyond this amount given the campus's location in a dense, transit-rich environment. Furthermore, new parking spaces constructed should prioritize Electric Vehicles, carpooling, and other sustainable modes (e.g., using secure spaces for bikes, scooters, etc.)

Response 10: The proposed is a maximum parking supply. The College will confirm specific parking needs at the time buildings are designed and constructed and reduce parking where reasonable. The College will include electric vehicle charging stations in new/renovated parking facilities. This information has been integrated into the DMIMP. See Chapter 3, subsection on Parking.

Comment 11: Referencing chapter 3, pg. 3-11 - SDOT advocates that SCC should think more holistically about how the majority of its users access campus by non-driving modes, and how that proportion will increase in the future. SCC should consider the opportunity to create improvements to Harvard Ave to create a shared street, Stay Healthy Street, or similar concept in which non-driving users (pedestrians, cyclists, scooter users, and others) are provided highest priority, while still allowing local vehicular access at very slow speeds. Improvements could take the form of "gateway" treatments should such as raised crosswalks and signage, traffic calming measures such as diverters or chicanes, and/or a raised street bed in which sidewalk and street level are the same (e.g., a "curb less' street). This comment references this portion of the draft: "Primary vehicle traffic occurs along Broadway and Pine Streets. Harvard traffic is largely limited to localized neighborhood use, campus ADA parking, and campus services loading zones. The proposed plan calls for maintaining all existing vehicular access points and circulation with no recommendations for improvements."

Response 11: The College has revised the language regarding vehicular access noted above. The College appreciates SDOT's encouragement to create enhanced pedestrian space in the Harvard Ave ROW. If an activated pedestrian street is proposed with any projects, the College will advocate for additional pedestrian safety measures including but not limited to bollards, gateway treatments, raised crosswalks, chicanes, cobblestones and/or textured paving (via deviation process as noted in Seattle ROW Improvements Manual; 3.2 Sidewalks), and other measures to limit vehicle speeds and throughput. The College will work closely with SDOT on any ROW changes and/or improvements.

Comment 12: Referencing appendix B, pg. 3 - SDOT would like to confirm that a reduction in parking, and revenue related to it, should not impact SCC's transit subsidy program which will be an essential element of the TMP associated with this new MIMP.

Response 12: The College has agreed to provide funding for the transit pass subsidy via local funds.

Comment 13: Referencing chapter 4, pg. 1-18 - Pedestrian Street Crossing section notes the consideration of traffic calming elements along Harvard to improve pedestrian crossings. Any traffic calming elements will require approval by SDOT. Recommend working closely with SDOT when evaluating potential traffic calming options to better understand City policy / guidance on when various alternatives would be considered appropriate and supported by SDOT.

Response 13: The College understands that close coordination with SDOT will be vital to the successful planning and implementation of pedestrian crossings, traffic calming measures, and any other work occurring in the right-of-way (ROW). When planning and design commences for any projects, the College will consult SDOT regarding any ROW improvements as early as possible.

Comment 14: Referencing chapter 4, pg. 1-18 - Sidewalk Improvements section discusses use of sidewalk paving materials and treatments. Please refer to City of Seattle's Street Illustrated for requirements / guidance on construction of sidewalks in the right of way and the use of special paving materials /treatments.

Response 14: As noted in response 11 and response 13, the College understands that all ROW improvements need to be closely coordinated with SDOT. If alternative sidewalk or street paving materials/patterns are pursued, the College will engage in the deviation process as required (per Seattle ROW Improvements Manual; 3.2 Sidewalks).

Comment 15: See earlier comment regarding traffic calming measures. (chapter 4 / pg. 42)

Response 15: See response 14.

Comment 16: Referencing Appendix B, pg. 4 - Response 20 notes SCC is studying proposal of curb cut for service vehicle at the intersection of Harvard and Howell. New curb cuts would need approval from SDCI, SDOT would not support curb cut at the intersection but look to have it located a minimum of 40' from the intersection.

Response 16: This item was previously approved by SDOT and SDCI. Construction was completed in 2023.